

Tobacco Road Lot 124

ASHPORT
ELEVATION G



3-CAR
FRONT LOAD
OPTION

DOOR STYLE PER
PURCHASE ORDER
SIZE 3/0 x 8/0

INCLUDED OPTIONS:
1st FLOOR
COVERED PORCH
GOURMET KITCHEN
FIREPLACE W/ BUILT-INS
BOX OAK STAIRS
OPEN RAIL
TRAY CEILING @ OWNERS
OWNERS DELUXE BATH
GUEST SUITE ILO STUDY
GUEST SHOWER ILO TUB
BENCH @ MUD ROOM
GARAGE SERVICE DOOR
3RD CAR GARAGE
Door to Laundry Room
Opt. Windows in Casual Dining

2nd FLOOR
BEDROOM 4 W/ BATH
2ND SINK @ BATH 2
UNFINISHED STORAGE

NOTES & CODES

GENERAL NOTES

- NATIONAL, STATE, AND LOCAL CODES ARE REGULATIONS SHALL BE APPLIED TO ALL WORK BEING PERFORMED
- BUILDER TO VERIFY ALL SITE CONDITIONS, STRUCTURAL OPTIONS AND DIMENSIONS BEFORE START OF CONSTRUCTION. DISCREPANCIES SHALL BE REPORTED TO PLAN DESIGNER FOR CORRECTIONS OR CLARIFICATION BEFORE CONTINUING WORK.
- DIMENSIONS ONLY BE READ OR CALCULATED AND NOT SCALED

DESIGN CRITERIA

ROOF LIVE LOAD: 20PSF.
FLOOR LIVE LOAD: 40PSF. (INCLUDES DECKS, PATIOS
AND PORCHES)
BASIC WIND VELOCITY: 90MPH
SEISMIC DESIGN CATEGORY: B

SQUARE FOOTAGE

CONDITIONED SPACE:	
FIRST FLOOR	1690 SQ. FT.
SECOND FLOOR	1074 SQ. FT.
TOTAL CONDITIONED (BASE PLAN):	2764 SQ. FT.
UNCONDITIONED SPACE:	
FRONT ENTRY GARAGE	460 SQ. FT.
THIRD BAY GARAGE	670 SQ. FT.
FRONT PORCH	145 SQ. FT.
ADDITIONAL OPTIONS:	
COVERED PATIO/DECK	120 SQ. FT.
THE FINISHED SQUARE FOOTAGE OF A PLAN IS TO BE REPORTED TO THE NEAREST WHOLE FOOT. THE FINISHED SQUARE FOOTAGE CALCULATIONS FOR THIS PLAN WERE MADE BASED ON PLAN DIMENSIONS OR DIGITAL DRAWINGS FILES ONLY, AND MAY VARY FROM THE FINISHED SQUARE FOOTAGE OF THE PLAN AS BUILT.	

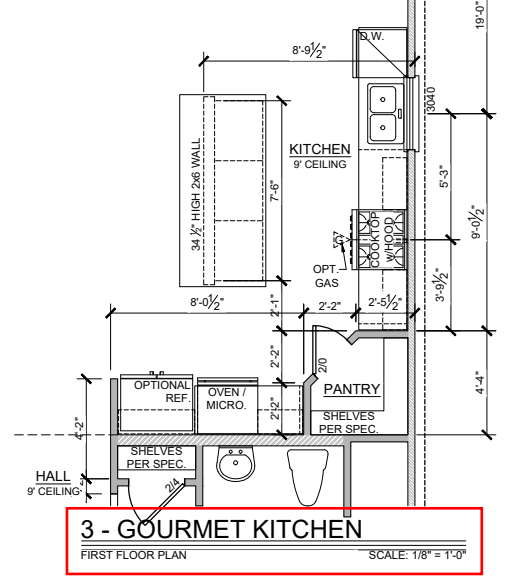
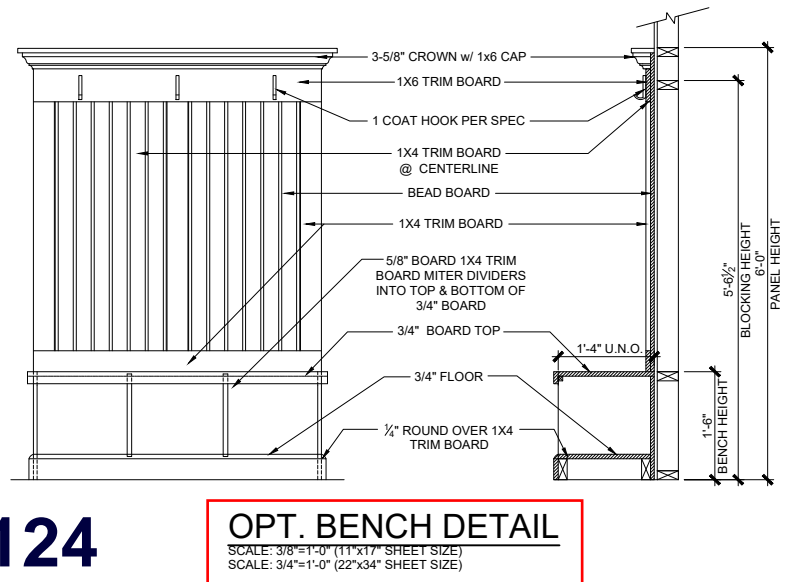
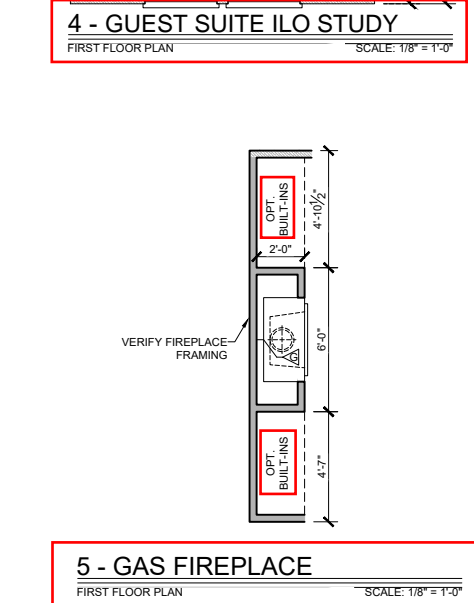
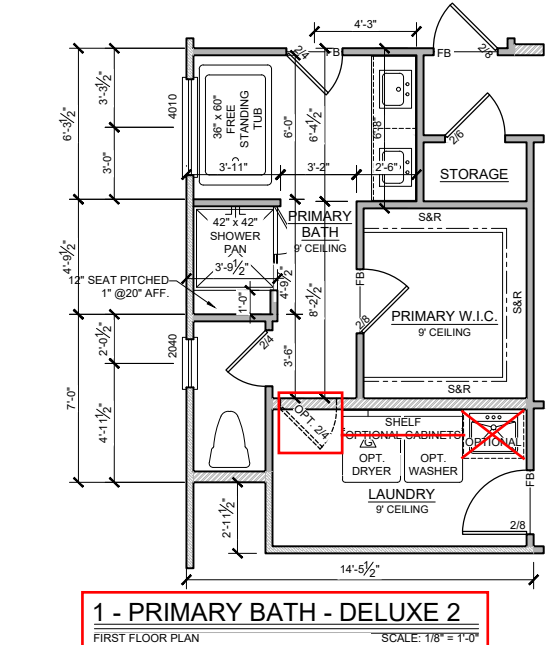
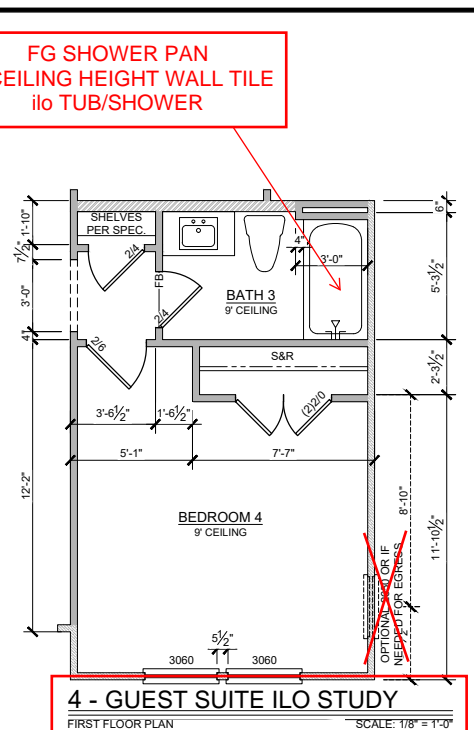
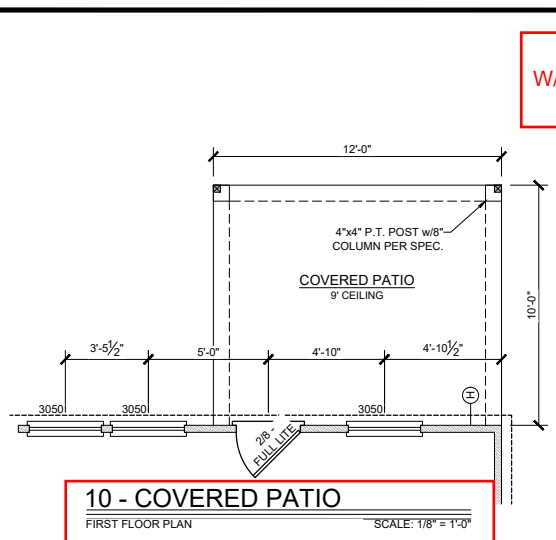
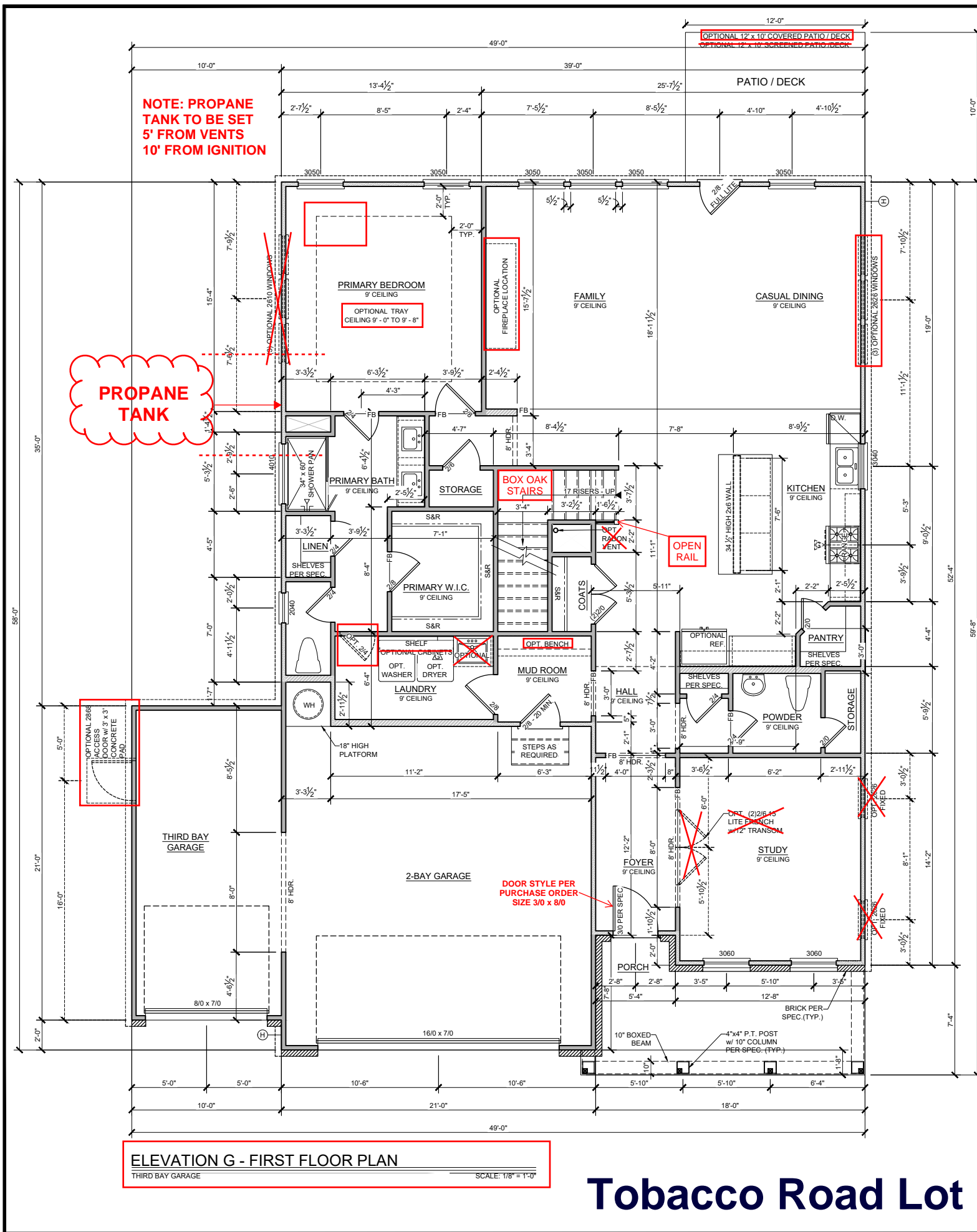


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336 JAMES RECORD ROAD
HUNTSVILLE, ALABAMA 35824
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PLAN NAME: ASHPORT - RLH
COMMUNITY: COMMUNITY
LOT: XX

REVISION NO. 1
BY ALFONSO.ARCINIEGAS
REVISION DATE 2025-09-22

COVER SHEET
SHEET NO: A3-CSG
SIDE ENTRY
GARAGE



WALL TYPE HATCH LEGEND	
2 x 4 FRAMED WALL	
2 x 6 FRAMED WALL	
DOUBLE FRAMED WALL	
LOAD BEARING WALL	
FIRE RATED WALL	
MASONRY TEXTURE	
CONCRETE TEXTURE	
THERMAL BARRIER	
GENERAL FRAMING NOTES	
• DOOR FRAMES NEAR CORNERS SHALL BE A MINIMUM 4 1/2" FROM CORNER. WHEN TWO DOORS ARE AT SAME CORNER, MINIMUM OF 6" FROM CORNER. DOORS AT CLOSETS (UNLESS NOTED OTHERWISE) SHALL BE CENTERED ON CLOSET.	
• REFER TO DOOR AND WINDOW MANUFACTURER SPECIFICATIONS FOR ROUGH OPENING SIZES.	
• ATTIC ACCESS SHALL BE PROVIDED AT ALL ATTIC AREAS WITH A HEIGHT GREATER THAN 30" MINIMUM. CLEAR ATTIC ACCESS SHALL BE 20"x30", PULL DOWN STAIRS AND ACCESS DOORS IN KNEE WALLS MEETING MINIMUM CRITERIA ARE ALSO ACCEPTABLE.	
• GARAGE WALLS, AT A MINIMUM SHALL BE SEPARATED FROM THE LIVING SPACE BY INSTALLING 1/2" GYPSUM BOARD ON THE GARAGE SIDE OF THE WALL. WITH HABITABLE SPACE ABOVE, THE INSIDE OF ALL GARAGE WALLS REQUIRE 1/2" GWB SUPPORTING 5/8" TYPE X GWB ON CEILING.	
• EXTERIOR GUARD RAILS SHALL BE NOT LESS THAN 36", AND COMPLY WITH IRC SECTION R312.1	
• DIMENSIONS ARE ONLY TO BE READ OR CALCULATED, NOT SCALED	

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PLAN NAME: ASHPORT - RLH

COMMUNITY: COMMUNITY

LOT: XX

REVISION NO. 1

BY ALFONSO.ARCINIEGAS

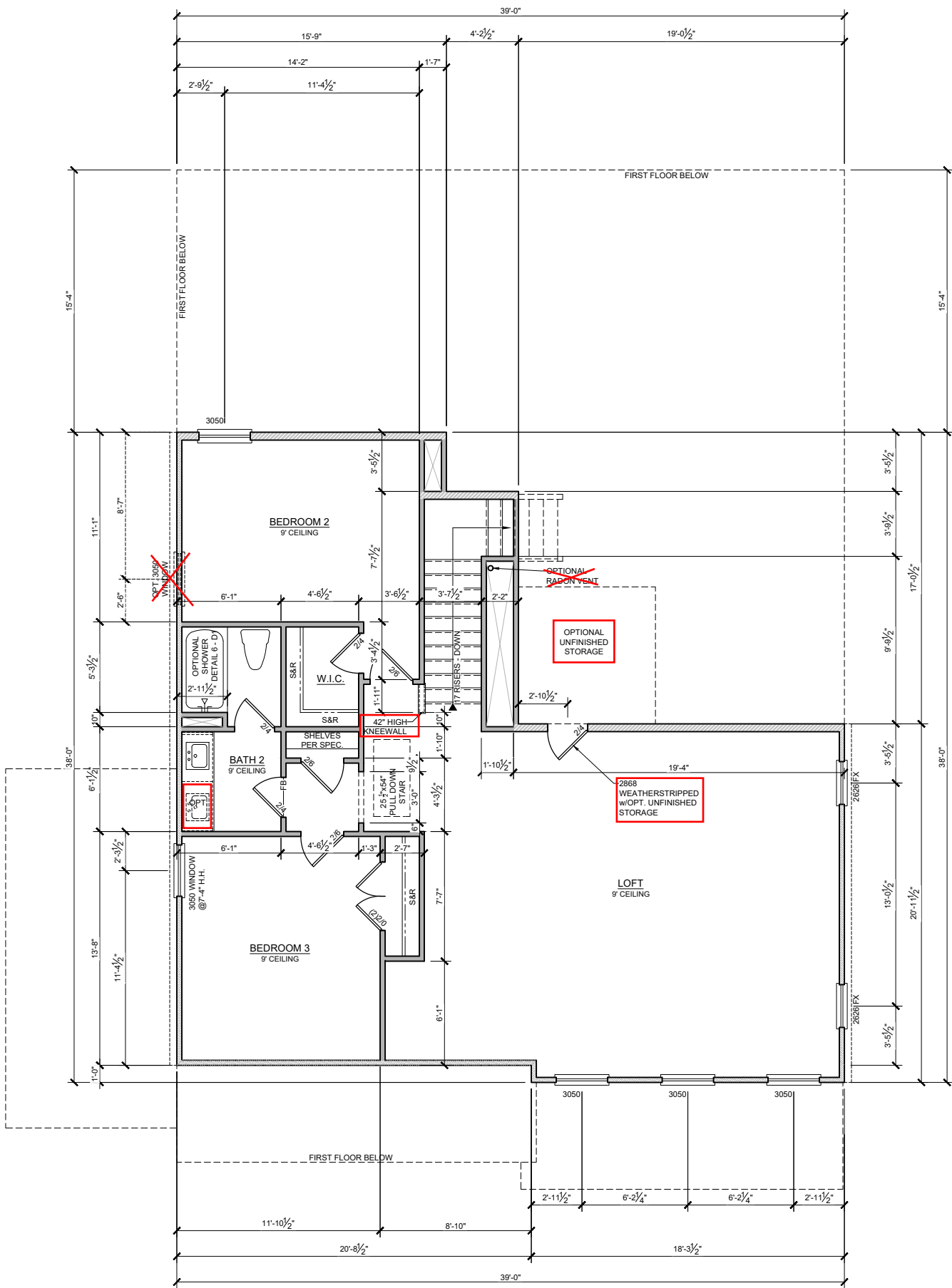
REVISION DATE 2025-09-22

FIRST FLOOR PLAN

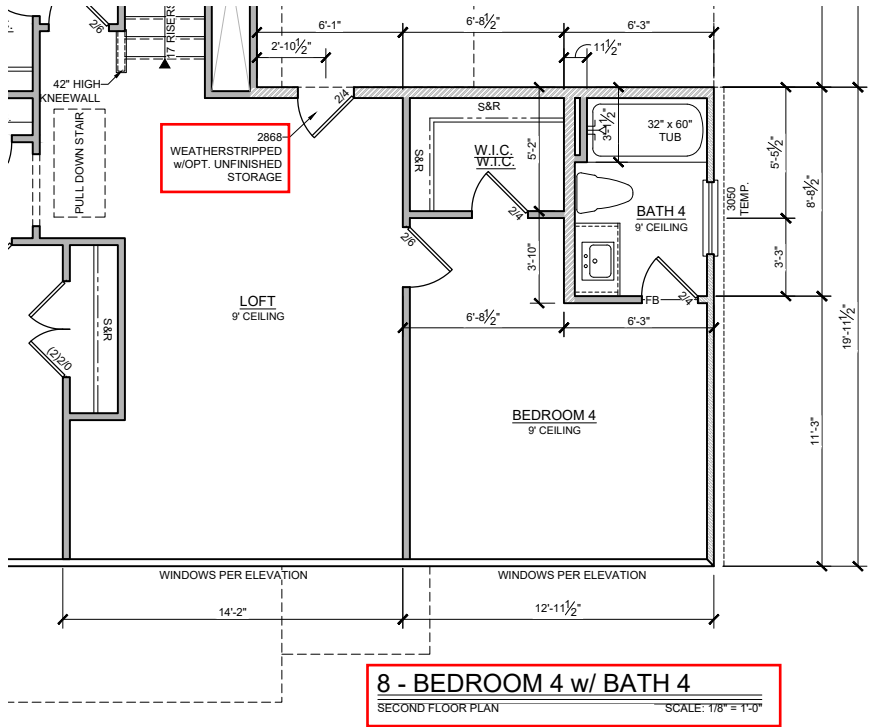
SHEET NO: A3-1.0G

THIRD BAY GARAGE

Tobacco Road Lot 124



ELEVATION G - SECOND FLOOR PLAN
THIRD BAY GARAGE
SCALE: 1/8" = 1'-0"



8 - BEDROOM 4 w/ BATH 4
SECOND FLOOR PLAN
SCALE: 1/8" = 1'-0"

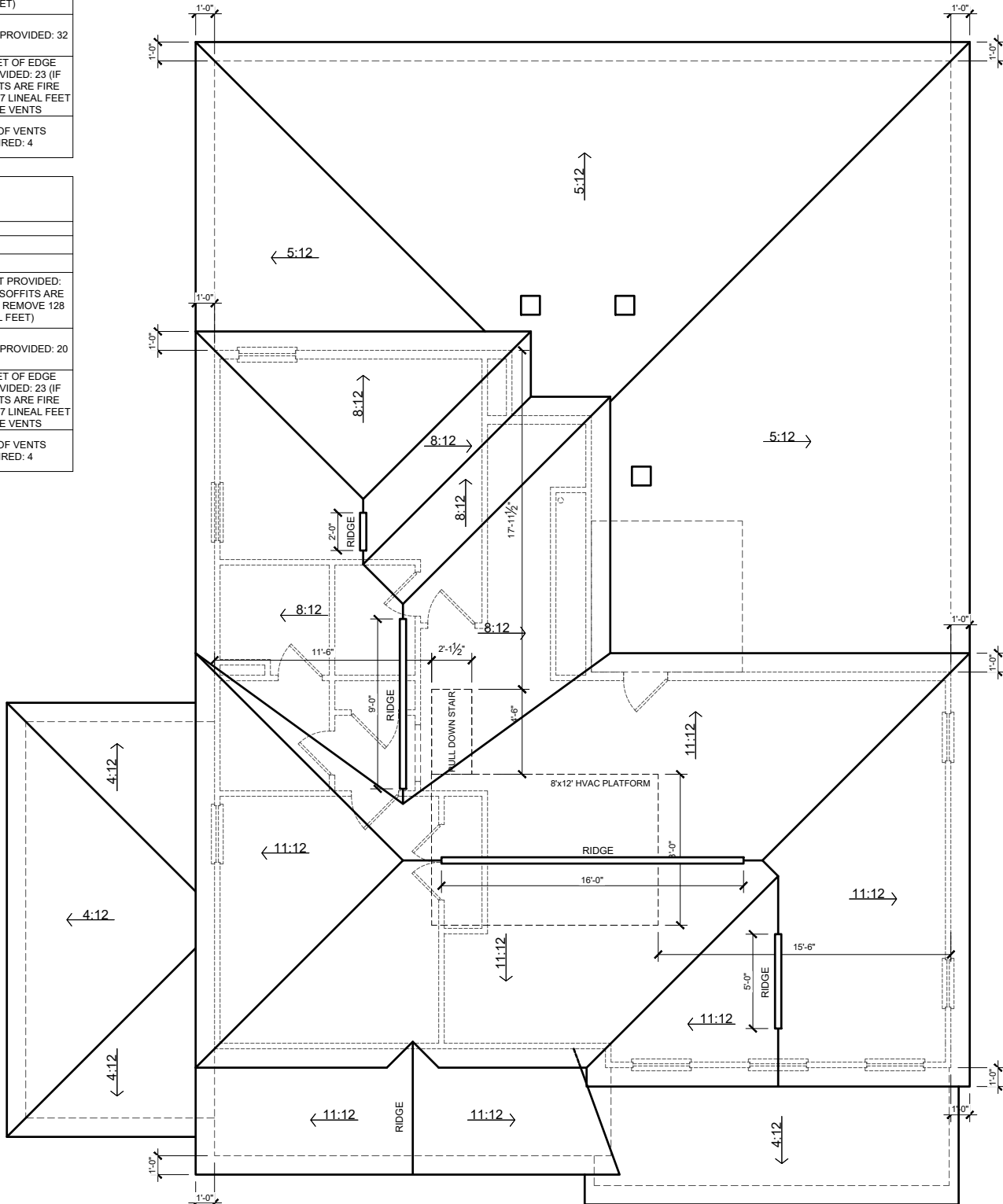
WALL TYPE HATCH LEGEND	
2 x 4 FRAMED WALL	
2 x 6 FRAMED WALL	
DOUBLE FRAMED WALL	
LOAD BEARING WALL	
FIRE RATED WALL	
MASONRY TEXTURE	
CONCRETE TEXTURE	
THERMAL BARRIER	

- GENERAL FRAMING NOTES
- DOOR FRAMES NEAR CORNERS SHALL BE A MINIMUM 4 1/2" FROM CORNER. WHEN TWO DOORS ARE AT SAME CORNER, MINIMUM OF 6" FROM CORNER. DOORS AT CLOSETS (UNLESS NOTED OTHERWISE) SHALL BE CENTERED ON CLOSET.
 - REFER TO DOOR AND WINDOW MANUFACTURER SPECIFICATIONS FOR ROUGH OPENING SIZES.
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 - GARAGE WALLS, AT A MINIMUM SHALL BE SEPARATED FROM THE LIVING SPACE BY INSTALLING 1/2" GYPSUM BOARD ON THE GARAGE SIDE OF THE WALL, WITH HABITABLE SPACE ABOVE. THE INSIDE OF ALL GARAGE WALLS REQUIRE 1/2" GWB SUPPORTING 5/8" TYPE X GWB ON CEILING.
 - EXTERIOR GUARD RAILS SHALL BE NOT LESS THAN 36", AND COMPLY WITH IRC SECTION R312.1
 - DIMENSIONS ARE ONLY TO BE READ OR CALCULATED, NOT SCALED

Tobacco Road Lot 124

MAIN HOUSE ATTIC VENTING - PORCH/GARAGE CALCULATIONS (1:300 GUIDELINE)		
ATTIC AREA	233	
INTAKE AREA REQUIRED	111.840000	
EXHAUST AREA REQUIRED	111.840000	
VENTED SOFFIT SPECS - 12 SQ. IN. PER LINEAL FOOT	22.368000	LINEAL FEET PROVIDED: 154 (IF SIDE SOFFITS ARE FIRE RATED, REMOVE 128 LINEAL FEET)
RIDGE VENT SPECS - 18 SQ. IN. PER LINEAL FOOT	6.2133	LINEAL FEET PROVIDED: 32
EDGE VENT SPECS (IF REQUIRED) - 10 SQ. IN. PER LINEAL FOOT	11.184000	LINEAL FEET OF EDGE VENTS PROVIDED: 23 (IF SIDE SOFFITS ARE FIRE RATED, ADD 87 LINEAL FEET OF EDGE VENTS)
ROOF VENT SPECS (IF REQUIRED) - 150 SQ. IN. PER VENT	0.745600	# OF ROOF VENTS REQUIRED: 4

THIRD BAY GARAGE ATTIC VENTING CALCULATIONS (1:150 GUIDELINE)		
ATTIC AREA	230	
INTAKE AREA REQUIRED	110.400000	
EXHAUST AREA REQUIRED	110.400000	
VENTED SOFFIT SPECS - 5 SQ. IN. PER LINEAL FOOT	22.080000	LINEAL FEET PROVIDED: 206 (IF SIDE SOFFITS ARE FIRE RATED, REMOVE 128 LINEAL FEET)
RIDGE VENT SPECS - 34.5 SQ. IN. PER LINEAL FOOT	3.2000	LINEAL FEET PROVIDED: 20
EDGE VENT SPECS (IF REQUIRED) - 10 SQ. IN. PER LINEAL FOOT	11.040000	LINEAL FEET OF EDGE VENTS PROVIDED: 23 (IF SIDE SOFFITS ARE FIRE RATED, ADD 87 LINEAL FEET OF EDGE VENTS)
ROOF VENT SPECS (IF REQUIRED) - 150 SQ. IN. PER VENT	0.736000	# OF ROOF VENTS REQUIRED: 4



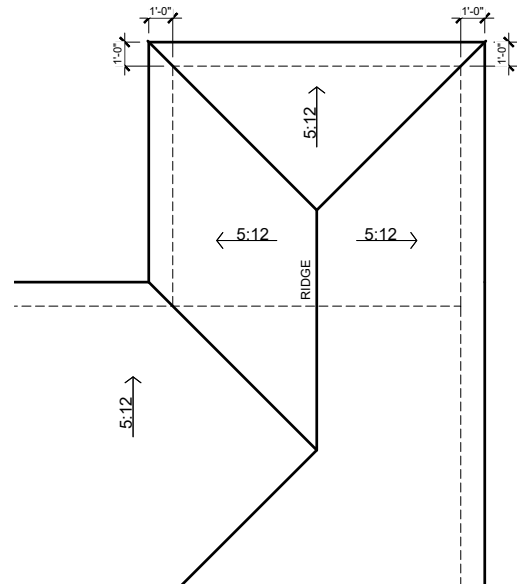
ELEVATION G - ROOF PLAN

THIRD BAY GARAGE

SCALE: 1/8" = 1'-0"

MAIN HOUSE ATTIC VENTING - SECOND FLOOR CALCULATIONS (1:300 GUIDELINE)		
ATTIC AREA	1122	
INTAKE AREA REQUIRED	538.560000	
EXHAUST AREA REQUIRED	538.560000	
VENTED SOFFIT SPECS - 12 SQ. IN. PER LINEAL FOOT	107.712000	LINEAL FEET PROVIDED: 154 (IF SIDE SOFFITS ARE FIRE RATED, REMOVE 128 LINEAL FEET)
RIDGE VENT SPECS - 18 SQ. IN. PER LINEAL FOOT	29.9200	LINEAL FEET PROVIDED: 32
EDGE VENT SPECS (IF REQUIRED) - 10 SQ. IN. PER LINEAL FOOT	53.856000	LINEAL FEET OF EDGE VENTS PROVIDED: 23 (IF SIDE SOFFITS ARE FIRE RATED, ADD 87 LINEAL FEET OF EDGE VENTS)
ROOF VENT SPECS (IF REQUIRED) - 150 SQ. IN. PER VENT	3.590400	# OF ROOF VENTS REQUIRED: 4

MAIN HOUSE ATTIC VENTING - FIRST FLOOR CALCULATIONS (1:300 GUIDELINE)		
ATTIC AREA	937	
INTAKE AREA REQUIRED	449.760000	
EXHAUST AREA REQUIRED	449.760000	
VENTED SOFFIT SPECS - 12 SQ. IN. PER LINEAL FOOT	89.952000	LINEAL FEET PROVIDED: 90 (IF SIDE SOFFITS ARE FIRE RATED, REMOVE 128 LINEAL FEET)
RIDGE VENT SPECS - 18 SQ. IN. PER LINEAL FOOT	24.9867	LINEAL FEET PROVIDED: 32
EDGE VENT SPECS (IF REQUIRED) - 10 SQ. IN. PER LINEAL FOOT	44.976000	LINEAL FEET OF EDGE VENTS PROVIDED: 23 (IF SIDE SOFFITS ARE FIRE RATED, ADD 87 LINEAL FEET OF EDGE VENTS)
ROOF VENT SPECS (IF REQUIRED) - 150 SQ. IN. PER VENT	2.998400	# OF ROOF VENTS REQUIRED: 4



ROOF PLAN

COVERED PATIO/DECK OR SCREENED PATIO/DECK SCALE: 1/8" = 1'-0"



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PLAN NAME: ASHPORT - RLH
COMMUNITY: COMMUNITY
LOT: XX

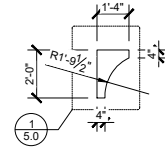
REVISION NO. 1
BY ALFONSO.ARCINIEGAS
REVISION DATE 2025-09-22
ROOF PLAN
SHEET NO: A3-4.0G
THIRD BAY GARAGE

GENERAL ELEVATION NOTES	
•	FASCIA, FRIEZE, RAKE, AND SKIRT BOARDS TO BE 1X UNLESS OTHERWISE NOTED
•	ALL OTHER TRIM TO BE 5/4 UNLESS OTHERWISE NOTED
•	LAP SIDING REVEALS PER SPEC., UNLESS OTHERWISE NOTED
•	ROOF COVERING TO BE SHINGLES PER SPEC., UNLESS NOTED OTHERWISE



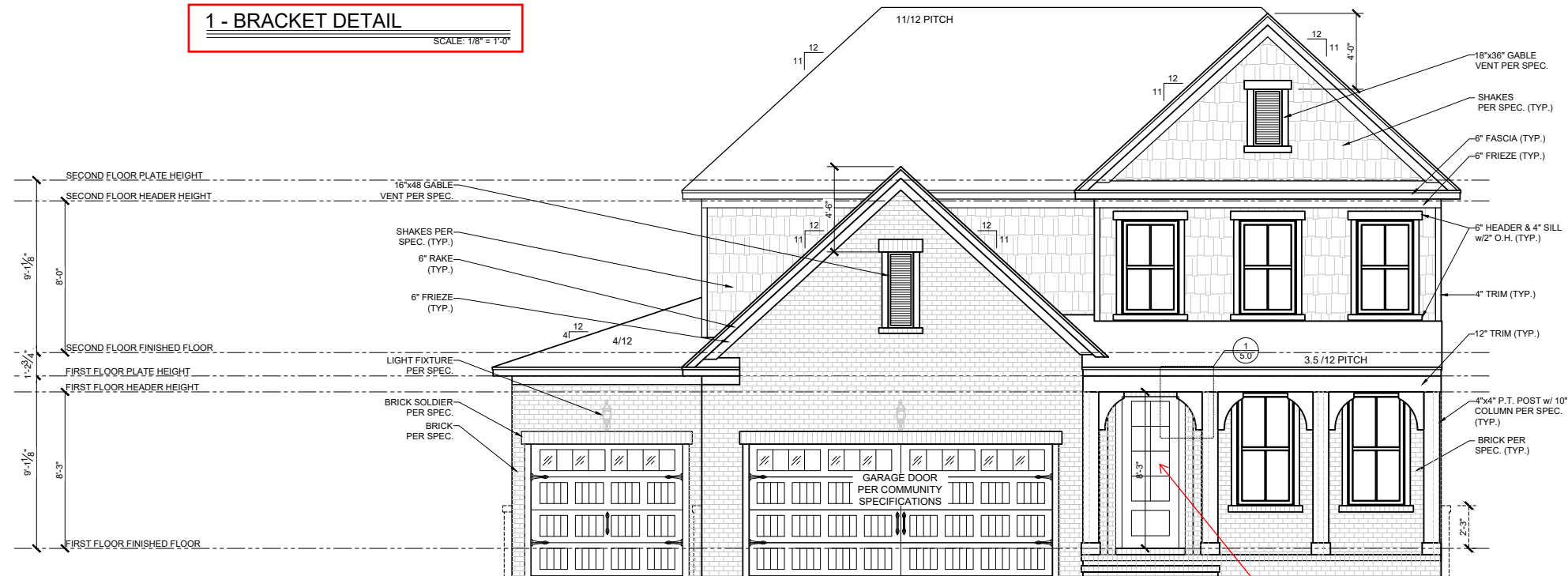
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1 - BRACKET DETAIL

SCALE: 1/8" = 1'-0"



ELEVATION G - FRONT ELEVATION

THIRD BAY GARAGE

SCALE: 1/8" = 1'-0"

**DOOR STYLE PER
PURCHASE ORDER
SIZE 3/0 x 8/0**

PLAN NAME:	ASHPORT - RLH
COMMUNITY:	COMMUNITY
LOT:	XX

VISION NO. 1
ALFONSO.ARCINIEGAS
VISION DATE 2025-09-22
FRONT ELEVATION
9' CLG.
EET NO: A3-5.4G
THIRD BAY
GARAGE

GENERAL ELEVATION NOTES	
•	FASCIA, FRIEZE, RAKE, AND SKIRT BOARDS TO BE 1X UNLESS OTHERWISE NOTED
•	ALL OTHER TRIM TO BE 5/4 UNLESS OTHERWISE NOTED
•	LAP SIDING REVEALS PER SPEC., UNLESS OTHERWISE NOTED
•	ROOF COVERING TO BE SHINGLES PER SPEC., UNLESS NOTED OTHERWISE



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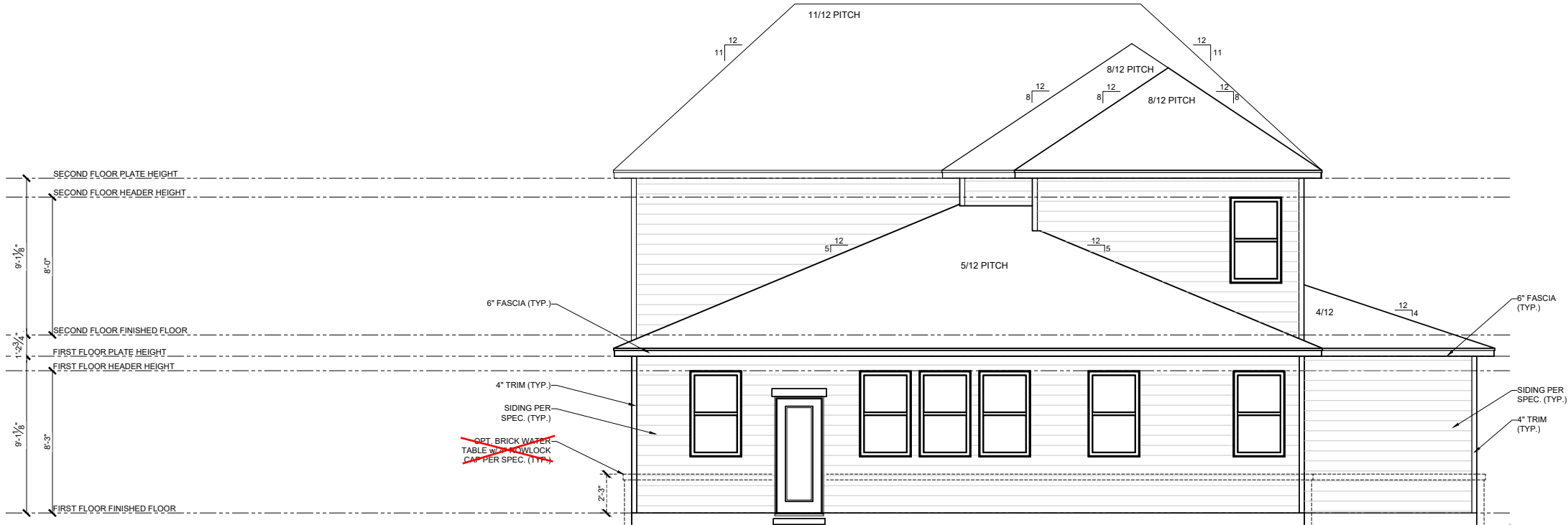
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HUNTSVILLE, ALABAMA 35824

PLAN NAME:	ASHPORT - RLH
COMMUNITY:	COMMUNITY
LOT:	XX

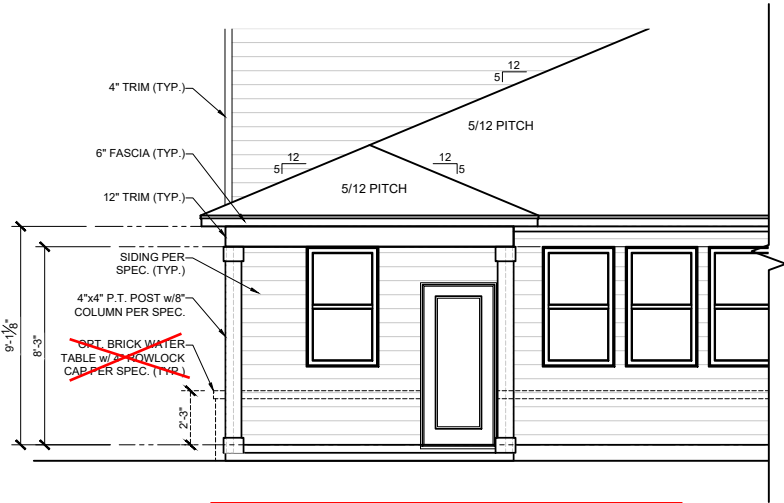
VISION NO. 1
ALFONSO.ARCINIEGAS
VISION DATE 2025-09-22
LEFT ELEVATION
9' CLG.
HEET NO: A3-5.5G
THIRD BAY
GARAGE

Tobacco Road Lot 124

- GENERAL ELEVATION NOTES
- FASCIA, FRIEZE, RAKE, AND SKIRT BOARDS TO BE 1X UNLESS OTHERWISE NOTED
 - ALL OTHER TRIM TO BE 5/4 UNLESS OTHERWISE NOTED
 - LAP SIDING REVEALS PER SPEC., UNLESS OTHERWISE NOTED
 - ROOF COVERING TO BE SHINGLES PER SPEC., UNLESS NOTED OTHERWISE



ELEVATION G - REAR ELEVATION
THIRD BAY GARAGE
SCALE: 1/8" = 1'-0"



COVERED PATIO - REAR ELEVATION
SLAB CONDITION
SCALE: 1/8" = 1'-0"



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PLAN NAME: ASHPORT - RLH
COMMUNITY: COMMUNITY
LOT: XX

REVISION NO. 1
BY ALFONSO.ARCINIEGAS
REVISION DATE 2025-09-22
REAR ELEVATION
9' CLG.
SHEET NO: A3-5.6G
THIRD BAY
GARAGE

Tobacco Road Lot 124

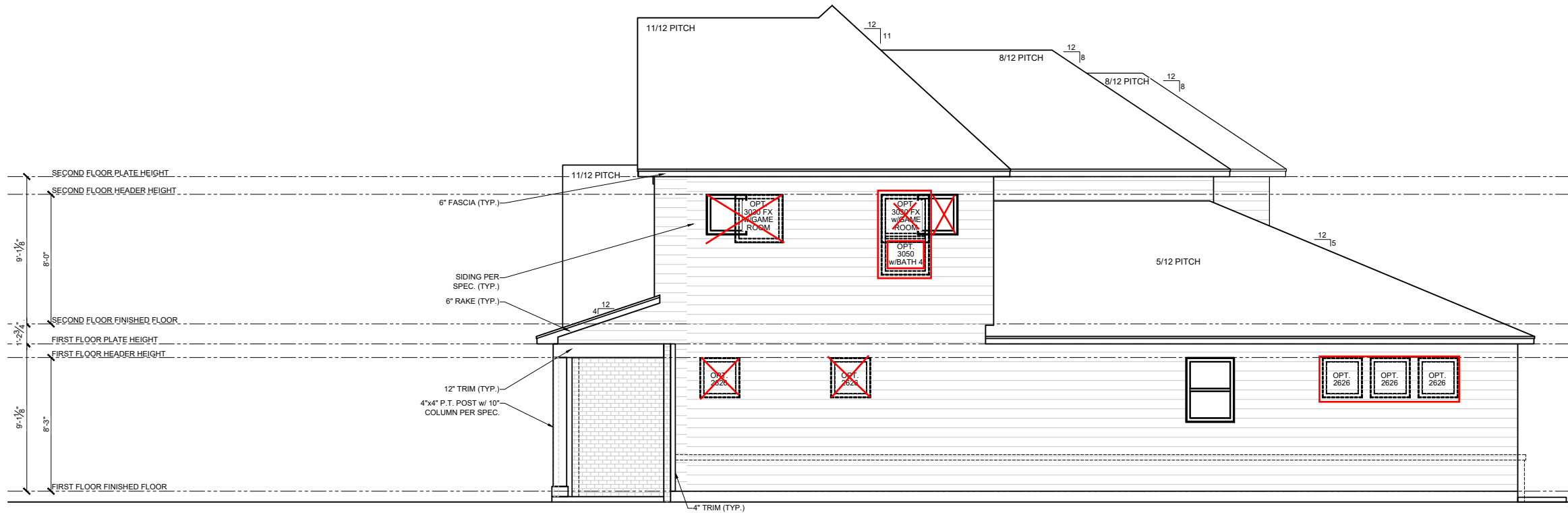
- GENERAL ELEVATION NOTES
- FASCIA, FRIEZE, RAKE, AND SKIRT BOARDS TO BE 1X UNLESS OTHERWISE NOTED
 - ALL OTHER TRIM TO BE 5/4 UNLESS OTHERWISE NOTED
 - LAP SIDING REVEALS PER SPEC., UNLESS OTHERWISE NOTED
 - ROOF COVERING TO BE SHINGLES PER SPEC., UNLESS NOTED OTHERWISE



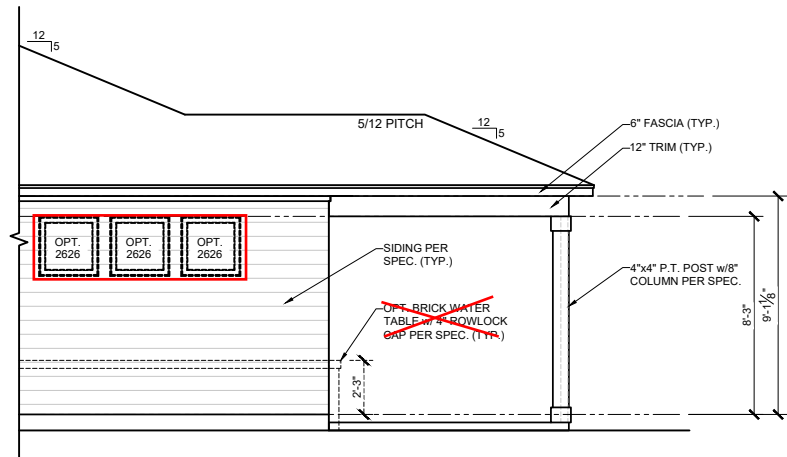
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PLAN NAME: ASHPORT - RLH
COMMUNITY: COMMUNITY
LOT: XX

REVISION NO. 1
BY ALFONSO.ARCINIEGAS
REVISION DATE 2025-09-22
RIGHT ELEVATION
9' CLG.
SHEET NO: A3-5.7G
THIRD BAY GARAGE



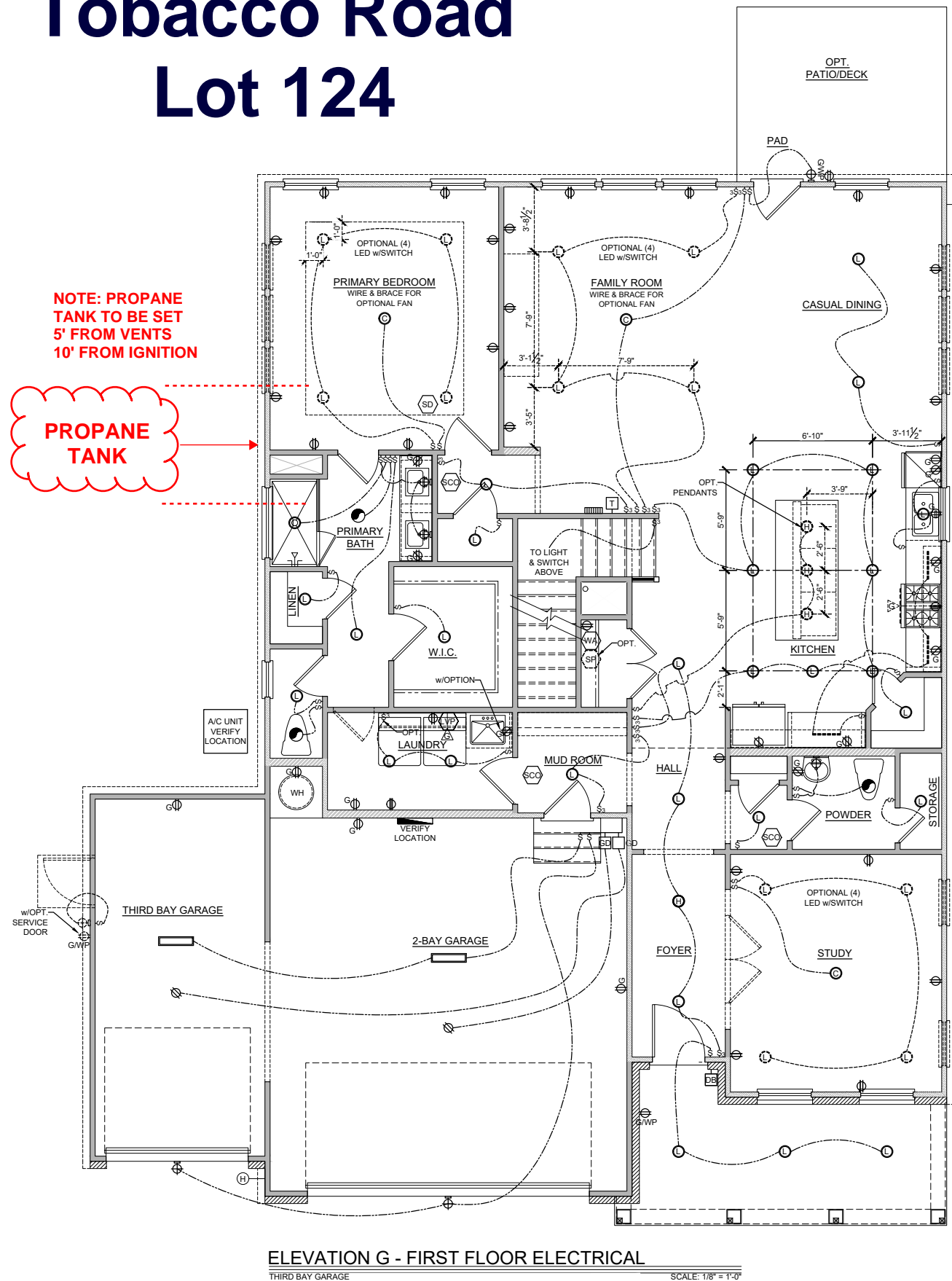
ELEVATION G - RIGHT ELEVATION
THIRD BAY GARAGE
SCALE: 1/8" = 1'-0"



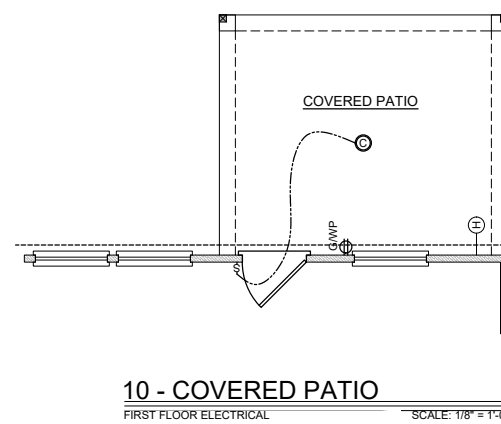
COVERED PATIO - RIGHT ELEVATION
SLAB CONDITION
SCALE: 1/8" = 1'-0"

Tobacco Road

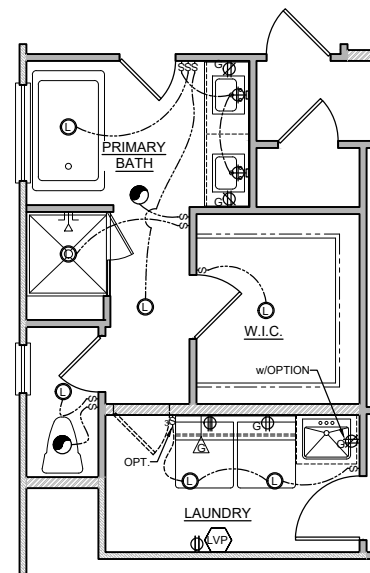
Lot 124



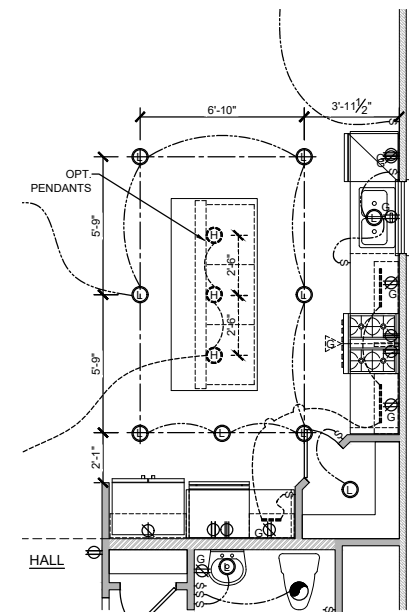
ELEVATION G - FIRST FLOOR ELECTRICAL



10 - COVERED PATIO
FIRST FLOOR ELECTRICAL



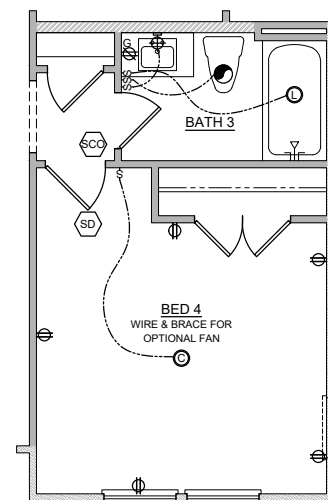
1 - PRIMARY BATH - DELUXE 2



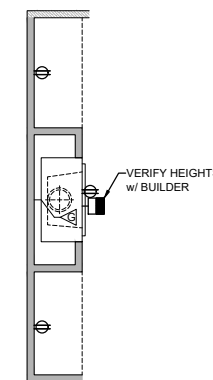
3 - GOURMET KITCHEN

FIRST FLOOR ELECTRICAL





















































SCALE: 1/8" = 1'-0"



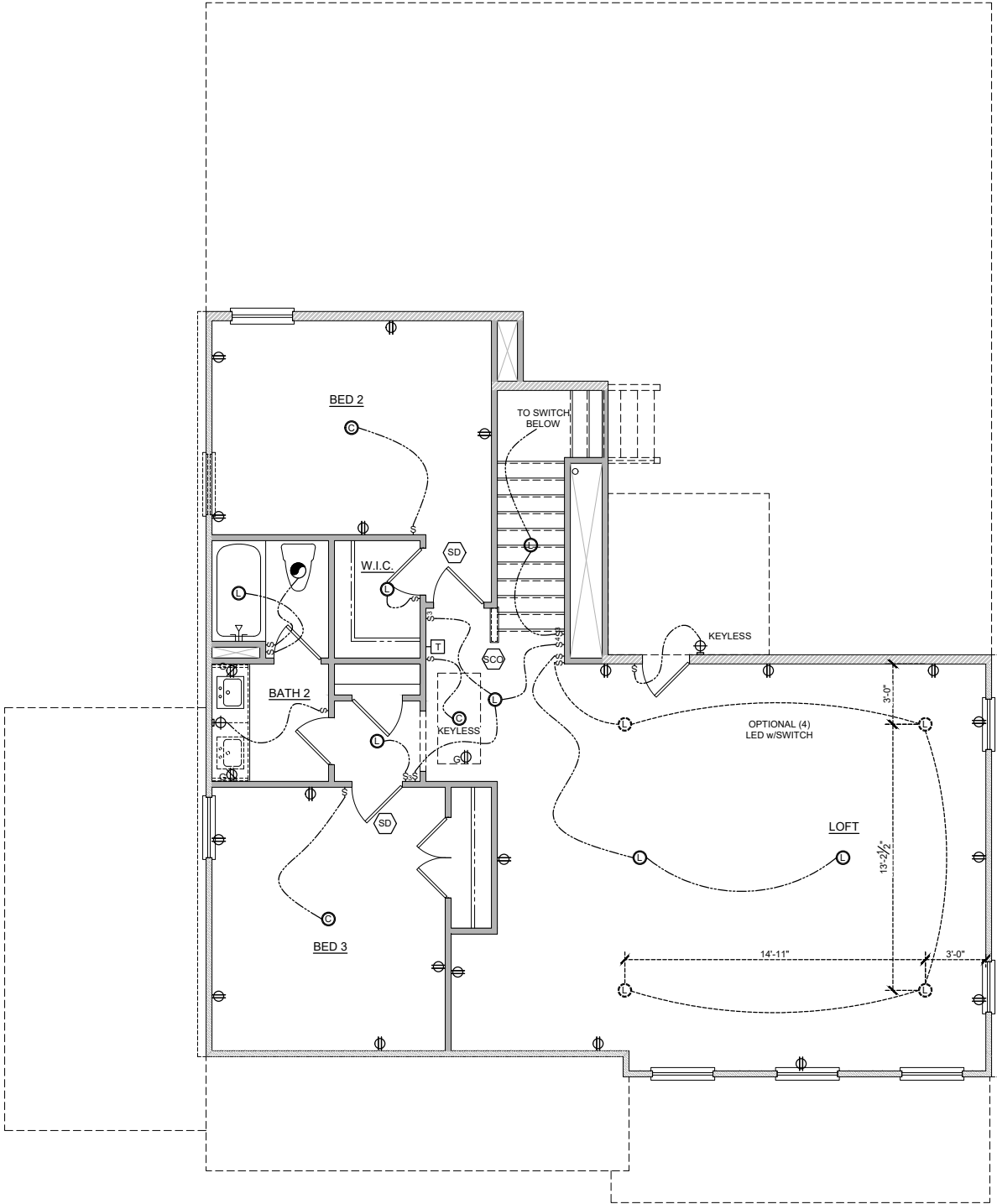
4 - GUEST SUITE ILO STUDY
FIRST FLOOR ELECTRICAL SCALE: 1/8" = 1'-0"



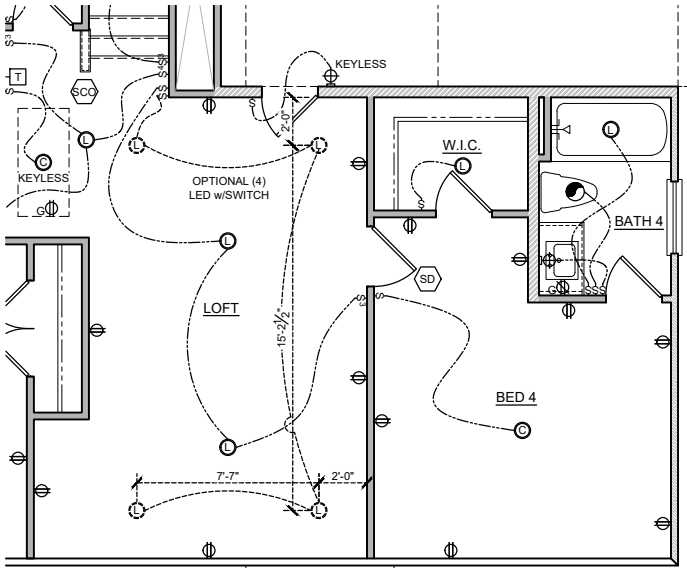
5 - GAS FIREPLACE

ELECTRICAL SCHEDULE			
SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
SWITCHES			
	SWITCH		SWITCH - 3 WAY
	SWITCH - 4 WAY		
OUTLETS			
	DEDICATED		COUNTER
	COUNTER USB		COUNTER U / GF
	COUNTER GFI		220 V
	110 V		110 V USB
	110 V GFI		110 V GFI / WP
	CEILING		FLOOR
LIGHTING			
	LED		CEILING MOUNT
	HANGING		RECESSED CAN
	FLOOD LIGHT		OPT. FLOOD LIGHT
	WALL MOUNT		
	FLUORESCENT		OPT. FLUORESCENT
	CEILING FAN W/ LIGHT		UNDER CABINET
			OPT. UNDER CABINET
UTILITIES			
	GARAGE DOOR OPENER		DOOR BELL
	THERMOSTAT		MEDIA
	DOOR BELL CHIME		OPTIONAL MEDIA
	LOW VOLTAGE PANEL		EXHAUST FAN
	SMOKE DETECTOR		SECURITY PANEL
	SMOKE & CO2 DETECTOR		CEILING SPEAKER
	STANDARD WATER HEATER		OPTIONAL STANDARD WATER HEATER
	TANKLESS WATER HEATER		OPT. TANKLESS WATER HEATER
	SUMP PUMP		OPTIONAL SUMP PUMP
	POT FILLER		OPTIONAL POT FILLER
	EXTERIOR HOSE BIBB		
	GAS STUB		OPTIONAL GAS STUB
METERS & PANELS			
			
NOTES & ABBREVIATIONS			
WP	WATER PROOF		
GFI	GROUND FAULT INTERCEPTOR		
FRB	FAN RATED BOX		

Tobacco Road Lot 124

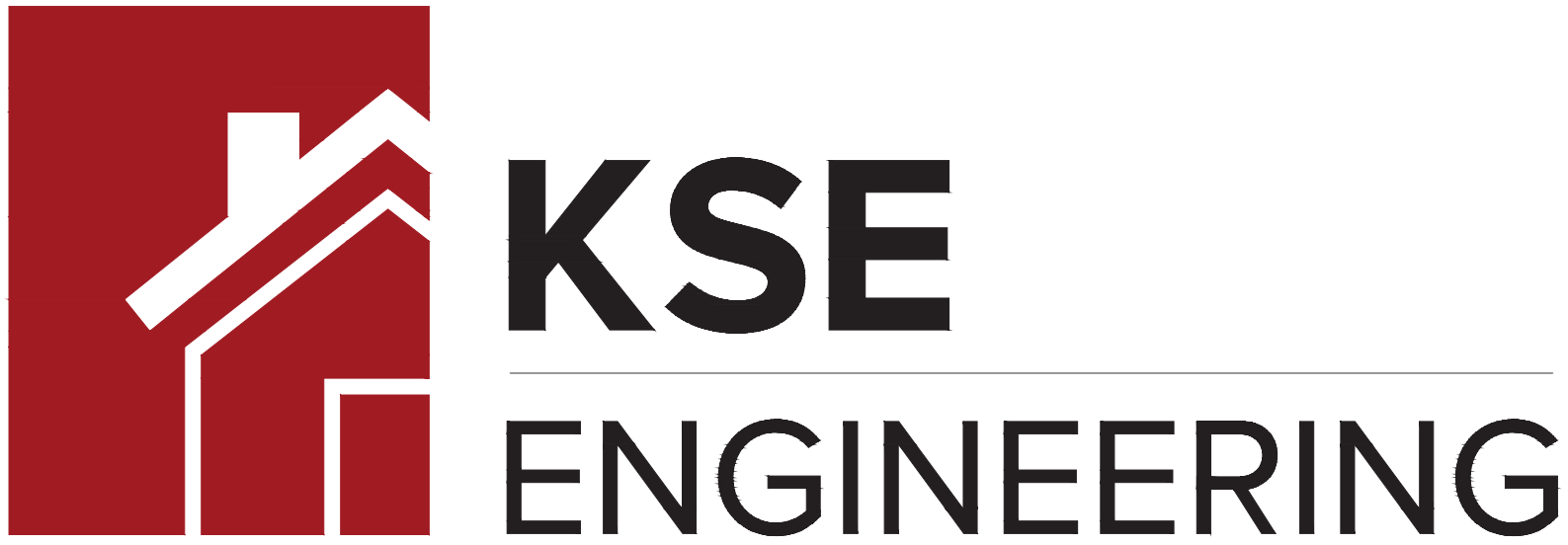


ELEVATION G - SECOND FLOOR ELECTRICAL
THIRD BAY GARAGE
SCALE: 1/8" = 1'-0"



8 - BEDROOM 4 w/ BATH 4
SECOND FLOOR ELECTRICAL
SCALE: 1/8" = 1'-0"

ELECTRICAL SCHEDULE			
SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
SWITCHES			
⌘	SWITCH	⌘3	SWITCH - 3 WAY
⌘4	SWITCH - 4 WAY		
OUTLETS			
⌘	DEDICATED	⌘	COUNTER
⌘U	COUNTER USB	⌘U/G	COUNTER U / GFI
⌘G	COUNTER GFI	⌘	220 V
⌘	110 V	⌘U	110 V USB
⌘G	110 V GFI	⌘G/WP	110 V GFI / WP
⌘	CEILING	⌘	FLOOR
LIGHTING			
⌘	LED	⌘	CEILING MOUNT
⌘	HANGING	⌘	RECESSED CAN
⌘	FLOOD LIGHT	⌘	OPT. FLOOD LIGHT
⌘	WALL MOUNT		
⌘	FLUORESCENT	⌘	OPT. FLUORESCENT
⌘	CEILING FAN W/ LIGHT	⌘	UNDER CABINET
		⌘	OPT. UNDER CABINET
UTILITIES			
⌘	GARAGE DOOR OPENER	⌘	DOOR BELL
⌘	THERMOSTAT	⌘	MEDIA
⌘	DOOR BELL CHIME	⌘	OPTIONAL MEDIA
⌘	LOW VOLTAGE PANEL	⌘	EXHAUST FAN
⌘	SMOKE DETECTOR	⌘	SECURITY PANEL
⌘	SMOKE & CO2 DETECTOR	⌘	CEILING SPEAKER
⌘	STANDARD WATER HEATER	⌘	OPTIONAL STANDARD WATER HEATER
⌘	TANKLESS WATER HEATER	⌘	OPT. TANKLESS WATER HEATER
⌘	SUMP PUMP	⌘	OPTIONAL SUMP PUMP
⌘	POT FILLER	⌘	OPTIONAL POT FILLER
⌘	EXTERIOR HOSE BIBB		
⌘	GAS STUB	⌘	OPTIONAL GAS STUB
METERS & PANELS			
⌘	ELECTRICAL METER	⌘	ELECTRICAL PANEL
NOTES & ABBREVIATIONS			
WP	WATER PROOF		
GFI	GROUND FAULT INTERCEPTOR		
FRB	FAN RATED BOX		



1900 AM DRIVE, SUITE 201, QUAKERTOWN, PA 18951
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Ashport RLH Model LH RALEIGH, NORTH CAROLINA

THESE DRAWINGS ARE TO BE USED IN CONJUNCTION WITH AND COORDINATED WITH THE ARCHITECTURAL, CIVIL, MECHANICAL, ELECTRICAL, AND PLUMBING DRAWINGS. THIS COORDINATION IS NOT THE RESPONSIBILITY OF THE STRUCTURAL ENGINEER OF RECORD (SER). SHOULD ANY DISCREPANCIES BECOME APPARENT, THE CONTRACTOR SHALL NOTIFY KSE ENGINEERING, P.C. BEFORE CONSTRUCTION BEGINS. IT IS THE INTENT OF THE ENGINEER LISTED ON THESE DOCUMENTS THAT THESE DOCUMENTS BE ACCURATE, PROVIDING LICENSED PROFESSIONALS CLEAR INFORMATION. EVERY ATTEMPT HAS BEEN MADE TO PREVENT ERROR. THE BUILDER AND ALL SUBCONTRACTORS ARE REQUIRED TO REVIEW ALL OF THE INFORMATION CONTAINED IN THESE DOCUMENTS PRIOR TO THE COMMENCEMENT OF ANY WORK. THE ENGINEER IS NOT RESPONSIBLE FOR ANY PLAN ERRORS, OMISSIONS, OR MISINTERPRETATIONS UNDETECTED AND NOT REPORTED TO THE ENGINEER PRIOR TO CONSTRUCTION. ALL CONSTRUCTION MUST BE IN ACCORDANCE TO THE INFORMATION FOUND IN THESE DOCUMENTS.

DESIGN SPECIFICATIONS:

DESIGN BUILDING CODE (REFERRED TO HEREIN AS 'THE BUILDING CODE'):

- 2024 NORTH CAROLINA RESIDENTIAL CODE

DESIGN LIVE LOADS:

- ROOF = 20 PSF (LOAD DURATION FACTOR=1.25)
- UNINHABITABLE ATTICS WITH LIMITED STORAGE = 20 PSF (WHERE SPECIFIED ON PLANS)
- HABITABLE ATTICS AND ATTICS SERVED WITH FIXED STAIRS = 30 PSF
- FLOOR = 40 PSF
- FLOOR (SLEEPING AREAS) = 30 PSF
- DECK = 40 PSF
- BALCONY = 40 PSF
- STAIRS = 40 PSF

DESIGN DEAD LOADS:

- ROOF TRUSS = 17 PSF (TC=7, BC=10)
- FLOOR TRUSS = 15 PSF (TC=10, BC=5)
- FLOOR JOIST = 10 PSF
- QUEEN ANNE BRICK = 25 PSF

NOTE: STRUCTURAL FRAMING HAS NOT BEEN DESIGNED FOR TILE, GRANITE, MARBLE OR OTHER MATERIALS HEAVIER THAN THE ABOVE LOADING UNLESS SPECIFICALLY NOTED ON PLANS.

DESIGN WIND LOADS:

- ULTIMATE WIND SPEED = 120 MPH
- EXPOSURE CATEGORY = B

ASSUMED SOIL BEARING CAPACITY = 2000 PSF

ASSUMED LATERAL SOIL PRESSURE = 45 PCF

FROST DEPTH = 12"

SEISMIC DESIGN CATEGORY = B

ENGINEERED LUMBER SHALL HAVE THE FOLLOWING MINIMUM DESIGN VALUES:

- TJI 210 SERIES (SERIES AND SPACING PER PLANS)
- LSL: E=1,550,000 PSI, F_B=2,325 PSI, F_V=310 PSI, F_C=900 PSI
- LVL: E=2,000,000 PSI, F_B=2,600 PSI, F_V=285 PSI, F_C=750 PSI
- PSL: E=2,000,000 PSI, F_B=2,900 PSI, F_V=290 PSI, F_C=625 PSI

THIS PLAN HAS BEEN DESIGNED PER THE 2024 EDITION OF THE NC RESIDENTIAL CODE. WHERE FRAMING, FOUNDATION, OR OTHER STRUCTURAL ITEMS DO NOT COMPLY WITH THE PRESCRIPTIVE METHODS OF THE CODE, THOSE ITEMS HAVE BEEN DESIGNED IN ACCORDANCE WITH ACCEPTED ENGINEERING PRACTICE PER NCRC R301.1.3.

Tobacco Road Lot 124



Cover Sheet

Ashport RLH Model – LH
120 M.P.H.
Raleigh, North Carolina

Project #:	190-25012
Designed By:	AAM
Checked By:	KRK
Issue Date:	8/25/25
Re-Issue:	9/24/25
Scale:	

S-0



GENERAL STRUCTURAL NOTES:

1. THE DESIGN PROFESSIONAL WHOSE SEAL APPEARS ON THESE DRAWINGS IS THE STRUCTURAL ENGINEER OF RECORD (SER) FOR THIS PROJECT. THE SER BEARS THE RESPONSIBILITY OF THE PRIMARY STRUCTURAL ELEMENTS AND THE PERFORMANCE OF THIS STRUCTURE. NO OTHER PARTY MAY REVISE, ALTER, OR DELETE ANY STRUCTURAL ASPECTS OF THESE CONSTRUCTION DOCUMENTS WITHOUT WRITTEN CONSENT OF KSE ENGINEERING, P.C. OR THE SER. FOR THE PURPOSES OF THESE CONSTRUCTION DOCUMENTS, THE SER AND KSE ENGINEERING SHALL BE CONSIDERED THE SAME ENTITY.
2. THE STRUCTURE IS ONLY STABLE IN ITS COMPLETED FORM. THE CONTRACTOR SHALL PROVIDE ALL REQUIRED TEMPORARY BRACING DURING CONSTRUCTION TO STABILIZE THE STRUCTURE.
3. THE SER IS NOT RESPONSIBLE FOR CONSTRUCTION SEQUENCES, METHODS, OR TECHNIQUES IN CONNECTION WITH THE CONSTRUCTION OF THIS STRUCTURE. THE SER WILL NOT BE HELD RESPONSIBLE FOR THE CONTRACTOR'S FAILURE TO CONFORM TO THE CONTRACT DOCUMENTS, SHOULD ANY NON-CONFORMITIES OCCUR.
4. THE SER DOES NOT CERTIFY DIMENSIONAL ACCURACY OR ARCHITECTURAL LAYOUT INCLUDING ROOF GEOMETRY. THE SER ASSUMES NO LIABILITY FOR CHANGES MADE TO THESE PLANS BY OTHERS, OR FOR CONSTRUCTION METHODS, OR FOR ANY DEVIATION FROM THE PLANS. THE SER SHALL BE NOTIFIED PRIOR TO CONSTRUCTION IF ANY DISCREPANCIES ARE NOTED ON THE PLANS.
5. ANY STRUCTURAL ELEMENTS OR DETAILS NOT FULLY DEVELOPED ON THE CONSTRUCTION DRAWINGS SHALL BE COMPLETED UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER. THESE SHOP DRAWINGS SHALL BE SUBMITTED TO KSE ENGINEERING FOR REVIEW BEFORE ANY CONSTRUCTION BEGINS. THE SHOP DRAWINGS WILL BE REVIEWED FOR OVERALL COMPLIANCE AS IT RELATES TO THE STRUCTURAL DESIGN OF THIS PROJECT. VERIFICATION OF THE SHOP DRAWINGS FOR DIMENSIONS, OR FOR ACTUAL FIELD CONDITIONS, IS NOT THE RESPONSIBILITY OF THE SER OR KSE ENGINEERING, P.C.
6. VERIFICATION OF ASSUMED FIELD CONDITIONS IS NOT THE RESPONSIBILITY OF THE SER. THE CONTRACTOR SHALL VERIFY THE FIELD CONDITIONS FOR ACCURACY AND REPORT ANY DISCREPANCIES TO KSE ENGINEERING, P.C. BEFORE CONSTRUCTION BEGINS.
7. THE SER IS NOT RESPONSIBLE FOR ANY SECONDARY STRUCTURAL ELEMENTS OR NON-STRUCTURAL ELEMENTS, EXCEPT FOR THE ELEMENTS SPECIFICALLY NOTED ON THE STRUCTURAL DRAWINGS.
8. THIS STRUCTURE AND ALL CONSTRUCTION SHALL CONFORM TO ALL APPLICABLE SECTIONS OF THE BUILDING CODE AND ANY LOCAL CODES OR RESTRICTIONS.
9. DO NOT SCALE DRAWINGS. WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS. ALL DIMENSIONS ARE TO FACE OF STUD OR TO FACE OF FRAMING UNLESS OTHERWISE NOTED.
10. PROVIDE MOISTURE PROTECTION AND FLASHING PER ARCHITECTURAL DETAILS.

FOUNDATIONS:

1. FOUNDATIONS SHALL BE CONSTRUCTED IN ACCORDANCE WITH CHAPTER 4 OF THE BUILDING CODE.
2. CONTRACTOR IS SOLELY RESPONSIBLE FOR VERIFYING THE SUITABILITY OF THE SITE SOIL CONDITIONS AT THE TIME OF CONSTRUCTION. THE BUILDER SHALL FURNISH ANY AND ALL REPORTS RECEIVED FROM THE GEOTECHNICAL ENGINEER ON THE STUDY OF THE PROPOSED SITE TO THE DESIGNER, STRUCTURAL ENGINEER, AND GENERAL CONTRACTOR.
3. MAXIMUM DEPTH OF UNBALANCED FILL AGAINST MASONRY WALLS TO BE AS SPECIFIED IN THE BUILDING CODE.
4. THE SER HAS NOT PERFORMED A SUBSURFACE INVESTIGATION. VERIFICATION OF THE ASSUMED VALUE IS THE RESPONSIBILITY OF THE OWNER OR THE CONTRACTOR. SHOULD ANY ADVERSE SOIL CONDITION BE ENCOUNTERED, THE SER MUST BE CONTACTED BEFORE PROCEEDING.
5. THE BOTTOM OF ALL FOOTINGS SHALL EXTEND BELOW THE FROST LINE FOR THE REGION IN WHICH THE STRUCTURE IS TO BE CONSTRUCTED, BUT NOT LESS THAN A MINIMUM OF 12" BELOW GRADE. ALL FOOTINGS TO HAVE A MINIMUM PROJECTION OF 2" ON EACH SIDE OF FOUNDATION WALLS. MAXIMUM FOOTING PROJECTION SHALL NOT EXCEED THE THICKNESS OF THE FOOTING.
6. WOOD SILL PLATES SHALL BE ANCHORED TO THE FOUNDATION WITH ½" ANCHOR BOLTS WITH MINIMUM 7" EMBEDMENT, SPACED A MAXIMUM OF 6'-0" O.C. INSTALL MINIMUM 2 ANCHOR BOLTS PER SECTION, 12" MAXIMUM FROM CORNERS. ½" DIAMETER x 8" LONG SIMPSON TITEN HD OR USP SCREW-BOLT+ SCREWS MAY BE SUBSTITUTED ON A 1 FOR 1 BASIS.
7. ANY FILL SHALL BE PLACED UNDER THE DIRECTION OR RECOMMENDATION OF A LICENSED PROFESSIONAL ENGINEER. THE RESULTING SOIL SHALL BE COMPACTED TO A MINIMUM OF 95% MAXIMUM DRY DENSITY.
8. EXCAVATIONS OF FOOTINGS SHALL BE LINED TEMPORARILY WITH A 6 MIL POLYETHYLENE MEMBRANE IF PLACEMENT OF CONCRETE DOES NOT OCCUR WITHIN 24 HOURS OF EXCAVATION.
9. NO CONCRETE SHALL BE PLACED AGAINST ANY SUBGRADE CONTAINING WATER, ICE, FROST, OR LOOSE MATERIAL.
10. PROVIDE FOUNDATION WATERPROOFING AND DRAIN WITH POSITIVE SLOPE TO OUTLET AS REQUIRED BY SITE CONDITIONS (SEE ARCHITECTURAL PLANS AND DETAILS).
11. NONE OF THE FOUNDATION DESIGNS IN THESE DOCUMENTS ARE SUITABLE FOR INSTALLATION IN SHRINK/SWELL CONDITIONS. REFER TO GEOTECHNICAL ENGINEER FOR APPROPRIATE DESIGN.
12. LOTS SHALL BE GRADED TO DRAIN SURFACE WATER AWAY FROM FOUNDATION WALLS. THE GRADE SHALL FALL A MINIMUM OF 6 INCHES WITHIN THE FIRST TEN FEET.
13. CRAWL SPACE TO BE GRADED LEVEL AND CLEAR OF ALL DEBRIS.
14. PROVIDE MINIMUM 6 MIL APPROVED VAPOR BARRIER. ALL JOINTS TO BE LAPPED MINIMUM 12" AND SEALED.

Tobacco Road Lot 124

CONCRETE & REINFORCING

1. CONCRETE DESIGN BASED ON ACI 318 AND ACI 318.1 OR ACI 332. CONCRETE SHALL HAVE A NORMAL WEIGHT AGGREGATE AND A MINIMUM COMPRESSIVE STRENGTH (f'c) = 3,000 PSI MINIMUM AT 28 DAYS PER CODE (VARIES W/ WEATHER), UNLESS OTHERWISE NOTED ON THE PLAN.
2. CONCRETE SHALL BE PROPORTIONED, MIXED, AND PLACED IN ACCORDANCE WITH THE LATEST EDITIONS OF ACI 318: "BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE" AND ACI 301: "SPECIFICATIONS FOR STRUCTURAL CONCRETE FOR BUILDINGS".
3. AIR ENTRAINED CONCRETE MUST BE USED FOR ALL STRUCTURAL ELEMENTS EXPOSED TO FREEZE/THAW CYCLES AND DEICING CHEMICALS. AIR ENTRAINMENT AMOUNTS (IN PERCENT) SHALL BE WITHIN -1% TO +2% OF 5% FOR FOOTINGS AND EXTERIOR SLABS.
4. NO ADMIXTURES SHALL BE ADDED TO ANY STRUCTURAL CONCRETE WITHOUT WRITTEN PERMISSION OF THE SER. WATER ADDED TO CONCRETE ON SITE SHALL NOT EXCEED THAT ALLOWED BY THE MIX DESIGN.
5. CONCRETE SLABS-ON-GRADE SHALL BE CONSTRUCTED IN ACCORDANCE WITH ACI 302.1R: "GUIDE FOR CONCRETE SLAB AND SLAB CONSTRUCTION".
6. CONTROL OR SAW CUT JOINTS (CUT OR TOOLED) SHALL BE SPACED IN INTERIOR SLABS-ON-GRADE AT A MAXIMUM OF 15'-0" O.C. AND IN EXTERIOR SLABS-ON-GRADE AT A MAXIMUM OF 10'-0" UNLESS OTHERWISE NOTED. CARE SHALL BE TAKEN TO AVOID RE-ENTRANT CORNERS.
7. CONTROL OR SAW CUT JOINTS SHALL BE PRODUCED USING CONVENTIONAL CUT OR TOOLED PROCESSES WITHIN 4 TO 12 HOURS AFTER THE SLAB HAS BEEN FINISHED.
8. ALL WELDED WIRE FABRIC (W.W.F.) FOR CONCRETE SLABS-ON-GRADE SHALL BE PLACED AT MID-DEPTH OF SLAB. THE W.W.F. SHALL BE SECURELY SUPPORTED DURING THE CONCRETE POUR. FIBROUS CONCRETE REINFORCEMENT, OR POLYPROPYLENE FIBERS MAY BE USED IN LIEU OF W.W.F. APPLICATION OF POLYPROPYLENE FIBERS PER CUBIC YARD OF CONCRETE SHALL BE PER MANUFACTURER AND COMPLY WITH ASTM C1116, ANY LOCAL BUILDING CODE REQUIREMENTS AND SHALL MEET OR EXCEED CURRENT INDUSTRY STANDARD.
9. POLYPROPYLENE REINFORCING TO BE 100% VIRGIN, CONTAINING NO REPROCESSED OLEFIN MATERIALS AND SPECIFICALLY MANUFACTURED FOR USE AS CONCRETE SECONDARY REINFORCEMENT.
10. STEEL REINFORCING BARS SHALL BE NEW BILLET STEEL CONFORMING TO ASTM A615, GRADE 60.
11. DETAILING, FABRICATION, AND PLACEMENT OF REINFORCING STEEL SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF ACI 315: "MANUAL OF STANDARD PRACTICE FOR DETAILING CONCRETE STRUCTURES".
12. HORIZONTAL FOOTING AND WALL REINFORCEMENT SHALL BE CONTINUOUS AND SHALL HAVE 90° BENDS, OR CORNER BARS WITH THE SAME SIZE/SPACING AS THE HORIZONTAL REINFORCEMENT.
13. PROVIDE REINFORCEMENT LAP AS NOTED BELOW, UNLESS NOTED OTHERWISE:
#4 BARS – 30" LENGTH
#5 BARS – 38" LENGTH
#6 BARS – 45" LENGTH
14. WHERE REINFORCING DOWELS ARE REQUIRED, THEY SHALL BE EQUIVALENT IN SIZE AND SPACING TO THE VERTICAL REINFORCEMENT. THE DOWEL SHALL EXTEND 48 BAR DIAMETERS VERTICALLY AND 20 BAR DIAMETERS INTO THE FOOTING. SEE KSE FOUNDATION DETAILS.
15. WHERE FOOTING BOTTOMS ARE TO BE STEPPED AT SLOPING GRADE CONDITIONS, PROVIDE CONTINUOUS REINFORCING WITH Z BARS (TO MATCH FOOTING REINFORCING) AS REQUIRED.
16. BAR SUPPORT ACCESSORIES SHALL BE PROVIDED IN ACCORDANCE WITH THE LATEST ACI MANUAL OF STANDARD PRACTICE FOR DETAILING REINFORCED CONCRETE STRUCTURES, EXCEPT THAT REINFORCING SHALL BE CHAIRED ON THE BOTTOM AND/OR THE SIDES ON BOLSTERS SPACED NOT MORE THAN 4 FEET ON CENTER. NO ROCKS, CMU, CLAY TILE, OR BRICK SHALL BE USED TO SUPPORT REINFORCING.
17. FOR GRADE SUPPORTED SLABS, SLAB REINFORCING SHALL BE HELD IN PLACE BY BAR SUPPORTS AND ACCESSORIES AS DESCRIBED IN THE CRSI MANUAL OF STANDARD PRACTICE. BAR SUPPORTS SHALL BE SPACED A MAXIMUM OF 4'-0" O.C. BOTH WAYS IN STRAIGHT LINES ON THE MESH GRID.

MASONRY

1. ALL MASONRY SHALL CONFORM TO ASTM C-90, F'm=1500 PSI. ALL BRICK SHALL CONFORM TO ASTM C-216, F'm=1500 PSI. ALL MORTAR SHALL BE TYPE 'S' (TYPE 'M' BELOW GRADE) AND CONFORM TO ASTM C-270. COARSE GROUT SHALL CONFORM TO ASTM C-476 WITH A MAXIMUM AGGREGATE SIZE OF ¾" AND A MINIMUM COMPRESSIVE STRENGTH OF 2,000 PSI.
2. ALL MASONRY WORK SHALL BE IN ACCORDANCE WITH "BUILDING CODE REQUIREMENTS FOR MASONRY STRUCTURES" ACI 530/ASCE 5/TMS 402 AND "SPECIFICATIONS FOR MASONRY STRUCTURES" ACI 530.1/ ASCE 6/TMS 602.
3. THE UNSUPPORTED HEIGHT OF SOLID MASONRY PIERS SHALL NOT EXCEED TEN TIMES THEIR LEAST DIMENSION. UNFILLED HOLLOW PIERS MAY BE USED IF THE UNSUPPORTED HEIGHT IS NOT MORE THAN FOUR TIMES THEIR LEAST DIMENSION.
4. EACH CRAWL SPACE PIER SHALL BEAR IN THE MIDDLE THIRD OF ITS RESPECTIVE FOOTING AND EACH GIRDER SHALL BEAR IN THE MIDDLE THIRD OF THE PIERS. PILASTERS TO BE BONDED TO PERIMETER FOUNDATION WALL.
5. TOP COURSE OF MASONRY SHALL BE GROUTED SOLID.
6. HORIZONTAL WALL JOINT REINFORCEMENT SHALL BE STANDARD 9 GAGE GALVANIZED LADDER OR TRUSS TYPE SPACED AT 16" O.C., UNLESS SHOWN OTHERWISE ON THE DRAWINGS.
7. SPLICED WIRE REINFORCEMENT SHALL BE LAPPED AT LEAST 6" AND CONTAIN AT LEAST ONE CROSS WIRE OF EACH PIECE OF REINFORCEMENT WITHIN THE 6". LAP WITH STANDARD 'I' AND 'L' SHAPED PIECES AT INTERSECTIONS AND CORNERS.

WOOD FRAMING:

1. SOLID SAWN WOOD FRAMING MEMBERS SHALL CONFORM TO THE SPECIFICATIONS LISTED IN THE LATEST EDITION OF THE "NATIONAL DESIGN SPECIFICATION FOR WOOD CONSTRUCTION": (NDS). UNLESS OTHERWISE NOTED, ALL WOOD FRAMING MEMBERS ARE DESIGNED TO BE: SPRUCE-PINE-FIR (SPF) WITH THE FOLLOWING MINIMUM DESIGN VALUES:
E=1,400,000 PSI, F_b=875 PSI, F_v=135 PSI
1.1. FRAMING: SPF #2.
1.2. PLATES: SPF #2.
1.3. STUDS: SPF STUD GRADE.
2. WALL STUD SPACING, (MAXIMUM 10' NOMINAL PLATE HEIGHT):
1 & 2 STORY EXTERIOR AND INTERIOR BEARING:
2x4 @ 16" O.C. OR 2x6 @ 24" O.C., U.N.O.
BOTTOM OF 3 STORIES EXTERIOR AND INTERIOR BEARING:
2x6 @ 16" O.C., U.N.O.
INTERIOR NON-BEARING:
2x @ 24" O.C., U.N.O.
3. ALL LUMBER EXPOSED TO WEATHER OR IN CONTACT WITH CONCRETE SHALL BE PRESERVATIVE TREATED SOUTHERN YELLOW PINE #2 OR BETTER.
4. ANCHOR SILL PLATES IN ACCORDANCE W/ GENERAL STRUCTURAL NOTES.
5. ALL BEAMS SPECIFIED ARE MINIMUM SIZES ONLY. LARGER MEMBERS MAY BE SUBSTITUTED AS NEEDED FOR EASE OF CONSTRUCTION.
6. NAILS SHALL BE COMMON WIRE NAILS UNLESS OTHERWISE NOTED.
7. BOLT HOLES AND LEAD HOLES FOR LAG SCREWS SHALL BE IN ACCORDANCE WITH NDS SPECIFICATIONS.
8. INDIVIDUAL STUDS FORMING A COLUMN SHALL BE ATTACHED WITH (2) ROWS 10d NAILS @ 6" O.C. STAGGERED. THE STUD COLUMN SHALL BE FULLY BLOCKED AT ALL FLOOR LEVELS TO ENSURE PROPER LOAD TRANSFER. WALL SHEATHING SHALL BE NAILED TO EDGE OF EACH STUD.
9. FACE NAIL ALL MULTI-PLY BEAMS AND HEADERS WITH (2) ROWS 16d COMMON NAILS @ 16" O.C., STAGGERED, OR PER MANUFACTURER'S SPECIFICATIONS FOR ENGINEERED LUMBER. APPLY NAILING FROM BOTH FACES FOR (3) OR MORE PLYS.
10. FASTEN 4-PLY BEAMS WITH (1) ½" DIAMETER THROUGH BOLT W/ NUTS AND WASHERS AT 12" O.C. STAGGERED TOP AND BOTTOM, ½" MINIMUM EDGE DISTANCE. (UNLESS OTHERWISE NOTED)
11. ALL BEAMS AND HEADERS SHALL HAVE (1)2x JACK STUD & (1)2x KING STUD UNLESS OTHERWISE NOTED. THE NUMBER OF STUDS INDICATED ON PLANS ARE THE TOTAL NUMBER OF JACK STUDS REQUIRED, UNLESS OTHERWISE NOTED.
12. PROVIDE KING STUDS AT EACH END OF HEADERS AS NOTED BELOW.
(1) STUD UP TO 6' OPENING
(2) STUDS UP TO 8' OPENING
(3) STUDS UP TO 9' OPENING
13. ALL BEAMS TO BE CONTINUOUSLY SUPPORTED Laterally AND SHALL BEAR FULL WIDTH ON THE SUPPORTING WALLS OR COLUMNS INDICATED WITH A MINIMUM OF TWO STUDS, UNLESS OTHERWISE NOTED. ALL BEAM SPLICES SHALL OCCUR OVER SUPPORTS.
14. SOLID BLOCKING TO BE PROVIDED AT ALL POINT LOADS THROUGH FLOOR LEVELS TO THE FOUNDATION OR TO OTHER STRUCTURAL COMPONENTS.
15. ALL LUMBER SPECIFIED ON DRAWINGS IS INTENDED FOR DRY USE ONLY (MOISTURE CONTENT <19%) UNLESS OTHERWISE NOTED.
16. ALL WATERPROOFING AND FIRE SAFETY SYSTEMS ARE THE RESPONSIBILITY OF THE CONTRACTOR AND ARE TO BE DESIGNED AND DETAILED BY OTHERS.
17. ANY WOOD FRAME INTERIOR BEARING WALL STUDS THAT HAVE HOLES IN THE CENTER OF THE STUD UP TO 1" DIAMETER SHALL HAVE STUD PROTECTION SHIELDS. ALL HOLES OVER 1" IN DIAMETER FOR PLUMBING LINES, ETC. SHALL BE REPAIRED WITH SIMPSON HSS2 OR USP STS1 STUD SHOES, TYPICAL, UNLESS OTHERWISE NOTED.
18. BEARING WALLS SHALL BE SHEATHED ON NOT LESS THAN ONE SIDE WITH OSB OR GYPSUM BOARD. BRIDGING SHALL BE INSTALLED NOT GREATER THAN 4 FEET APART MEASURED VERTICALLY FROM EITHER END OF THE STUD IN LIEU OF SHEATHING.
19. DIAGONAL BRACING SHALL BE INSTALLED AT EACH END OF BASEMENT BEARING WALLS AND NOT MORE THAN 20' ON CENTER.

EXTERIOR WOOD FRAMED DECKS:

1. DECKS ARE TO BE FRAMED IN ACCORDANCE WITH APPLICABLE BUILDING CODES AND AS REFERENCED ON THE STRUCTURAL PLANS, EITHER THROUGH CODE REFERENCES OR CONSTRUCTION DETAILS.
2. PRESERVATIVE TREATED WOOD FRAMING TO BE SOUTHERN YELLOW PINE #2 OR BETTER.
3. GUARD RAILS REQUIRED AT DECKS. DESIGN BY OTHERS TO MEET MINIMUM CODE REQUIREMENTS.
4. PROVIDE DECK LATERAL LOAD AND BRACING CONNECTIONS PER BUILDING CODE.

RAFTER FRAMED ROOF CONSTRUCTION:

1. PROVIDE 2x4x4'-0" RAFTER TIES AT 48" O.C.
2. RAFTERS SHALL BE SUPPORTED BY PURLINS AND PURLIN BRACES AS SHOWN ON THE PLAN. PURLIN BRACES SHALL NOT BEAR ON ANY CEILING JOIST, STRONGBACK OR HEADER UNLESS SPECIFICALLY SHOWN ON PLAN. RAFTERS MAY BE SPLICED AT PURLIN LOCATIONS.
3. CEILING JOISTS SHALL HAVE LATERAL SUPPORT W/ 1x4 FLAT BRACING ON TOP EDGE OF JOIST AT LOOSE JOIST ENDS (WHERE JOISTS NOT FASTENED TO RAFTERS) OR FULL DEPTH BLOCKING. FASTEN END OF BRACING TO RAFTER OR CABLE END FRAMING.
4. FASTEN RAFTER AND CEILING JOIST WITH (6) 12d NAILS UNLESS OTHERWISE NOTED.
5. PROVIDE VERTICAL 2x6 STRONGBACKS AT CEILING JOISTS @ 8'-0" O.C. TIE STRONGBACK ENDS TO CABLE STUDS OR RAFTERS WHERE POSSIBLE. PROVIDE BLOCKING BETWEEN TOP PLATES AND STRONGBACKS. PROVIDE 2x4 FLAT FASTENED TO EACH JOIST WITH (2) 12d NAILS. FASTEN STRONGBACK TO 2x4 FLAT WITH 12d NAILS @ 12" O.C. AND FASTENED TO EACH JOIST WITH (1) 12d TOENAIL.

WOOD TRUSSES (FLOOR & ROOF):

1. THE WOOD TRUSS MANUFACTURER/FABRICATOR IS RESPONSIBLE FOR THE DESIGN OF THE WOOD TRUSSES. SUBMIT SEALED SHOP DRAWINGS AND SUPPORTING CALCULATIONS TO THE SER FOR REVIEW PRIOR TO FABRICATION. THE SER SHALL HAVE A MINIMUM OF (5) DAYS FOR REVIEW. THE REVIEW BY THE SER SHALL BE FOR OVERALL COMPLIANCE OF THE DESIGN DOCUMENTS. THE SER SHALL ASSUME NO RESPONSIBILITY FOR THE ACCURACY OF THE STRUCTURAL DESIGN FOR THE WOOD TRUSSES.
2. THE WOOD TRUSSES SHALL BE DESIGNED FOR ALL REQUIRED LOADINGS AS SPECIFIED IN THE LOCAL BUILDING CODE, THE ASCE STANDARD "MINIMUM DESIGN LOADS FOR BUILDINGS AND OTHER STRUCTURES." (ASCE 7), AND THE LOADING REQUIREMENTS SHOWN ON THESE SPECIFICATIONS. THE TRUSS DRAWINGS SHALL BE COORDINATED WITH ALL OTHER CONSTRUCTION DOCUMENTS AND PROVISIONS PROVIDED FOR LOADS SHOWN ON THESE DRAWINGS INCLUDING BUT NOT LIMITED TO HVAC EQUIPMENT, PIPING, AND ARCHITECTURAL FIXTURES ATTACHED TO THE TRUSSES.
3. THE TRUSSES SHALL BE DESIGNED, FABRICATED, AND ERECTED IN ACCORDANCE WITH THE LATEST EDITION OF THE ANSI/TPI 1: "NATIONAL DESIGN STANDARD FOR METAL PLATE CONNECTED WOOD TRUSS CONSTRUCTION".
4. THE TRUSS MANUFACTURER SHALL PROVIDE ADEQUATE BRACING INFORMATION IN ACCORDANCE WITH "BUILDING COMPONENT SAFETY INFORMATION GUIDE TO GOOD PRACTICE FOR HANDLING, INSTALLING, RESTRAINING & BRACING OF METAL PLATE CONNECTED WOOD TRUSSES" (BCSI). THIS BRACING, BOTH TEMPORARY AND PERMANENT, SHALL BE SHOWN ON THE SHOP DRAWINGS. ALSO, THE SHOP DRAWINGS SHALL SHOW THE REQUIRED ATTACHMENTS FOR THE TRUSSES.
5. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLING TEMPORARY BRACING AND SHORING FOR THE FLOOR AND ROOF TRUSSES AS REQUIRED DURING CONSTRUCTION. AT A MINIMUM, CONTRACTOR SHALL FOLLOW THE REQUIREMENTS OF THE LATEST BCSI. THE CONTRACTOR SHALL KEEP A COPY OF THE BCSI SUMMARY SHEETS ON SITE.
6. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLING ALL PERMANENT TRUSS BRACING SHOWN IN THE STRUCTURAL DRAWINGS AND IN THE TRUSS DESIGNS. ALL CONTINUOUS LATERAL BRACING OF WEBS REQUIRES BRACES. REFER TO BCSI SUMMARY SHEET B3 FOR TYPES OF DIAGONAL BRACES TO PROVIDE AT EACH CONTINUOUS LATERAL BRACE LINE. SUCH DIAGONAL BRACES SHALL NOT BE SPACED MORE THAN 20 FEET O.C. DIAGONAL BRACES SHALL BE FASTENED TO EACH TRUSS WEB WITH A MINIMUM OF TWO 10d FACE NAILS. WHERE CONTINUOUS LATERAL BRACING CANNOT BE INSTALLED, DUE TO A MINIMUM OF THREE ADJACENT TRUSSES NOT BEING IDENTICAL, THE CONTRACTOR SHALL COORDINATE WITH THE TRUSS SPECIALTY ENGINEER/MANUFACTURER TO DETERMINE WHAT TYPE OF ALTERNATE BRACE (I.E., T OR L BRACE, ETC.) IS REQUIRED.
7. ANY CHORDS OR TRUSS WEBS SHOWN ON THESE DRAWINGS HAVE BEEN SHOWN AS A REFERENCE ONLY. THE FINAL DESIGN OF THE TRUSSES SHALL BE PER THE MANUFACTURER.
8. TRUSS LAYOUT AND PLACEMENT BY MANUFACTURER TO COINCIDE WITH THE SUPPORT LOCATIONS SHOWN ON THE SEALED STRUCTURAL DRAWINGS. TRUSS PROFILES TO BE SEALED BY THE TRUSS MANUFACTURER. TRUSS PLANS TO BE COORDINATED WITH THE SEALED STRUCTURAL DRAWINGS.
9. TRUSS MANUFACTURER TO PROVIDE REQUIRED UPLIFT CONNECTORS FOR ALL TRUSSES.
10. PROVIDE SIMPSON H2.5A, USP RT7 OR EQUIVALENT AT EACH TRUSS TO TOP PLATE CONNECTION, UNLESS OTHERWISE NOTED.

WOOD I-JOIST FLOOR FRAMING:

1. THE I-JOIST MANUFACTURER IS RESPONSIBLE FOR THE DESIGN OF THE FLOOR I-JOISTS. SUBMIT I-JOIST LAYOUTS TO THE SER FOR REVIEW PRIOR TO INSTALLATION. THE SER SHALL HAVE A MINIMUM OF (5) DAYS FOR REVIEW. THE REVIEW BY THE SER SHALL BE FOR OVERALL COMPLIANCE OF THE DESIGN DOCUMENTS. THE SER SHALL ASSUME NO RESPONSIBILITY FOR THE ACCURACY OF THE STRUCTURAL DESIGN OF THE I-JOISTS.
2. I-JOISTS SHALL BE DESIGNED FOR ALL REQUIRED LOADINGS AS SPECIFIED IN THE LOCAL BUILDING CODE, THE ASCE STANDARD "MINIMUM DESIGN LOADS FOR BUILDINGS AND OTHER STRUCTURES." (ASCE 7), AND THE LOADING REQUIREMENTS SHOWN ON THESE SPECIFICATIONS. I-JOIST DESIGNS SHALL BE COORDINATED WITH ALL OTHER CONSTRUCTION DOCUMENTS AND PROVISIONS PROVIDED FOR LOADS SHOWN ON THESE DRAWINGS INCLUDING BUT NOT LIMITED TO HVAC EQUIPMENT, PIPING, AND ARCHITECTURAL FIXTURES ATTACHED TO THE I-JOISTS.
3. I-JOISTS SHALL BE DESIGNED FOR L/480 MAXIMUM LIVE LOAD DEFLECTION.
4. I-JOISTS ARE TO BE SPACED PER TILE COUNCIL OF NORTH AMERICA (TCNA, INC) SPECIFICATIONS WHERE SUPPORTING TILE FLOORING.
5. THE I-JOIST SPACING SHOWN ON THE SEALED STRUCTURAL DRAWINGS IS TO BE THE MAXIMUM SPACING OF THE FLOOR I-JOISTS.
6. THE I-JOIST MANUFACTURER IS RESPONSIBLE TO PROVIDE ADDITIONAL I-JOISTS BENEATH DOOR JAMBS, PARALLEL WALLS, KITCHEN COUNTERS AND KITCHEN ISLANDS AS REQUIRED.
7. I-JOIST LAYOUT AND PLACEMENT BY MANUFACTURER IS TO BE COORDINATED WITH THE SUPPORT LOCATIONS SHOWN ON THE SEALED STRUCTURAL DRAWINGS.
8. THE I-JOIST MANUFACTURER IS TO SPECIFY ALL REQUIRED CONNECTORS FOR ALL I-JOIST CONNECTIONS, U.N.O.
9. THE I-JOIST MANUFACTURER IS TO PROVIDE ALL STANDARD I-JOIST INSTALLATION SPECIFICATIONS AND DETAILS REQUIRED.

MECHANICAL FASTENERS:

1. ALL METAL HARDWARE AND FASTENERS TO BE SIMPSON STRONG-TIE OR APPROVED EQUIVALENT.
2. ALL HARDWARE AND FASTENERS IN CONTACT WITH PRESERVATIVE PRESSURE TREATED LUMBER SHALL BE HOT DIPPED GALVANIZED IN ACCORDANCE WITH ASTM A 153, G-185.
3. MANY OF THE NEW PRESSURE TREATED WOODS USE CHEMICALS THAT ARE CORROSIVE TO STEEL. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE TYPE OF WOOD TREATMENT AND SELECT APPROPRIATE CONNECTORS THAT WILL RESIST THE APPLICABLE CORROSIVE CHEMICALS.

WOOD STRUCTURAL PANELS:

1. FABRICATION AND PLACEMENT OF STRUCTURAL WOOD SHEATHING SHALL BE IN ACCORDANCE WITH THE APA DESIGN/CONSTRUCTION GUIDE "RESIDENTIAL AND COMMERCIAL," AND ALL OTHER APPLICABLE APA STANDARDS.
2. ALL REQUIRED WOOD SHEATHING SHALL BEAR THE MARK OF THE APA.
3. WOOD WALL SHEATHING SHALL COMPLY WITH THE REQUIREMENTS OF LOCAL BUILDING CODES FOR THE APPROPRIATE STATE AS INDICATED ON THESE DRAWINGS. REFER TO WALL BRACING NOTES IN PLAN SET FOR MORE INFORMATION. EXTERIOR WALLS TO BE FULLY SHEATHED USING ¾" OSB OR PLYWOOD MINIMUM. AT BRACED WALL PANELS, PROVIDE BLOCKING AT ALL SHEET EDGES NOT FALLING ON STUDS OR PLATES.
4. ROOF SHEATHING SHALL BE APA RATED SHEATHING EXPOSURE 1 OR 2. ROOF SHEATHING SHALL BE CONTINUOUS OVER TWO SUPPORTS MINIMUM AND ATTACHED TO ITS SUPPORTING ROOF FRAMING WITH 8d NAILS AT 6" O.C. AT PANEL EDGES AND AT 6" O.C. IN PANEL FIELD UNLESS OTHERWISE NOTED ON THE PLANS. SHEATHING SHALL BE APPLIED WITH THE LONG DIRECTION PERPENDICULAR TO FRAMING. SHEATHING SHALL HAVE A SPAN RATING CONSISTENT WITH THE FRAMING SPACING. PROVIDE SUITABLE EDGE SUPPORT BY USE OF PLYWOOD CLIPS OR LUMBER BLOCKING UNLESS OTHERWISE NOTED. PANEL END JOINTS SHALL OCCUR OVER FRAMING. ROOF SHEATHING TO BE ¾" OSB MINIMUM.
5. WOOD FLOOR SHEATHING SHALL BE APA RATED SHEATHING EXPOSURE 1 OR 2. ATTACH SHEATHING TO ITS SUPPORTING FRAMING WITH (1) 10d NAIL AT 6" O.C. AT PANEL EDGES AND AT 12" O.C. IN PANEL FIELD UNLESS OTHERWISE NOTED ON THE PLANS. SHEATHING SHALL BE APPLIED PERPENDICULAR TO FRAMING. SHEATHING SHALL HAVE A SPAN RATING CONSISTENT WITH THE FRAMING SPACING. PROVIDE SUITABLE EDGE SUPPORT BY USE OF T&G PLYWOOD OR LUMBER BLOCKING UNLESS OTHERWISE NOTED. PANEL END JOINTS SHALL OCCUR OVER FRAMING.
6. SHEATHING SHALL HAVE A ½" GAP AT PANEL ENDS AND EDGES AS RECOMMENDED IN ACCORDANCE WITH THE APA.

STRUCTURAL FIBERBOARD PANELS:

1. STRUCTURAL FIBERBOARD SHEATHING SHALL ONLY BE USED WHERE SPECIFICALLY NOTED ON THE STRUCTURAL PLANS.
2. FABRICATION AND PLACEMENT OF STRUCTURAL FIBERBOARD SHEATHING SHALL BE IN ACCORDANCE WITH THE APPLICABLE AFA STANDARDS.
3. FIBERBOARD WALL SHEATHING SHALL COMPLY WITH THE REQUIREMENTS OF LOCAL BUILDING CODES FOR THE APPROPRIATE STATE AS INDICATED ON THESE DRAWINGS. REFER TO WALL BRACING NOTES IN PLAN SET FOR MORE INFORMATION.
4. SHEATHING SHALL HAVE A ½" GAP AT PANEL ENDS AND EDGES AS RECOMMENDED IN ACCORDANCE WITH THE AFA.

STRUCTURAL STEEL:

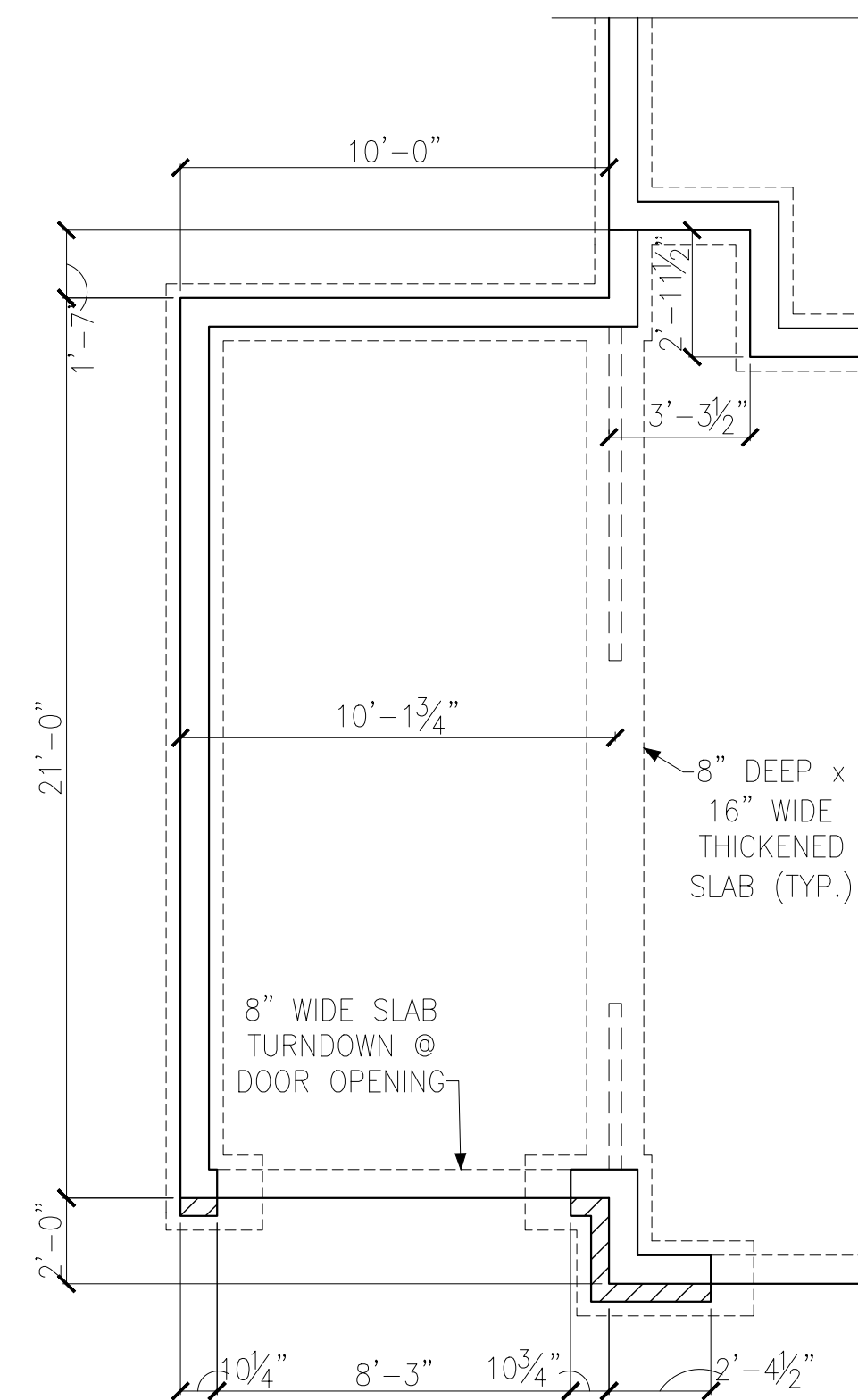
1. STRUCTURAL STEEL SHALL BE FABRICATED AND ERECTED IN ACCORDANCE WITH THE AMERICAN INSTITUTE OF STEEL CONSTRUCTION "CODE OF STANDARD PRACTICE FOR STEEL BUILDINGS AND BRIDGES" AND OF THE MANUAL OF STEEL CONSTRUCTION "LOAD RESISTANCE FACTOR DESIGN" LATEST EDITIONS.
2. ALL STEEL SHALL HAVE A MINIMUM YIELD STRESS (F_y) OF 50 KSI UNLESS OTHERWISE NOTED.
3. WELDING SHALL CONFORM TO THE LATEST EDITION OF THE AMERICAN WELDING SOCIETY'S STRUCTURAL WELDING CODE AWA D1.1. ELECTRODES FOR SHOP AND FIELDING WELDING SHALL BE CLASS E70XX. ALL WELDING SHALL BE PERFORMED BY A CERTIFIED WELDER PER THE ABOVE STANDARDS.
4. ALL STEEL BEAMS TO BE SUPPORTED AT EACH END WITH A MINIMUM BEARING LENGTH OF 3½" AND FULL FLANGE WIDTH UNLESS OTHERWISE NOTED. BEAMS MUST BE ATTACHED AT EACH END WITH A MINIMUM OF FOUR 16d NAILS OR (2) ½" x 4" LAG SCREWS UNLESS OTHERWISE NOTED.
5. INSTALL 2x WOOD PLATE ON TOP OF STEEL BEAMS, RIPPED TO MATCH BEAM WIDTH. FASTEN PLATE TO BEAM W/ HILTI X-DNI 52 P8 PINS AT 12" O.C. STAGGERED OR ½" DIAMETER BOLTS AT 24" O.C.

BRICK VENEER LINTEL SCHEDULE		
SPAN	LINTEL SIZE	END BEARING
UP TO 3'-0"	3½"x3½"x¼"	4"
UP TO 6'-3"	5"x3½"x½" L.L.V.	8"
UP TO 9'-6"	6"x3½"x½" L.L.V.	12"
LINTELS ARE NOT DESIGNED TO BE BOLTED TO HEADERS UNLESS SPECIFIED ON UNIT PLANS. SPANS OVER 4'-0" SHALL BE SHORED UP UNTIL CURED.		





**PROPANE
TANK**



Foundation plan showing concrete footings, block foundations, and slabs for a building and garage. The plan includes dimensions for overall size, footing spacing, and material specifications.

Overall Dimensions:

- Overall Width: 39'-0"
- Overall Depth: 69'-8"
- Overall Depth (excluding garage): 52'-4"

Foundation Details:

- 8" BLOCK FOUNDATION ON 8" DEEP x 16" WIDE CONCRETE FOOTING. 8" DEEP x 20" WIDE @ BRICK VENEER (TYP.)**
- 24"x24"x12" DEEP CONCRETE FOOTING (TYP.)**
- 30"x30"x12" DEEP CONCRETE FOOTING**
- 24"x24"x12" DEEP CONCRETE FOOTING (TYP. U.N.O.)**
- 8" BLOCK FOUNDATION ON 8" DEEP x 16" WIDE CONCRETE FOOTING.**
- 8" BLOCK FOUNDATION ON 8" DEEP x 20" WIDE CONCRETE FOOTING.**

Slab Details:

- GARAGE SLAB**
4" THICK CONCRETE SLAB W/ FIBERMESH ON 6 MIL VAPOR BARRIER ON 95% COMPACTED FILL. SLOPE 1/8" PER 1'-0" TOWARDS DOOR.
- 4" THICK CONCRETE SLAB W/ FIBERMESH ON 95% COMPACTED FILL**

Other Features:

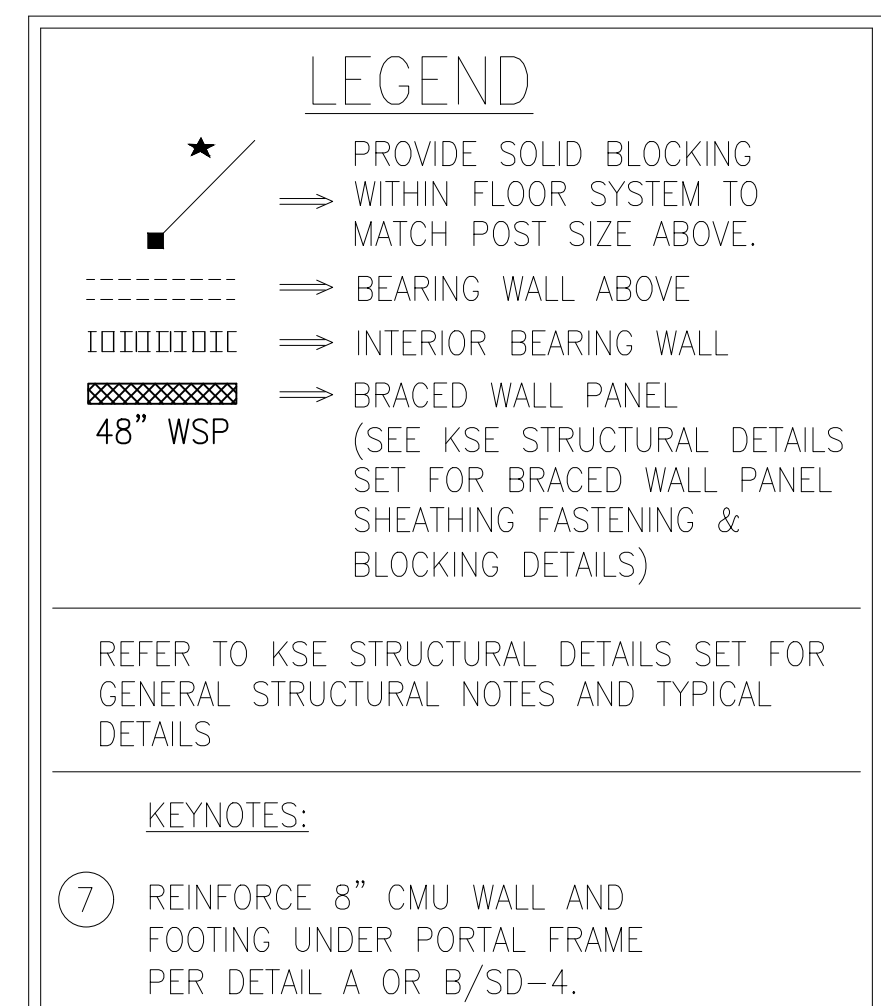
- BEAM POCKET, GROUT SOLID (TYP.)**
- 8" WIDE SLAB TURNDOWN @ DOOR OPENING**
- EXTEND BLOCK 16" BEYOND CORNER**

Dimensions and Spacing:

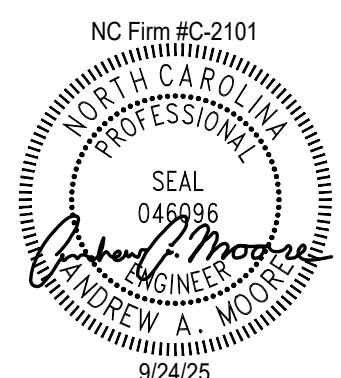
- Top Wall Footing Spacing: 6'-0", 12'-0", 6'-0"
- Left Wall Footing Spacing: 7'-2", 7'-2", 6'-2 1/4", 6'-2", 6'-2", 6'-2"
- Right Wall Footing Spacing: 6'-2", 6'-4", 6'-4", 6'-4"
- Bottom Wall Footing Spacing: 5'-6 1/4", 5'-6 1/4", 6'-1", 6'-1", 6'-3 1/4", 6'-3"
- Garage Slab Dimensions: 17'-5" (width), 24'-7" (depth)
- Garage Slab Slope: 1/8" PER 1'-0" TOWARDS DOOR
- Garage Slab Turndown: 8" WIDE

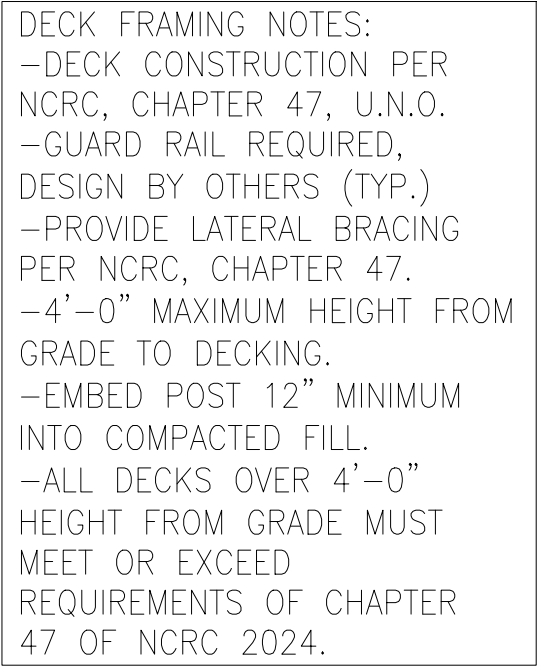
CRAWL SPACE FOUNDATION PLAN
ELEVATION 'G'

PIER ELEV. TO BE
9 1/4" BELOW TOP OF
WALL ELEV. (TYP.)

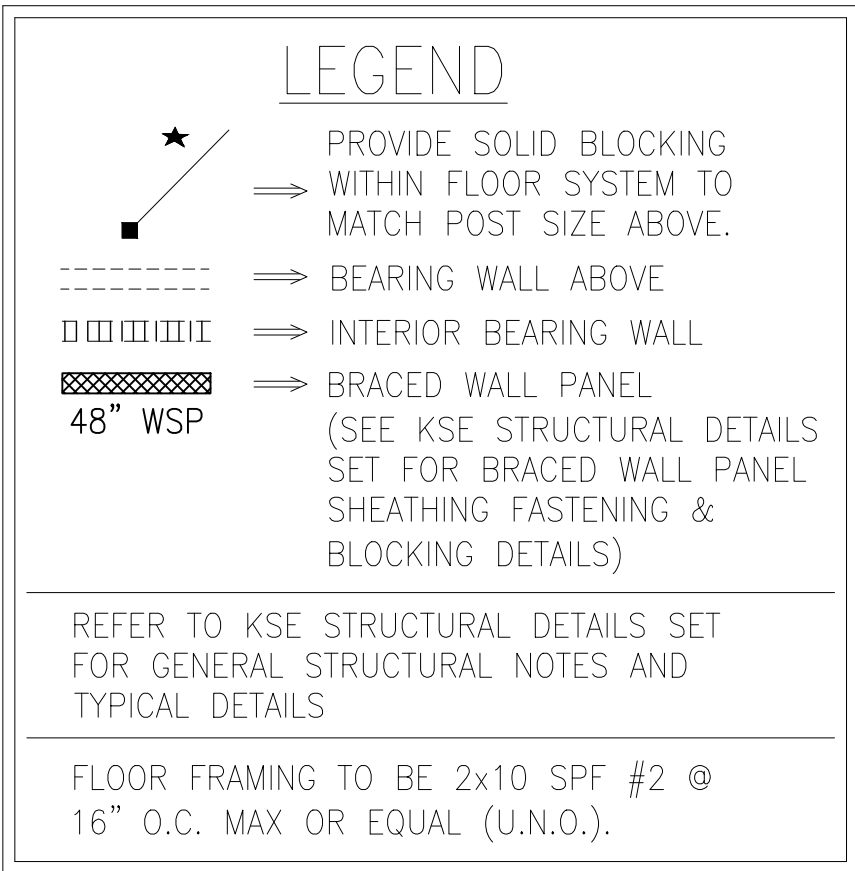


Tobacco Road Lot 124





NOTE:
BEAMS, HEADERS AND
FLOOR JOISTS MAY BE SYP
#2 GRADE LUMBER.

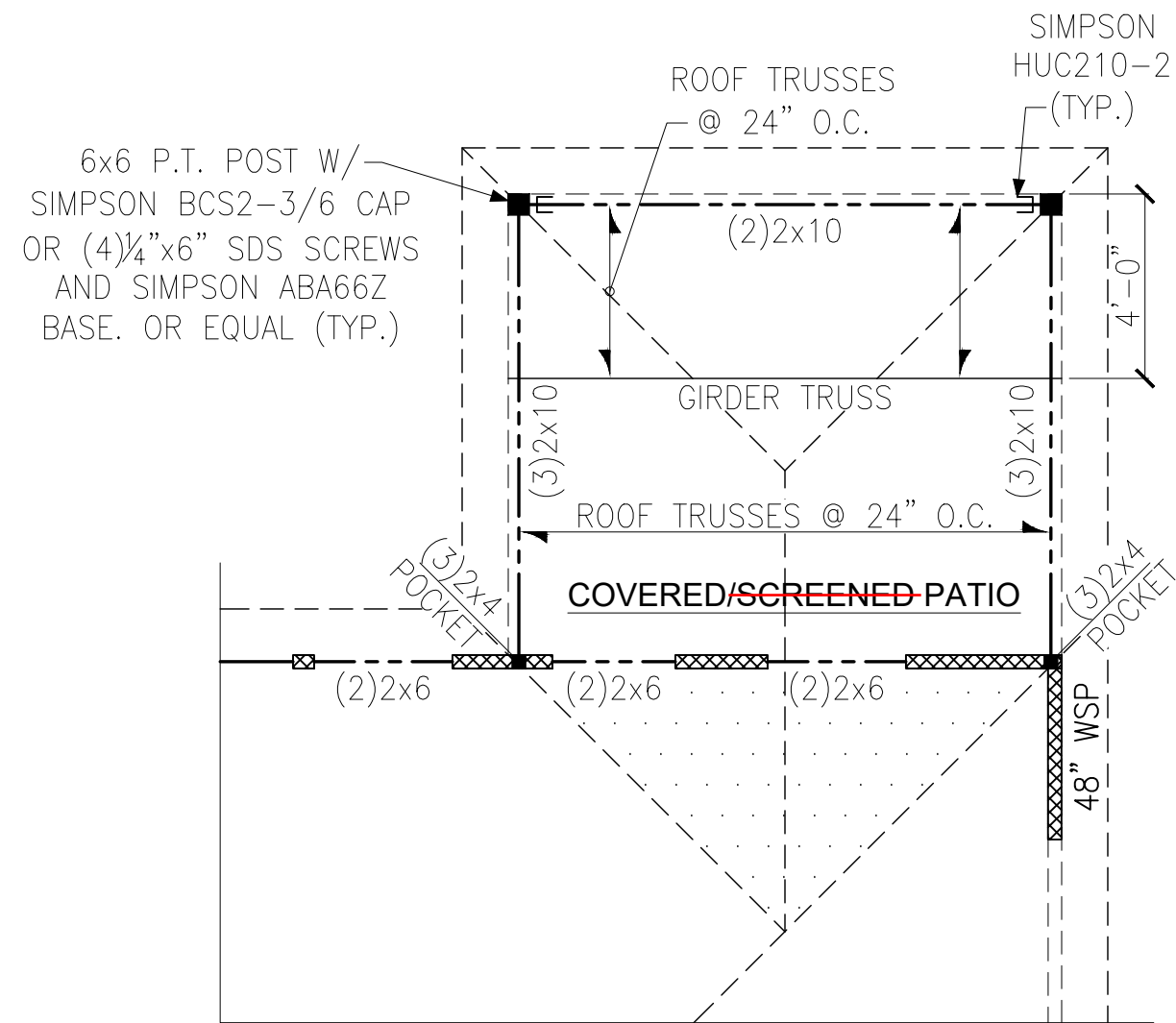


CRAWL SPACE FRAMING PLAN

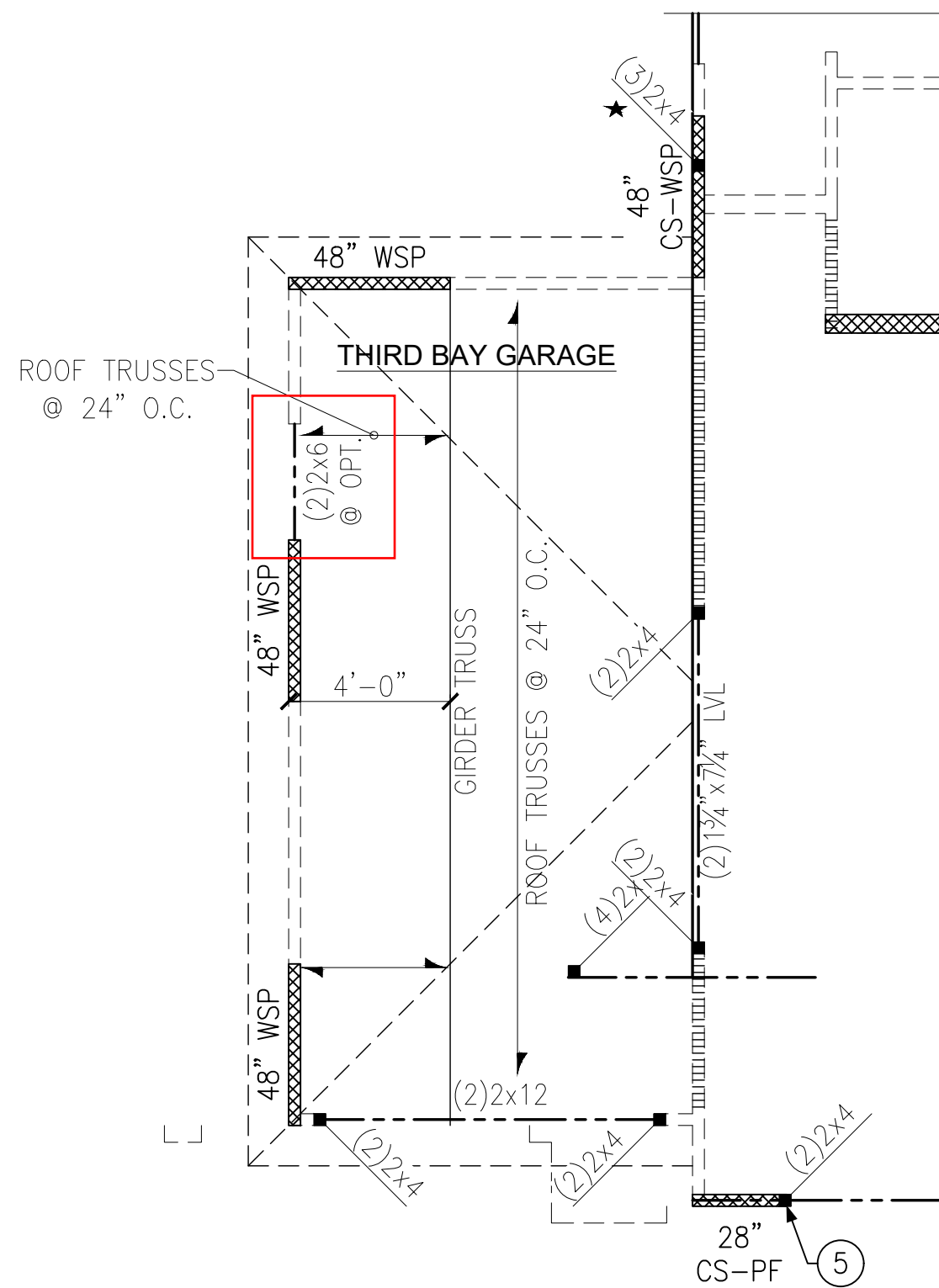
ELEVATION 'G'

Tobacco Road Lot 124

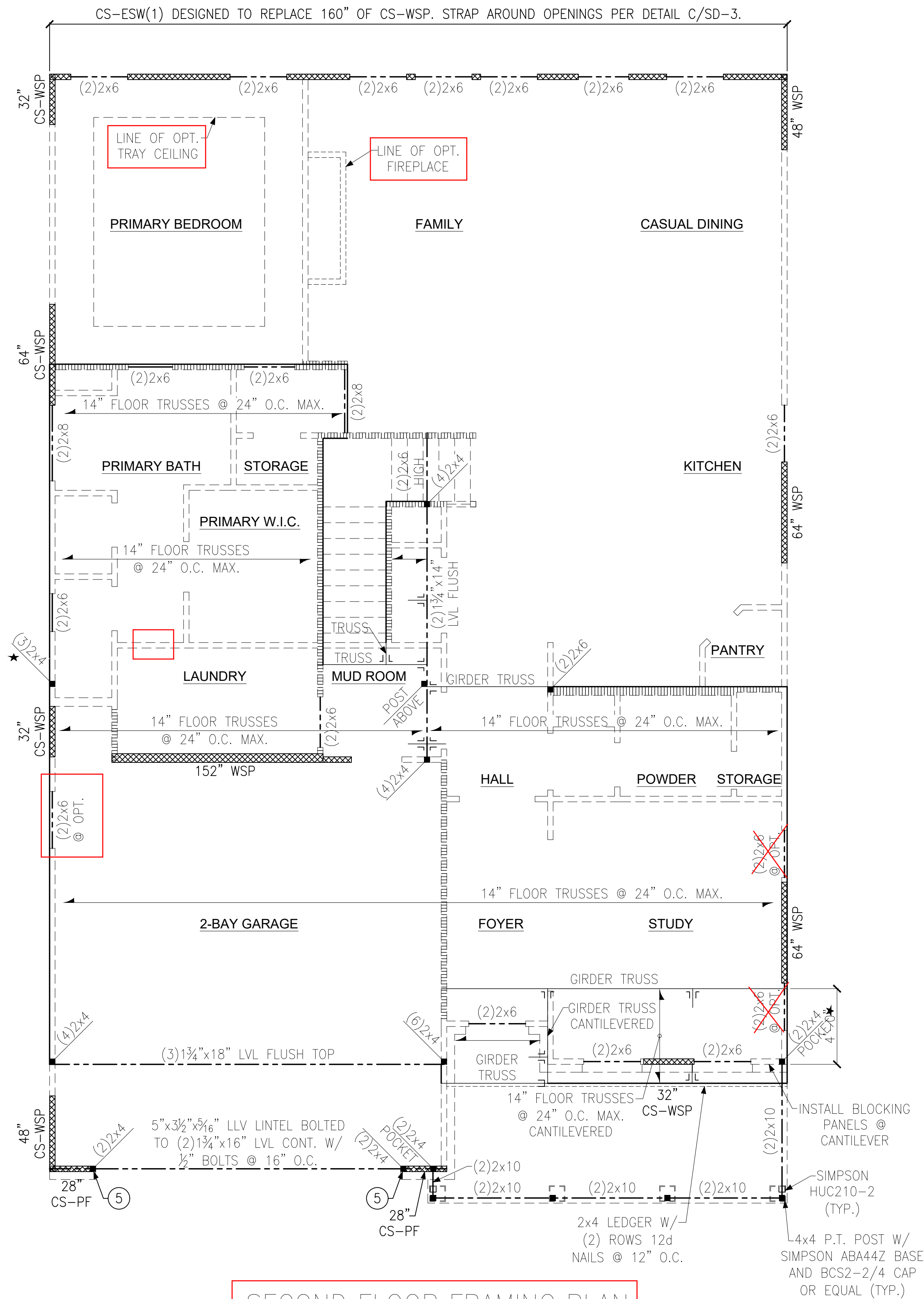




PARTIAL FRAMING PLAN
OPT. COVERED/SCREENED PATIO



PARTIAL FRAMING PLAN
OPT. THIRD BAY GARAGE



SECOND FLOOR FRAMING PLAN
ELEVATION 'G'

LEGEND

- ★ PROVIDE SOLID BLOCKING WITHIN FLOOR SYSTEM TO MATCH POST SIZE ABOVE.
- ===== BEARING WALL ABOVE
- ===== INTERIOR BEARING WALL
- ===== BRACED WALL PANEL (SEE KSE STRUCTURAL DETAILS SET FOR BRACED WALL PANEL SHEATHING FASTENING & BLOCKING DETAILS)
- NH NO HEADER REQUIRED

REFER TO KSE STRUCTURAL DETAILS SET FOR GENERAL STRUCTURAL NOTES AND TYPICAL DETAILS

PLAN DESIGNED WITH 9' WALL PLATES

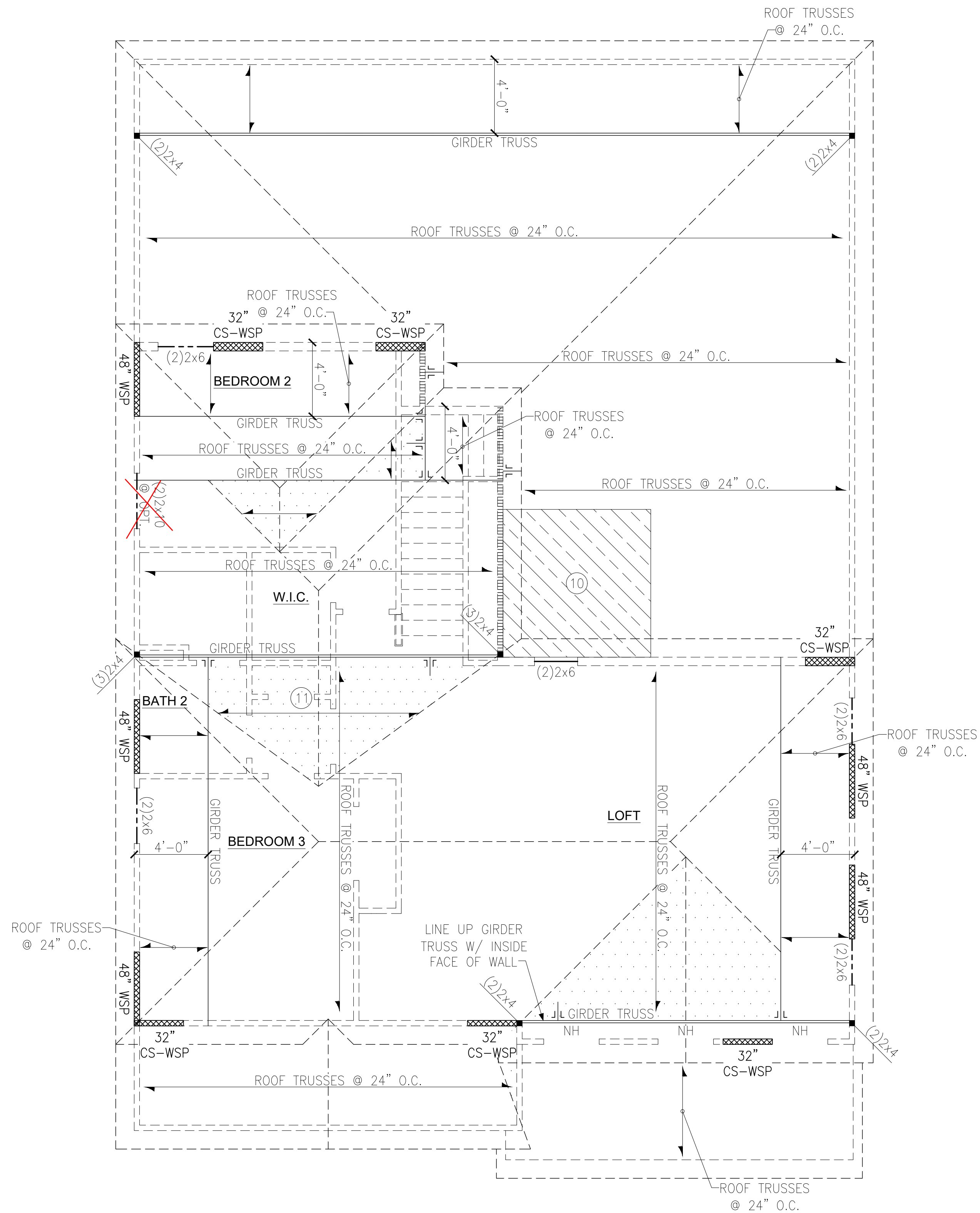
FLOOR FRAMING TO BE 14" DEEP OPEN WEB TRUSSES @ 24" O.C. MAXIMUM OR EQUAL (U.N.O.).

KEYNOTES:

- INSTALL ONE PANEL CS-PF PORTAL FRAME PER DETAIL A OR B/SD-4.
- INSTALL TWO PANEL CS-PF PORTAL FRAME PER DETAIL A OR B/SD-4.



Tobacco Road Lot 124



ROOF FRAMING PLAN
ELEVATION 'G'

LEGEND

★

⇒

PROVIDE SOLID BLOCKING WITHIN FLOOR SYSTEM TO MATCH POST SIZE ABOVE.

⇒

BEARING WALL ABOVE

|||||

⇒

INTERIOR BEARING WALL

|||||

48" WSP

⇒

BRACED WALL PANEL (SEE KSE STRUCTURAL DETAILS SET FOR BRACED WALL PANEL SHEATHING FASTENING & BLOCKING DETAILS)

NH

⇒

NO HEADER REQUIRED

REFER TO KSE STRUCTURAL DETAILS SET FOR GENERAL STRUCTURAL NOTES AND TYPICAL DETAILS

PLAN DESIGNED WITH 8' & 9' WALL PLATES

KEYNOTES:

10

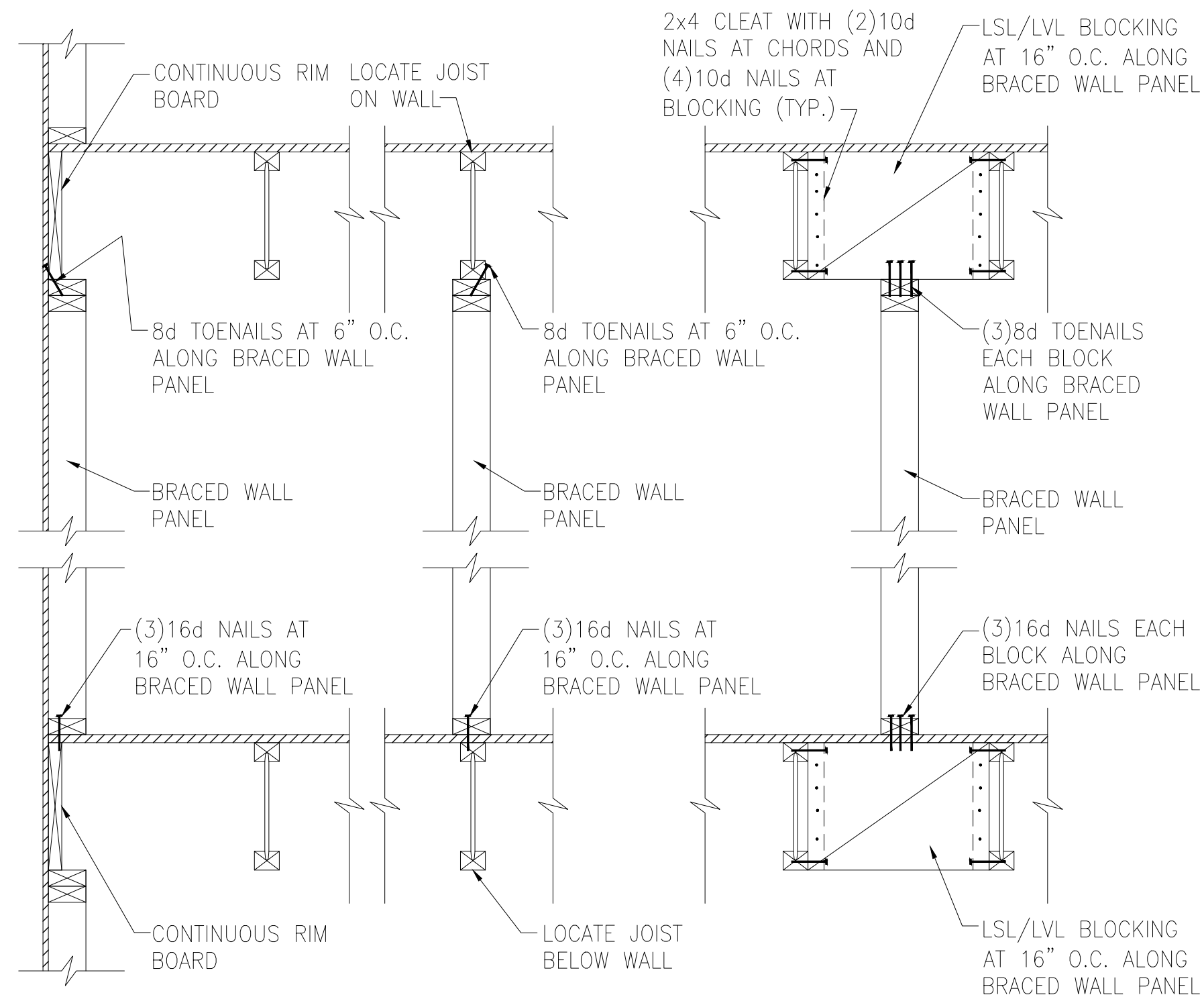
8'x8' HVAC PLATFORM TRUSSES DESIGNED TO SUPPORT HVAC UNITS.

11

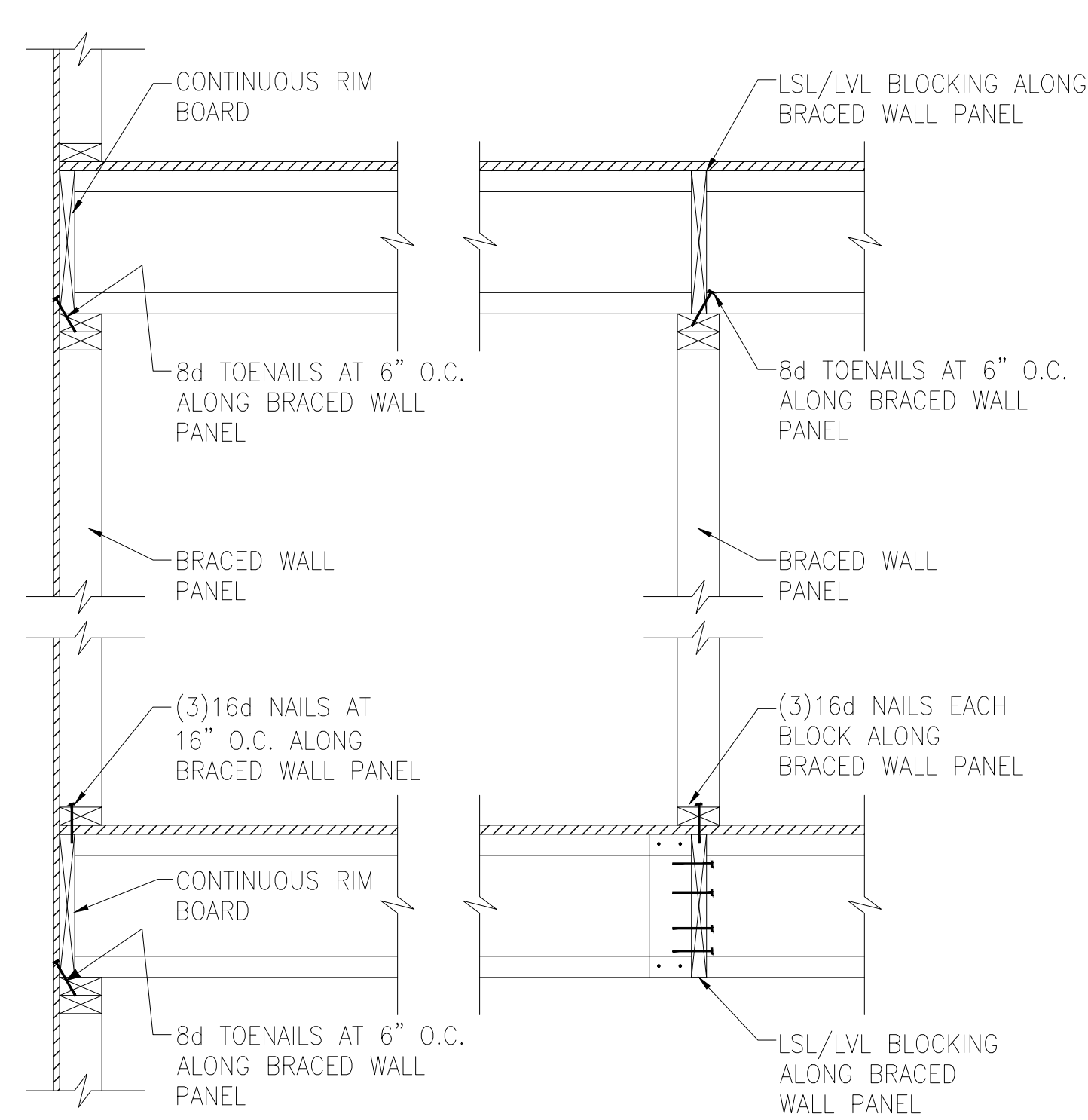
VALLEY SET TRUSSES @ 24" O.C. OR 2x6 OVERFRAMING @ 24" O.C. W/ 2x8 RIDGE & VALLEY PLATES (TYP.)

Tobacco Road Lot 124

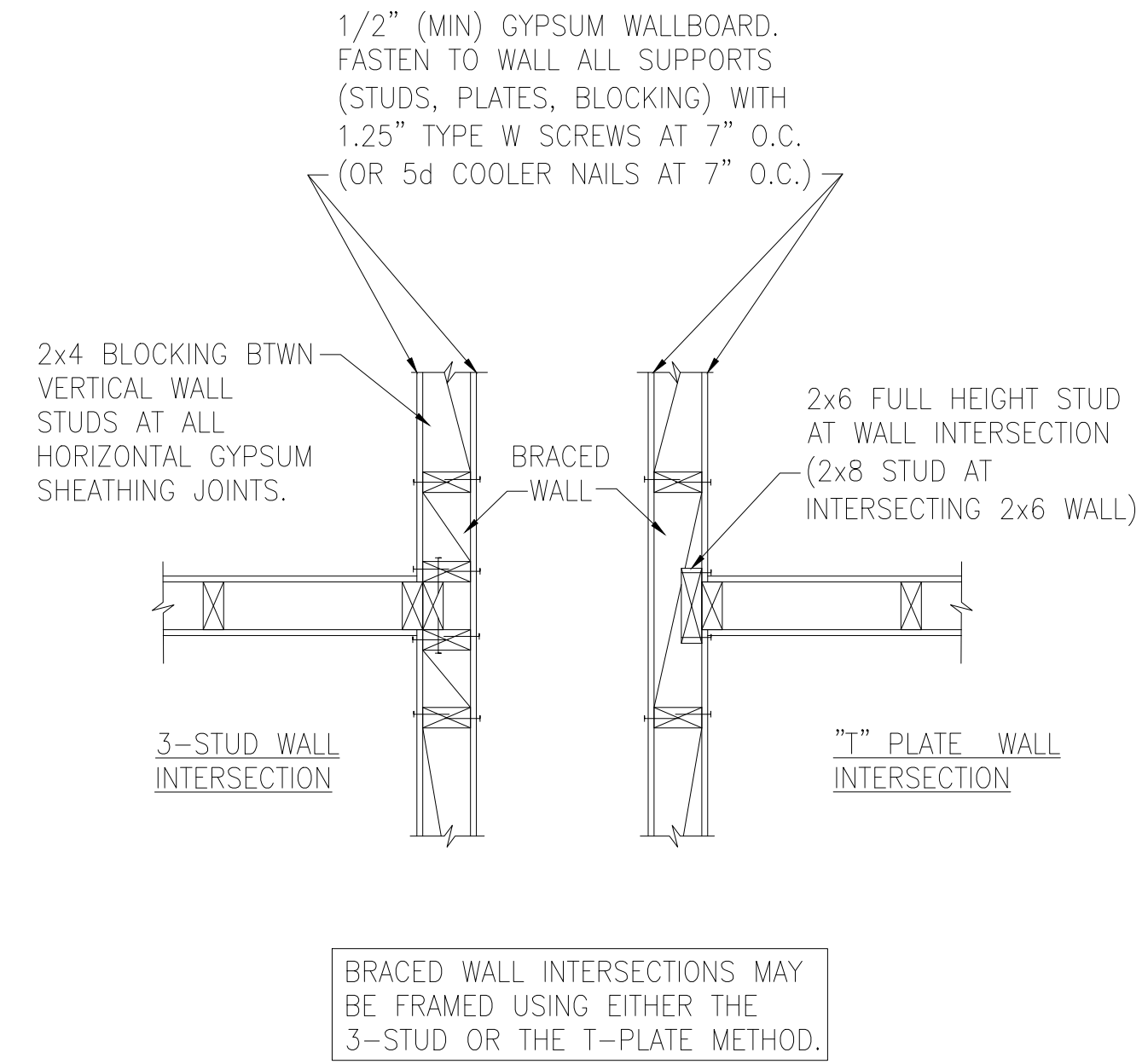




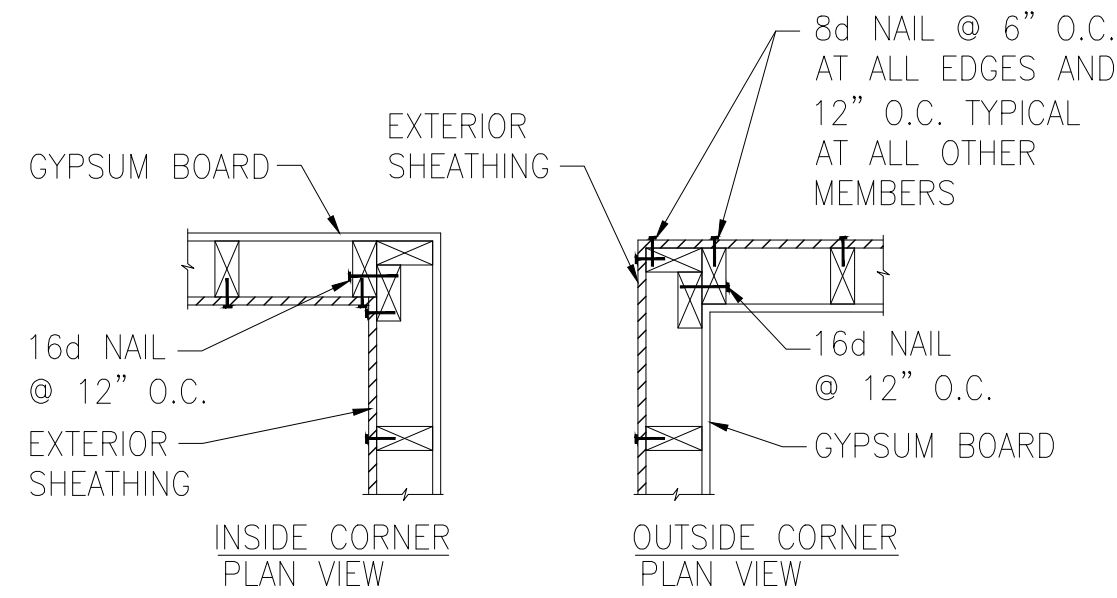
A TYPICAL BRACED WALL PANEL TO FLOOR/CEILING CONNECTION
BRACED WALL PANELS PARALLEL TO I-JOISTS



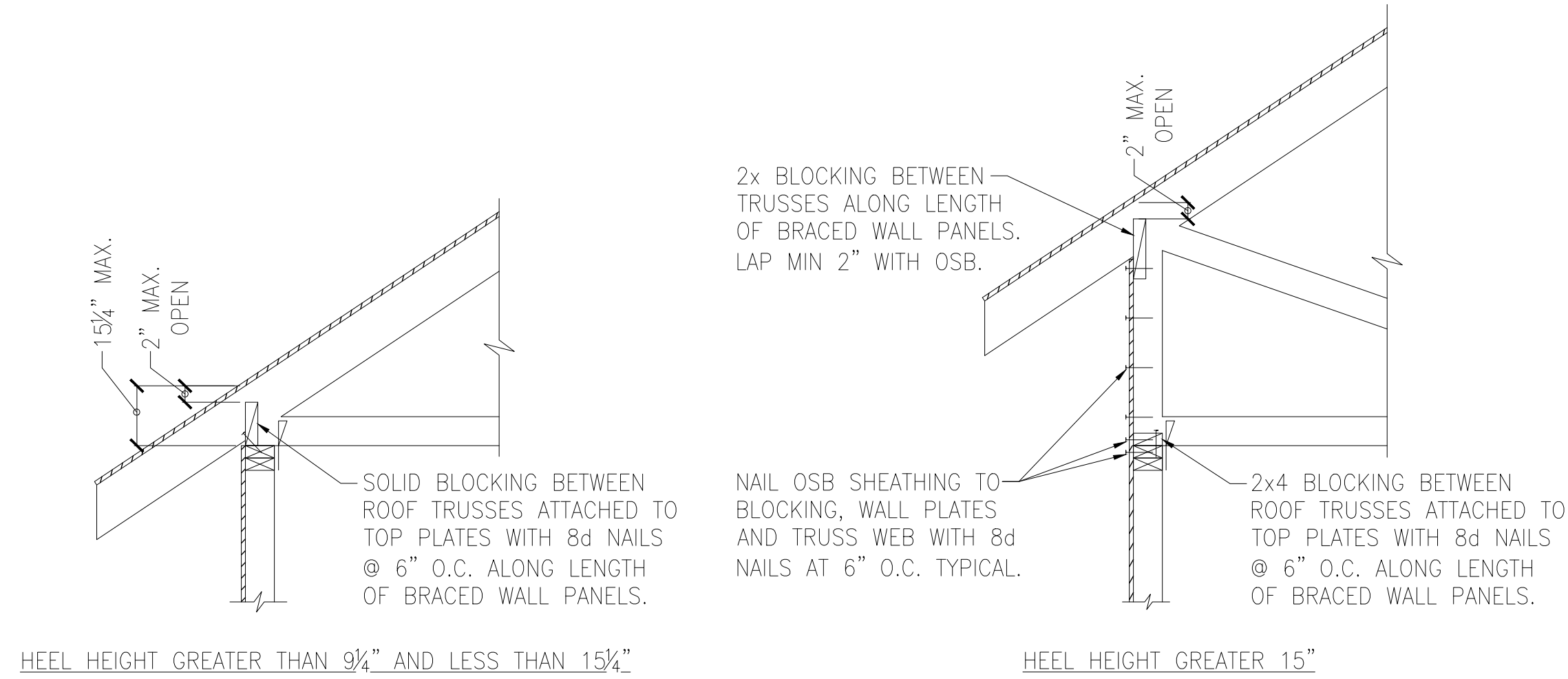
B TYPICAL BRACED WALL PANEL TO FLOOR/CEILING CONNECTION
BRACED WALL PANELS PERPENDICULAR TO I-JOISTS



C METHOD GB(1) AND GB(2) INTERSECTION DETAILS



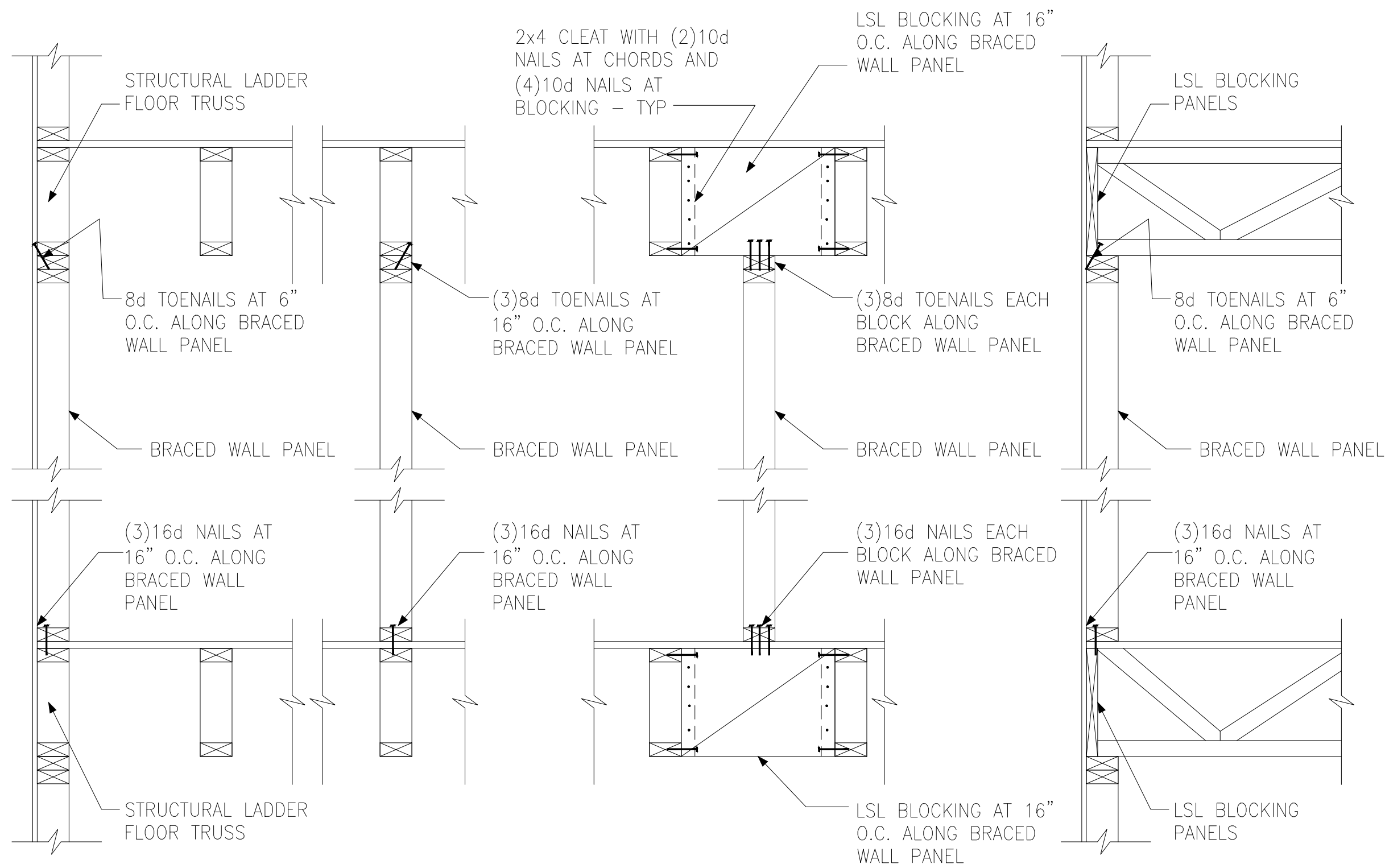
D TYPICAL EXTERIOR CORNER WALL FRAMING



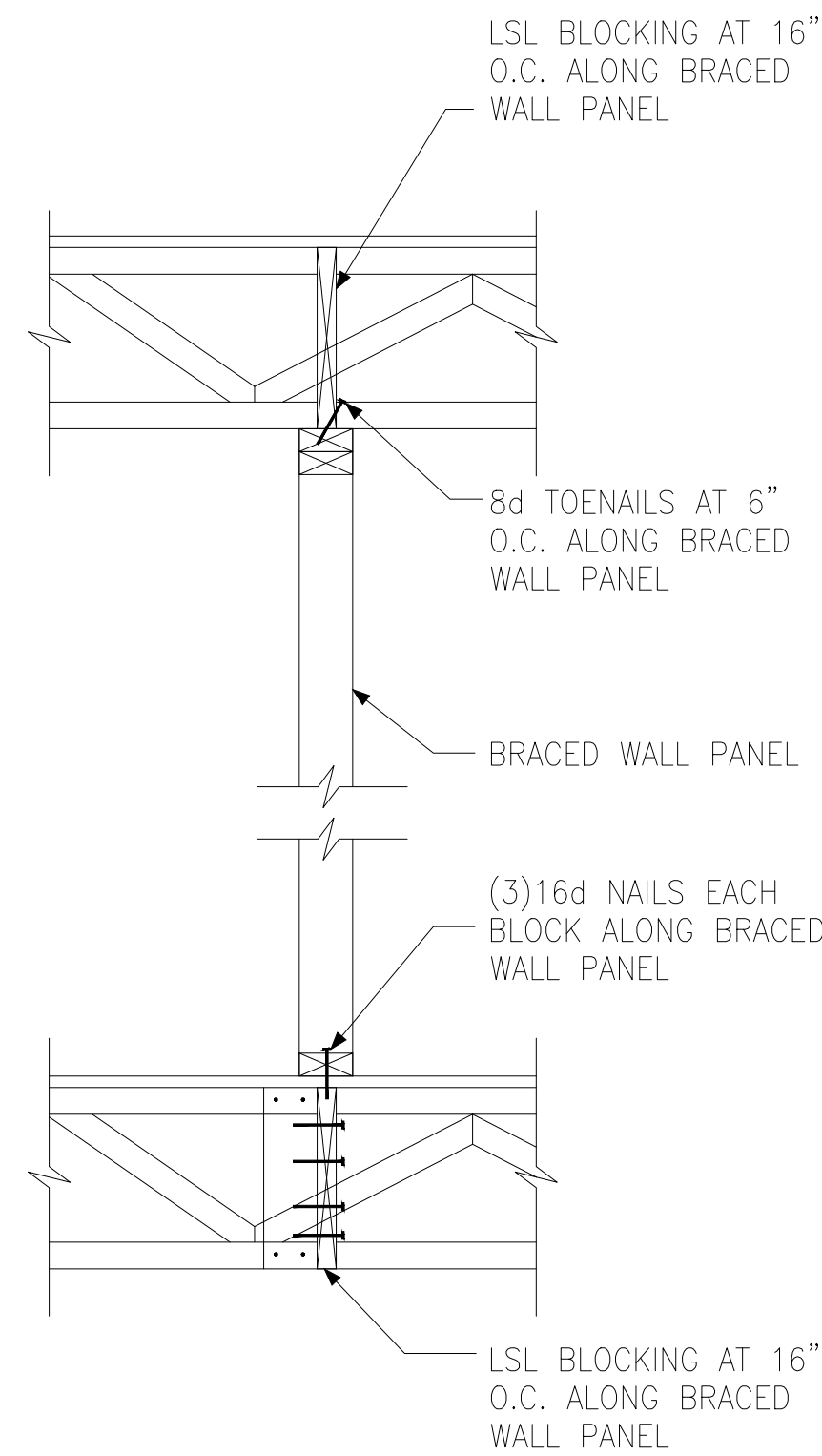
E ROOF TRUSS BEARING/BLOCKING AT BRACED WALL PANELS
ONLY REQUIRED AT BRACED WALL PANELS

Tobacco Road Lot 124

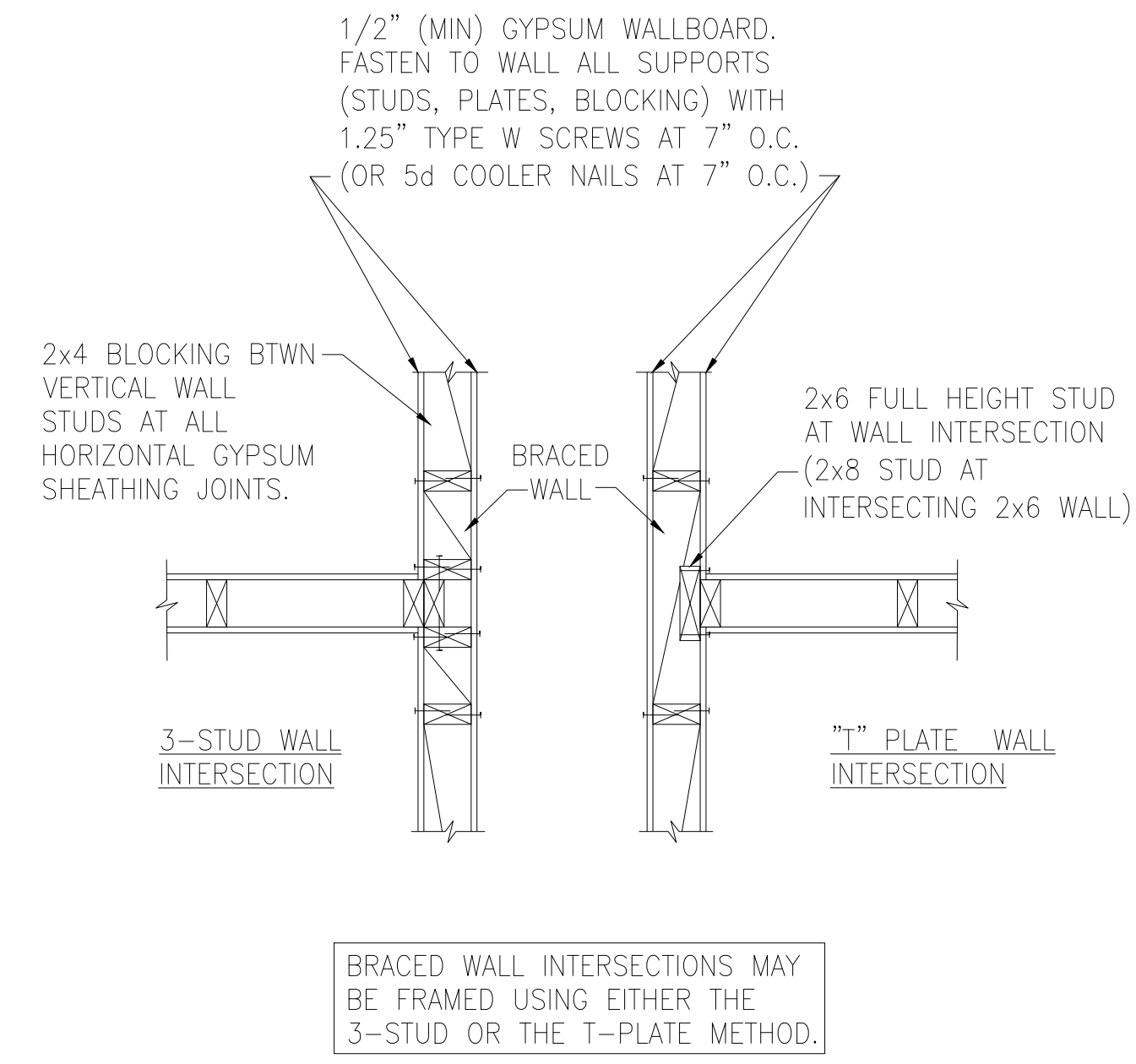




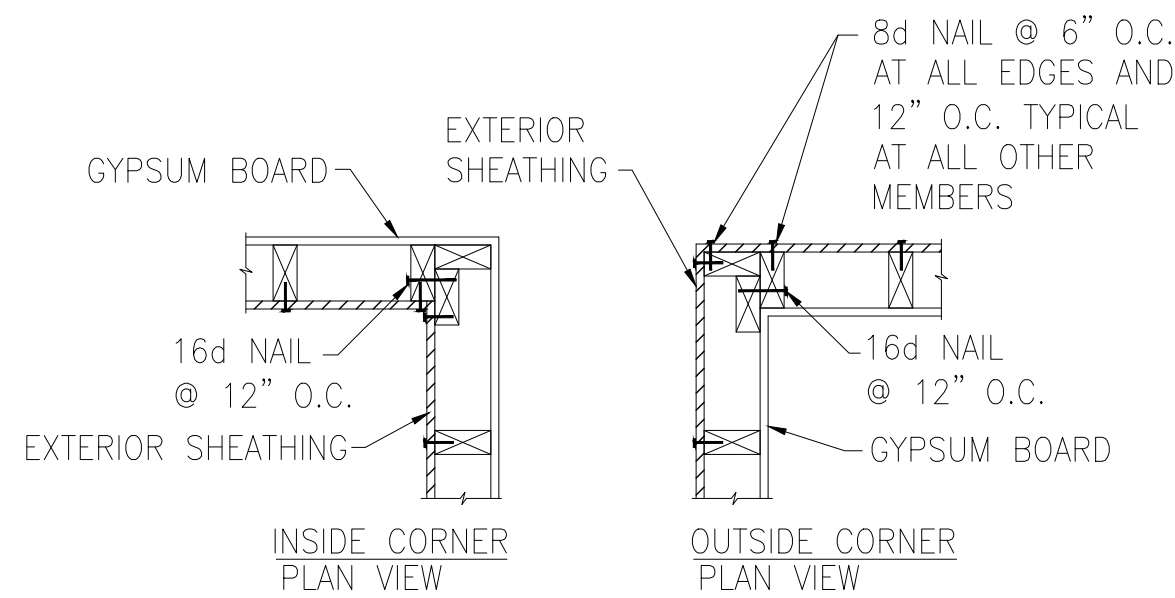
A TYPICAL BRACED WALL PANEL TO FLOOR / CEILING CONNECTION
BRACED WALL PANELS PARALLEL TO TRUSSES



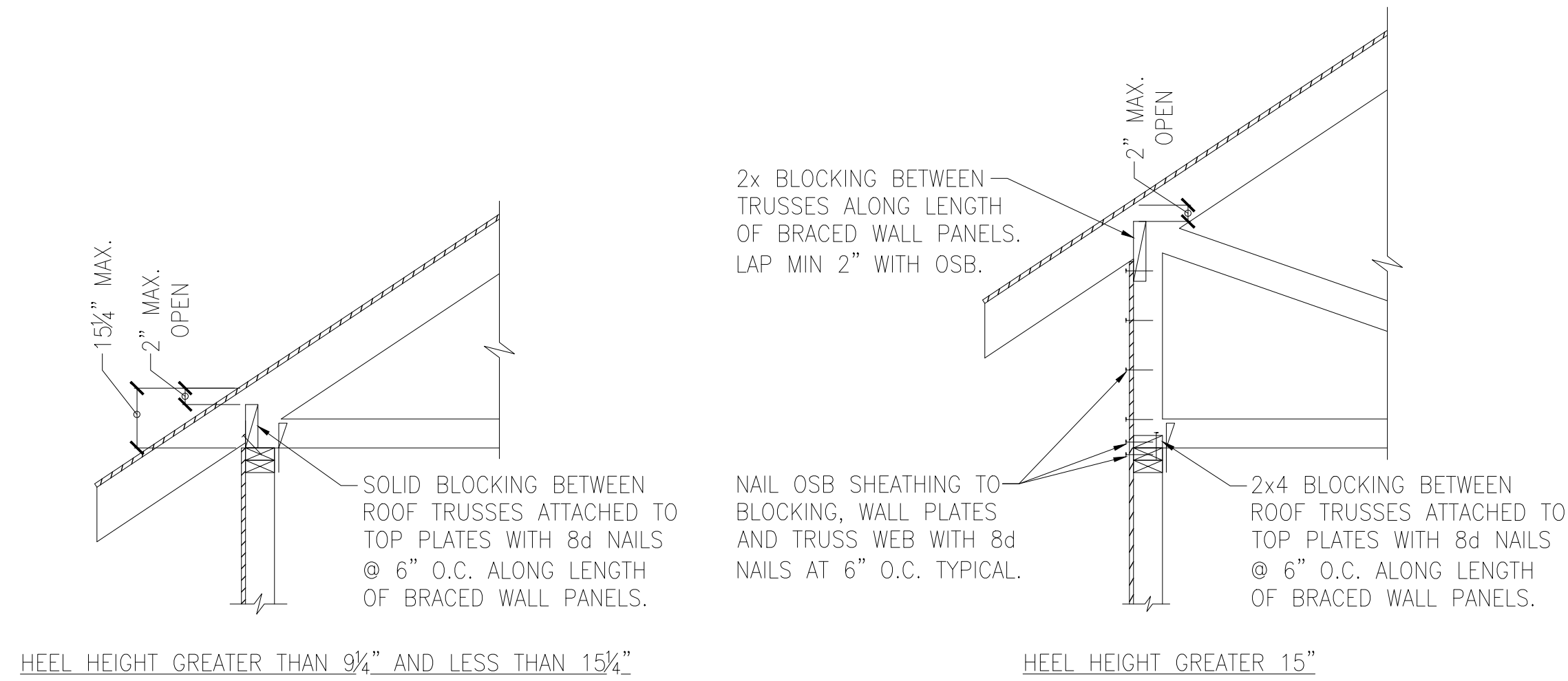
B TYPICAL BRACED WALL PANEL TO FLOOR / CEILING CONNECTION
BRACED WALL PANELS PERPENDICULAR TO TRUSSES



C METHOD GB(1) AND GB(2) INTERSECTION DETAILS



D TYPICAL EXTERIOR CORNER WALL FRAMING



E ROOF TRUSS BEARING/BLOCKING AT BRACED WALL PANELS
ONLY REQUIRED AT BRACED WALL PANELS

Tobacco Road Lot 124

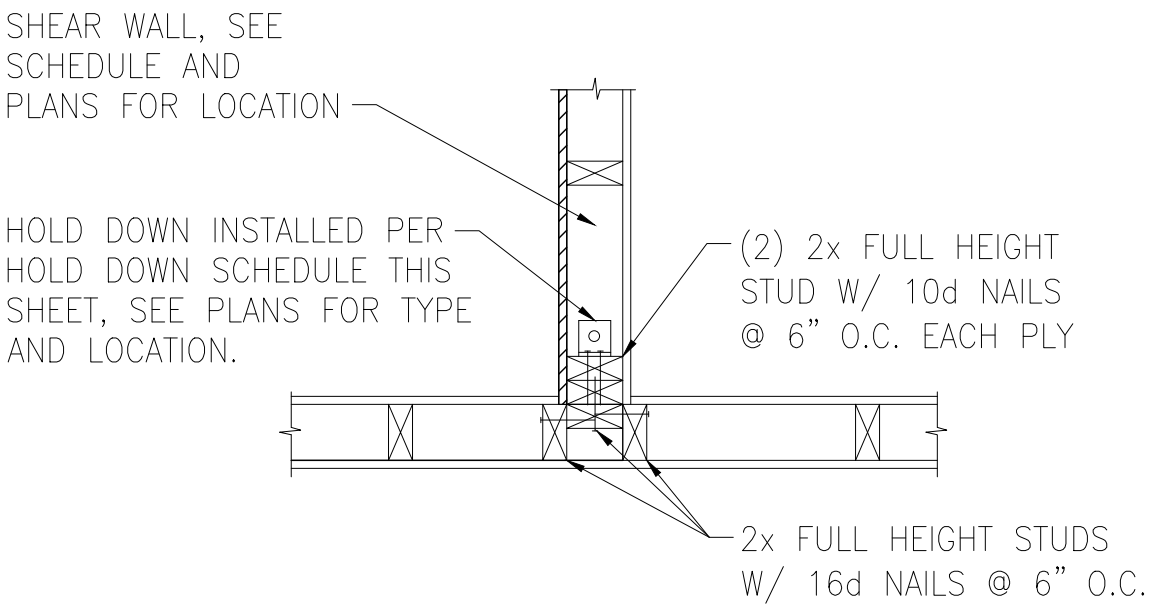


Braced Wall Details

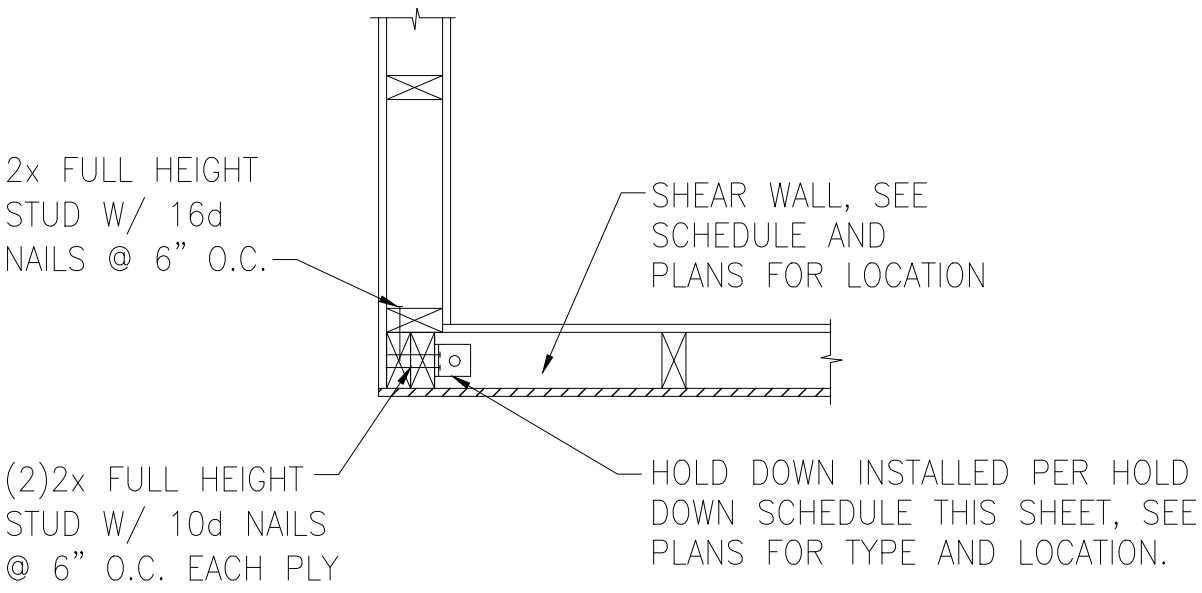
120 M.P.H.
Raleigh, North Carolina

Project #: 190-20000
Designed By: KRK
Checked By: AAM
Issue Date: 9/1/20
Re-Issue:
Scale: Not To Scale

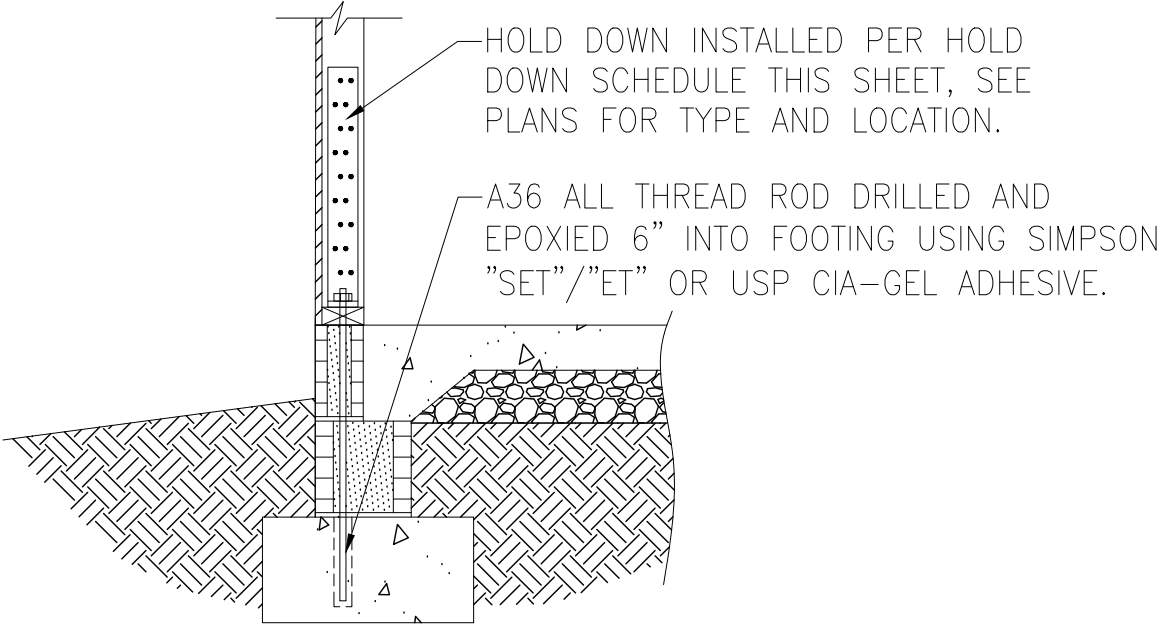
SD-1



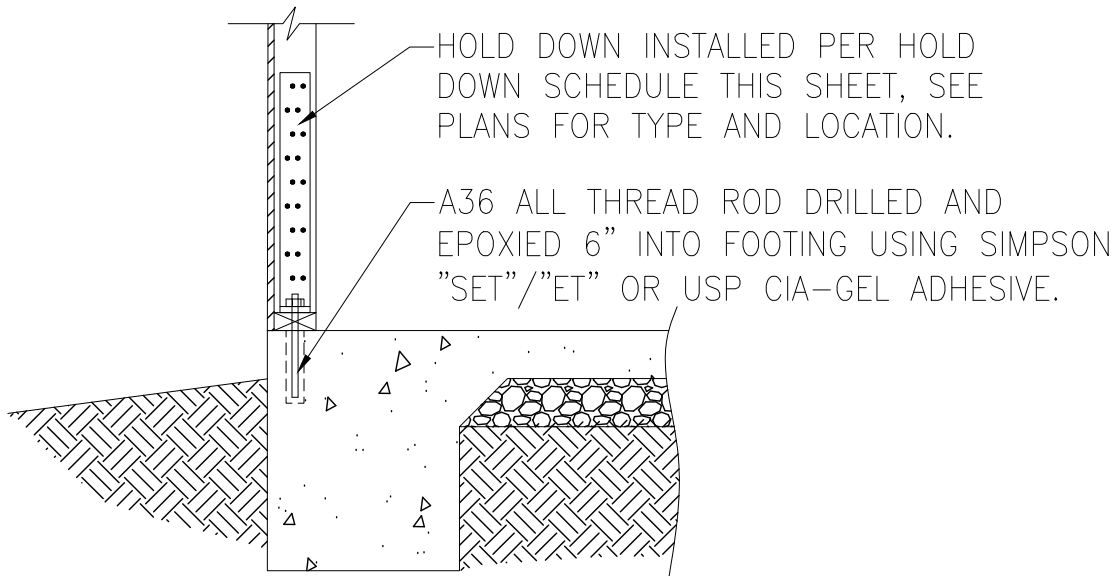
A TYPICAL HOLD DOWN DETAIL



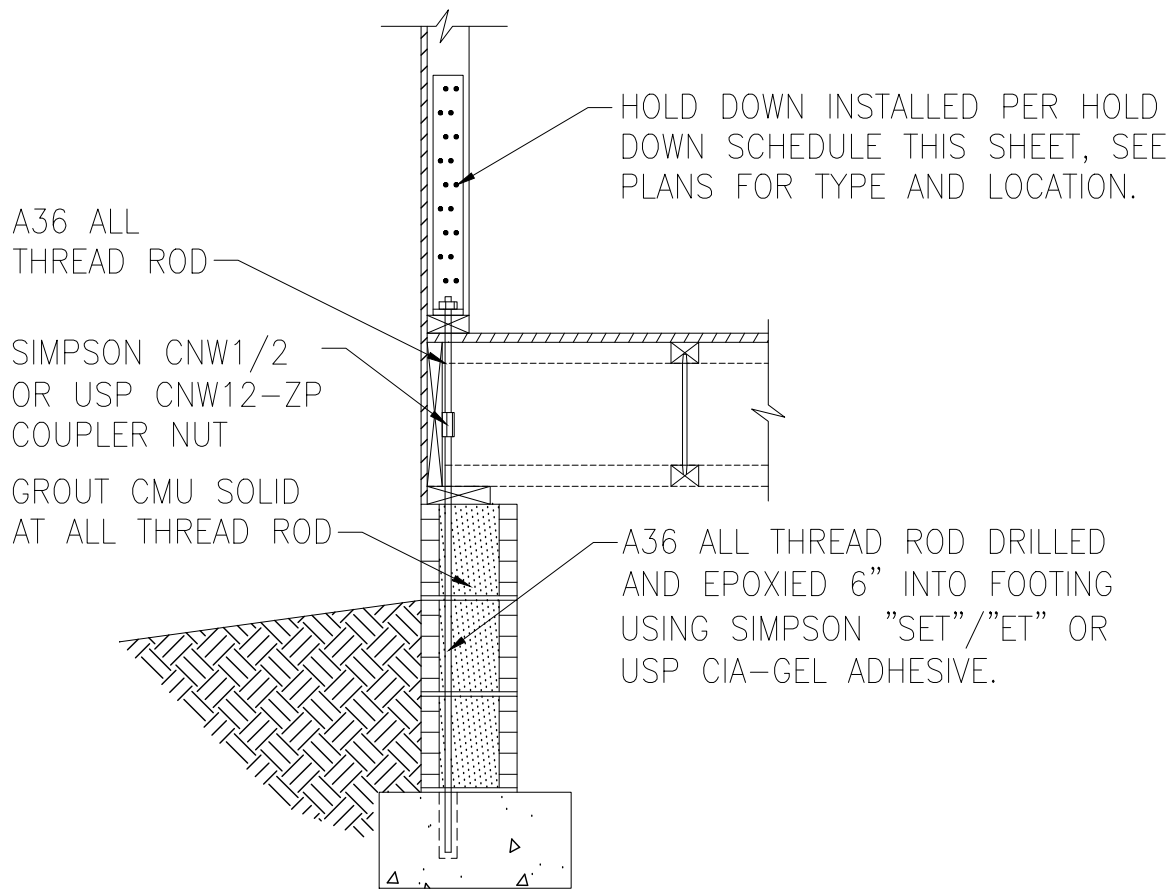
B TYPICAL HOLD DOWN DETAIL



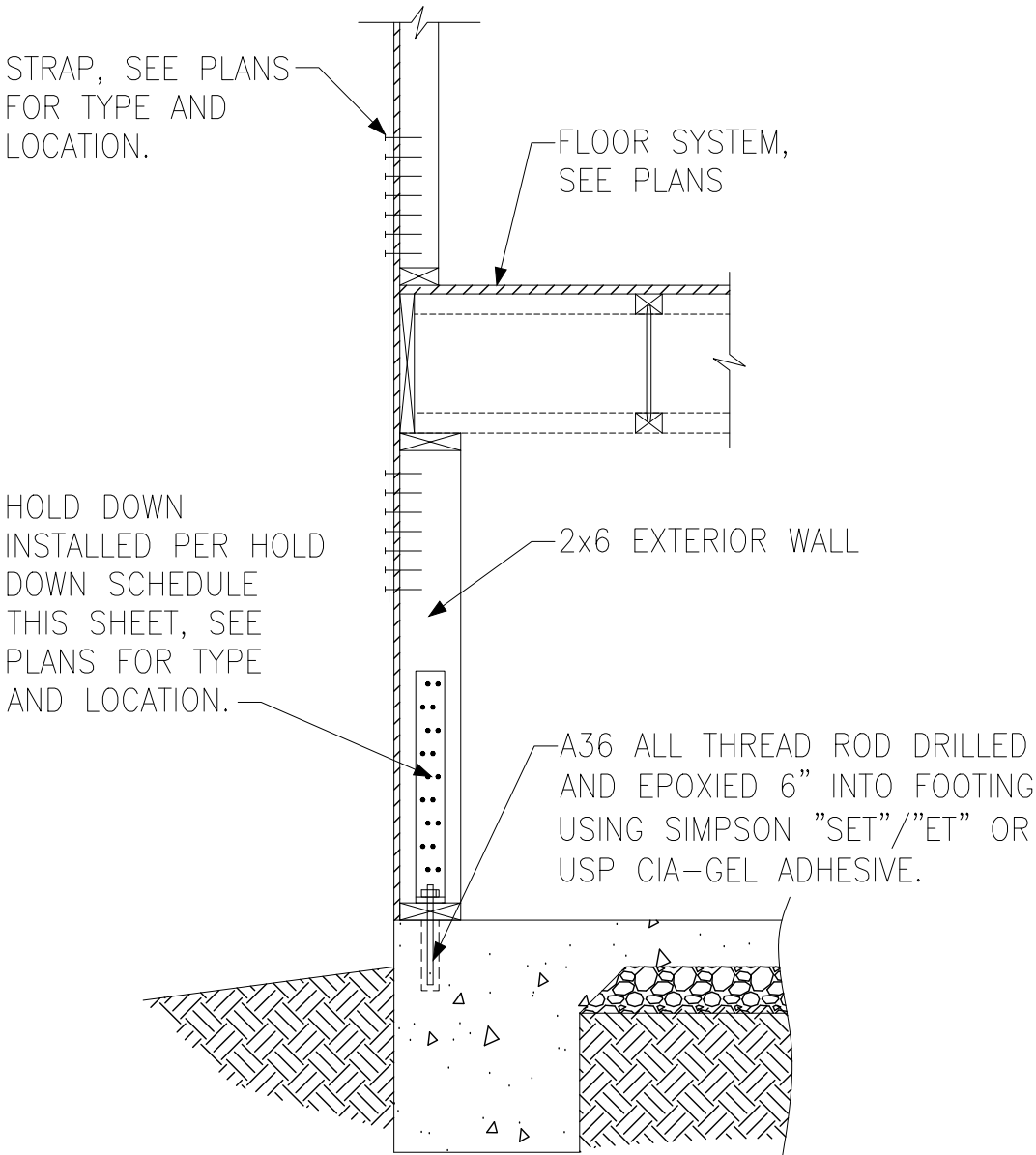
C HOLD DOWN AT STEMWALL SLAB FOUNDATION



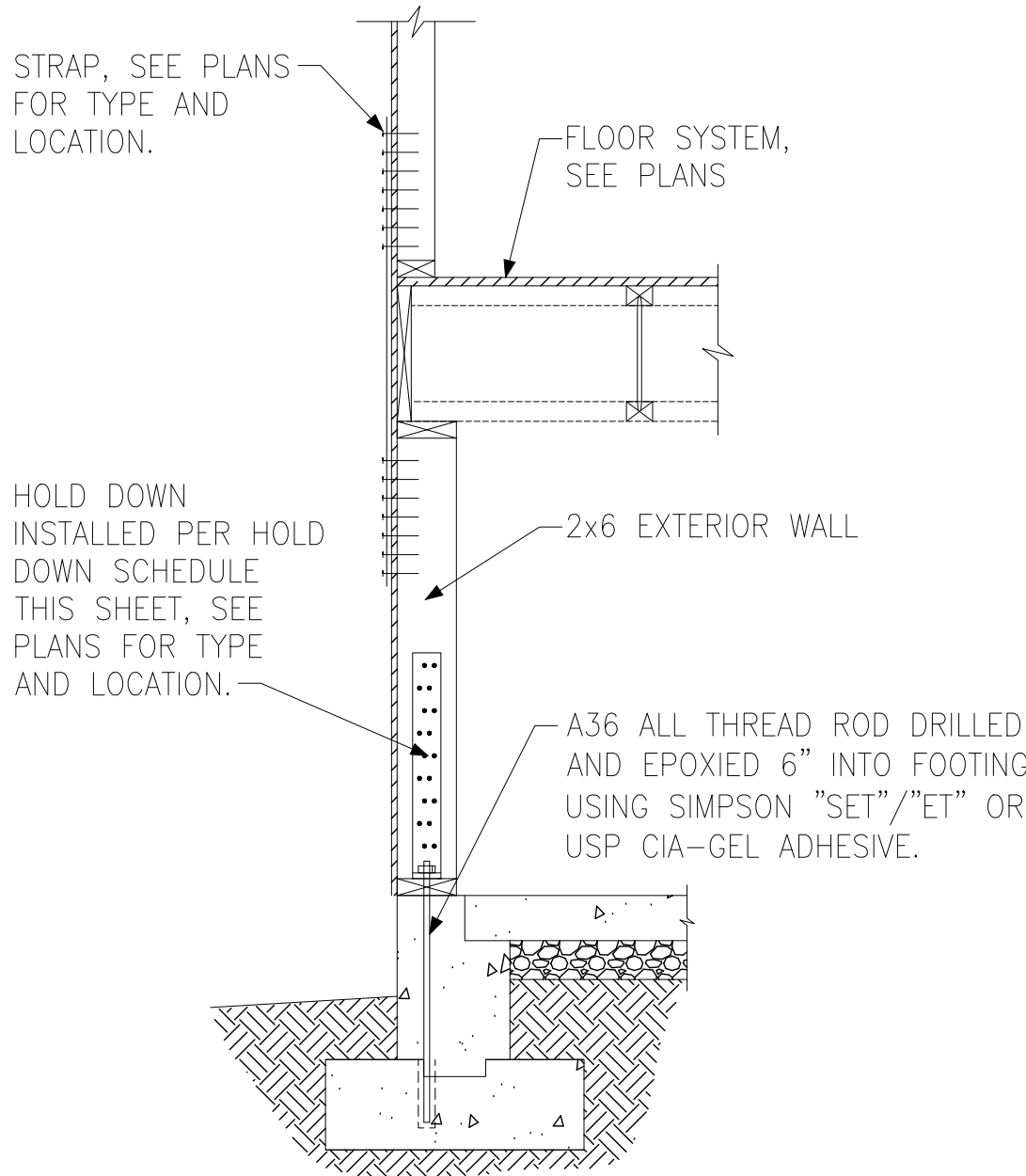
D HOLD DOWN AT MONOLITHIC SLAB FOUNDATION



E HOLD DOWN AT CRAWL SPACE FOUNDATION



F HOLD DOWN AT BASEMENT FOUNDATION MONOLITHIC TURN-DOWN



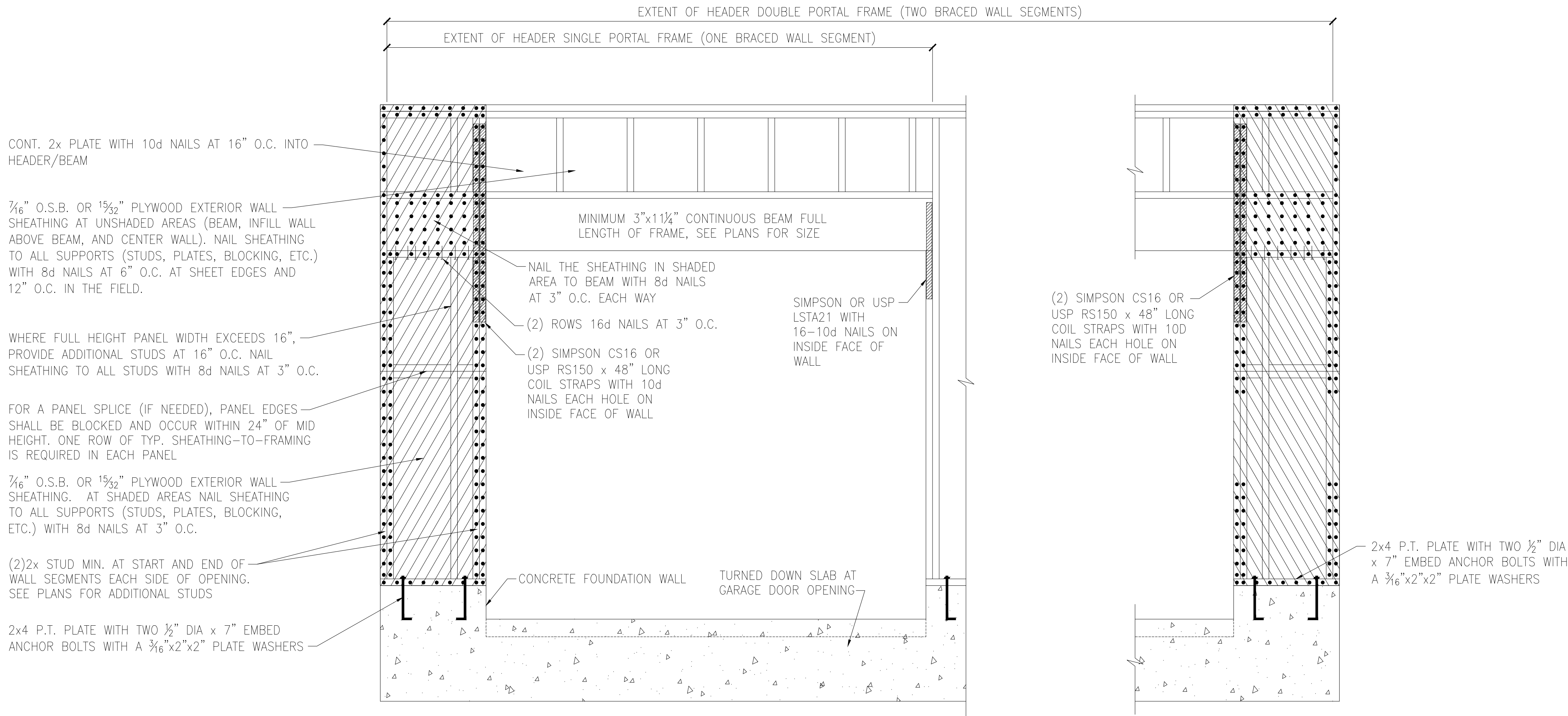
G HOLD DOWN AT BASEMENT FOUNDATION STEM WALL

HOLD DOWN SCHEDULE			
HOLD DOWN		ALL THREAD ROD	FASTENERS
SIMPSON	USP		
LTT20B	LTS20B	½" DIA.	(10)10d NAILS
HTT4	HTT16	5⁄8" DIA.	(18)16d×2½" LONG NAILS
HTT5	HTT45	5⁄8" DIA.	(26)16d×2½" LONG NAILS

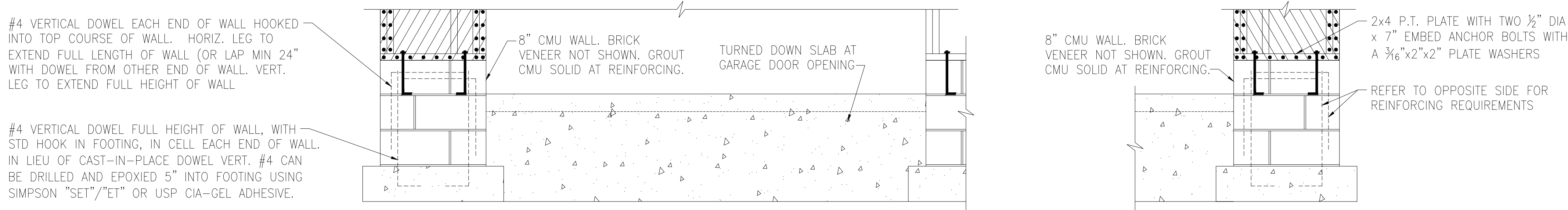
Tobacco Road Lot 124



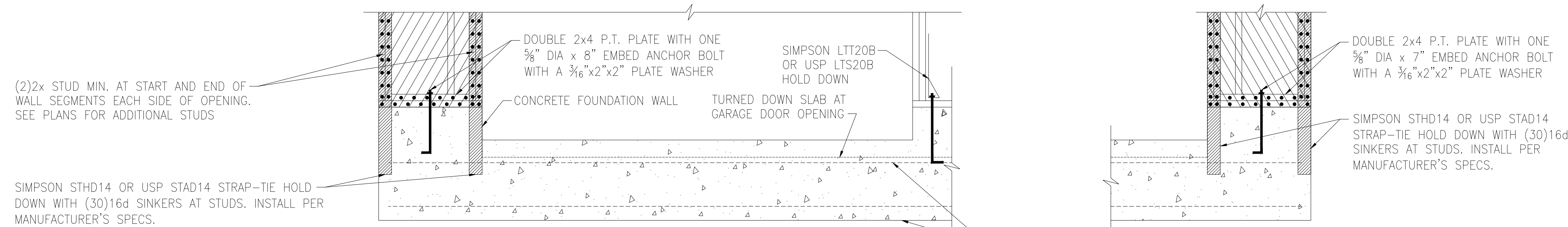




(A) METHOD CS-PF: CONTINUOUS PORTAL FRAME PANEL CONSTRUCTION
MONOLITHIC SLAB OR BASEMENT FOUNDATION



(B) METHOD CS-PF: CONTINUOUS PORTAL FRAME PANEL CONSTRUCTION
STEMWALL SLAB OR CRAWL SPACE FOUNDATION

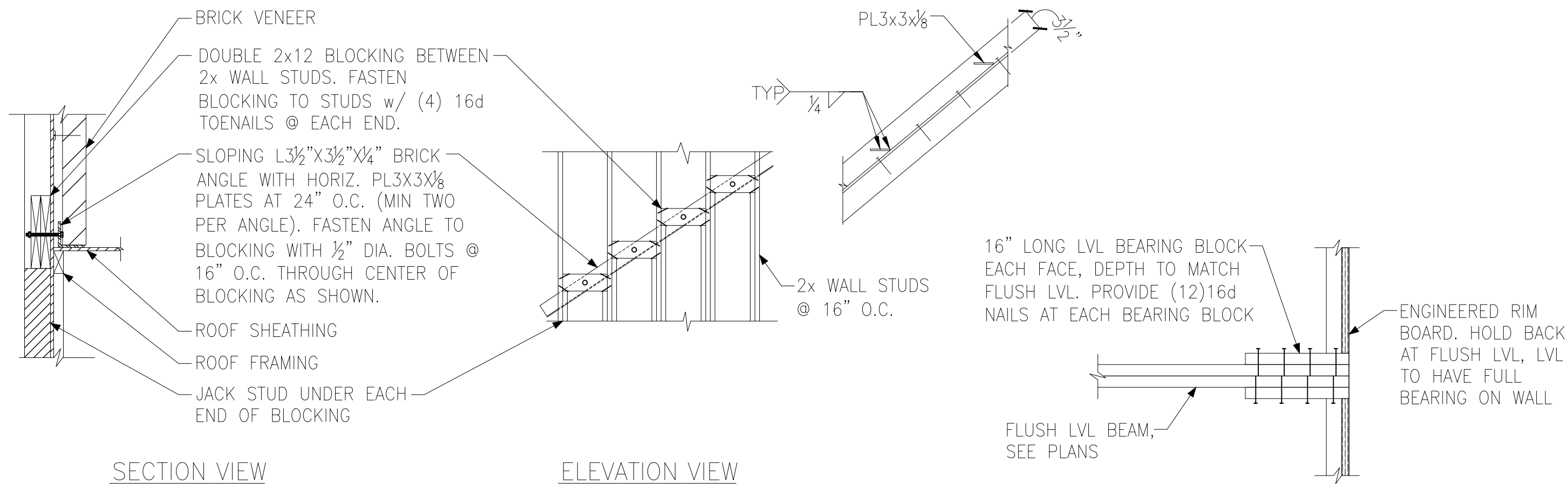


(C) METHOD PFH: PORTAL FRAME WITH HOLD-DOWNS
MONOLITHIC SLAB OR BASEMENT FOUNDATION

CONTINUOUS #4 HIGH AND LOW.
PROVIDE MIN 24" LAPS WHERE SPICED.

Tobacco Road Lot 124



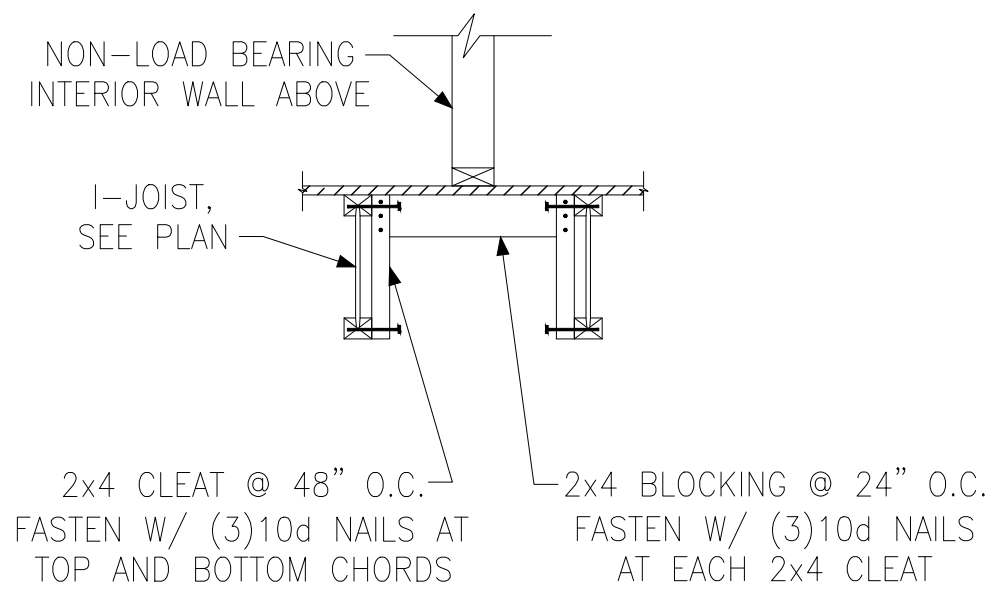


SECTION VIEW

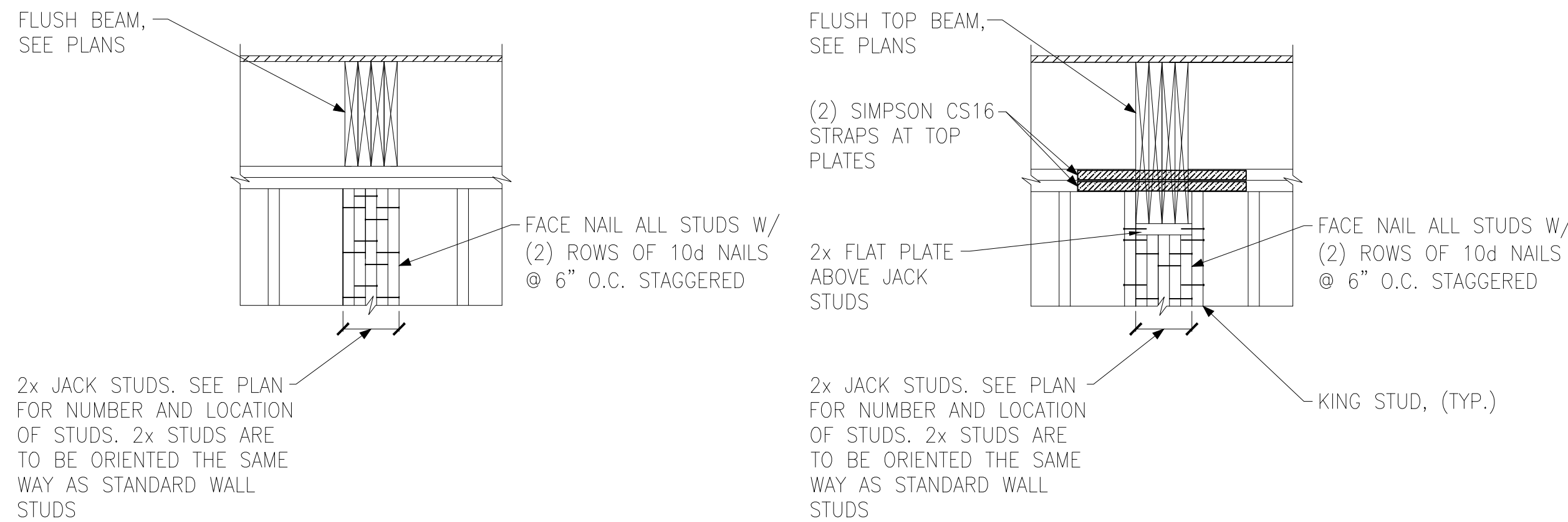
ELEVATION VIEW

A BRICK LEDGER CONNECTION DETAIL

B BEARING ENHANCER FLUSH LVL



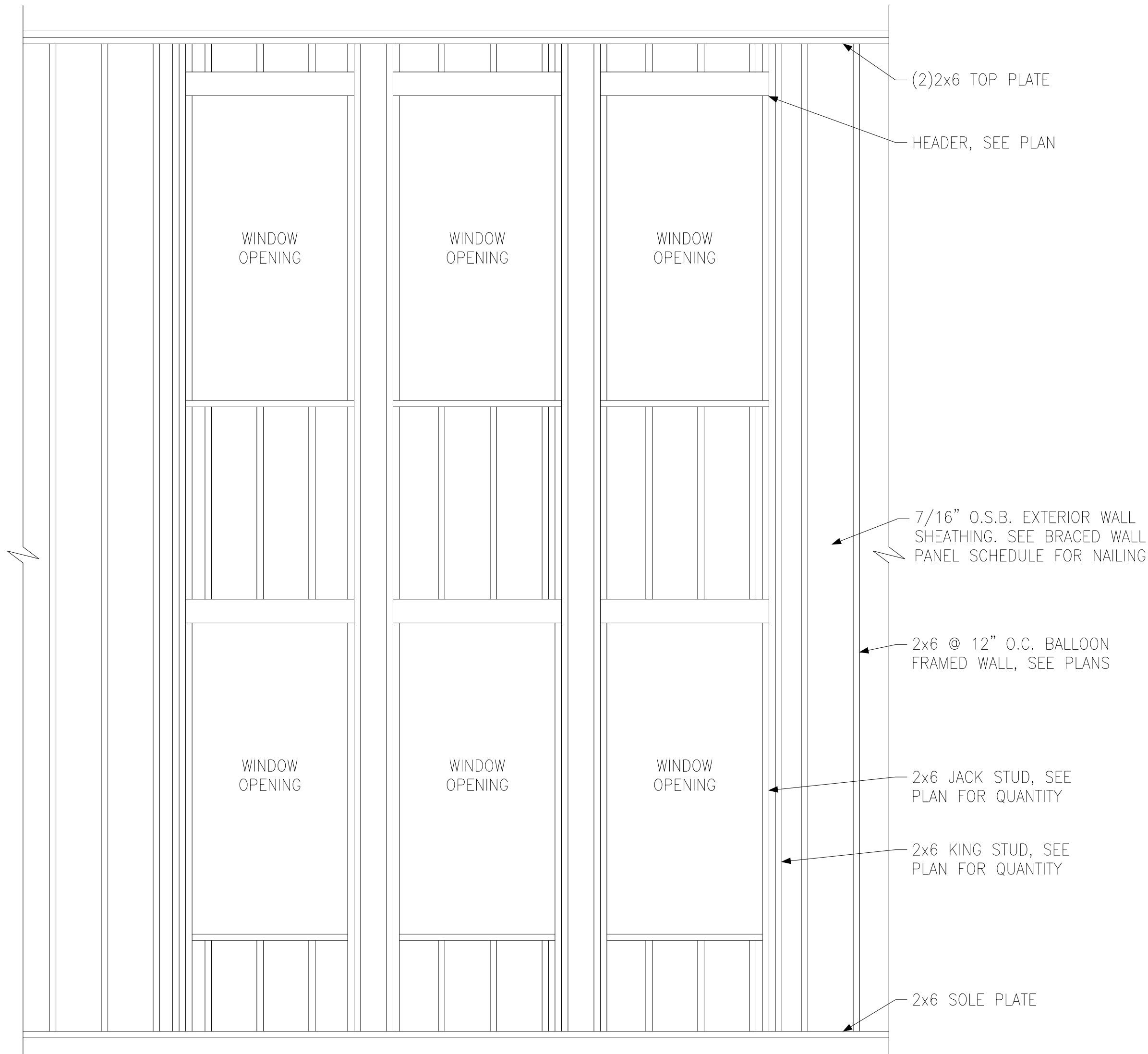
C I-JOIST LADDER BLOCKING AS REQUIRED @ PARALLEL WALLS



FLUSH BEAM

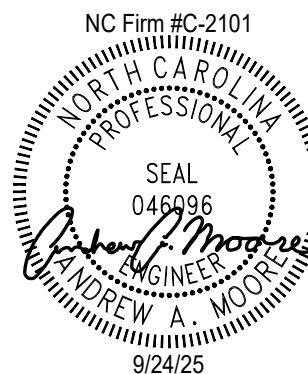
FLUSH TOP BEAM

E BUILT-UP STUD DETAIL SUPPORTING BEAM



D BALLOON FRAMED WALL DETAIL N.T.S.

Tobacco Road Lot 124



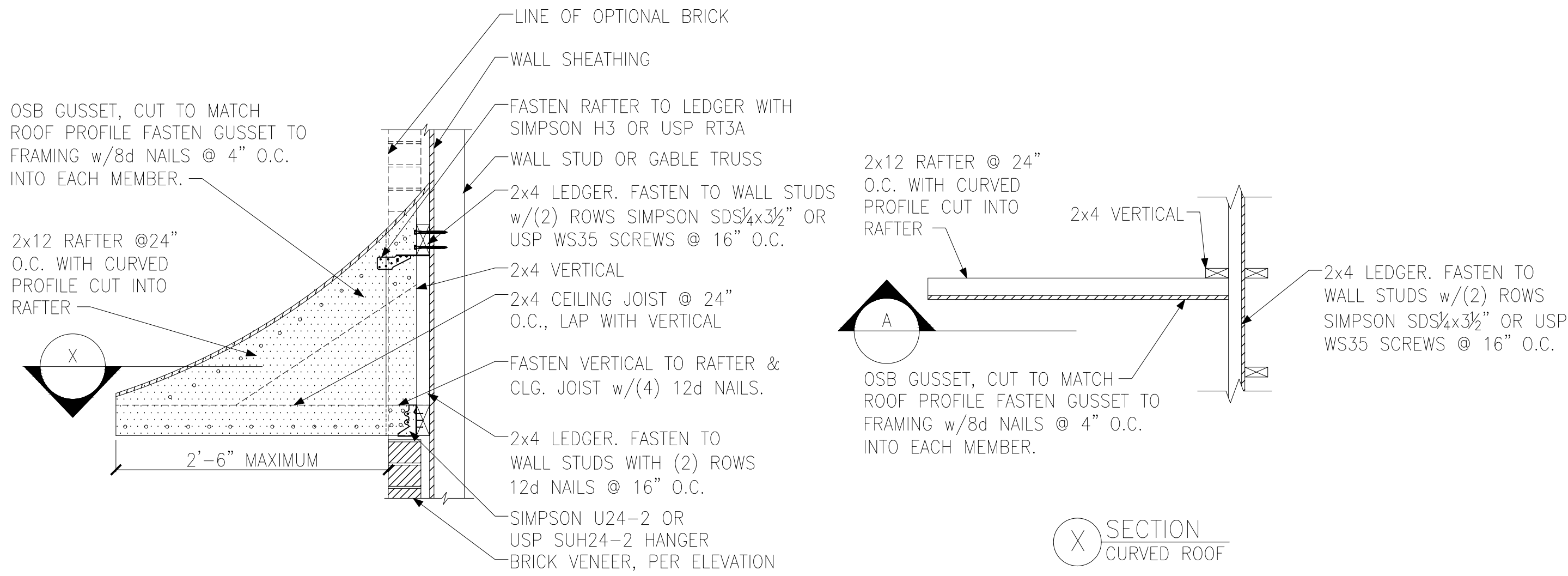
Miscellaneous Framing Details

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Raleigh, North Carolina

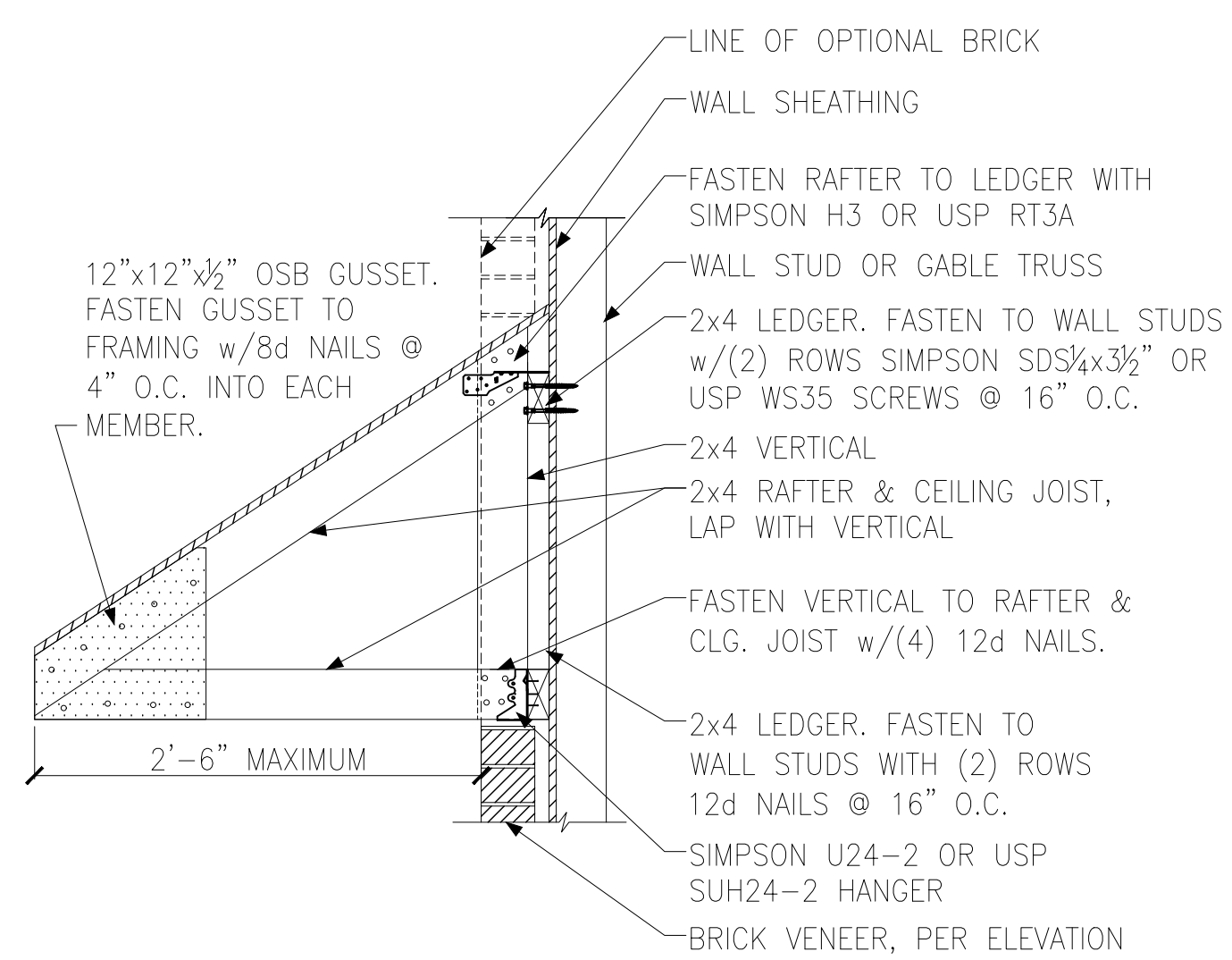
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Designed By: KRK
Checked By: AAM
Issue Date: 9/1/20
Re-Issue:
Scale: Not To Scale

SD-5

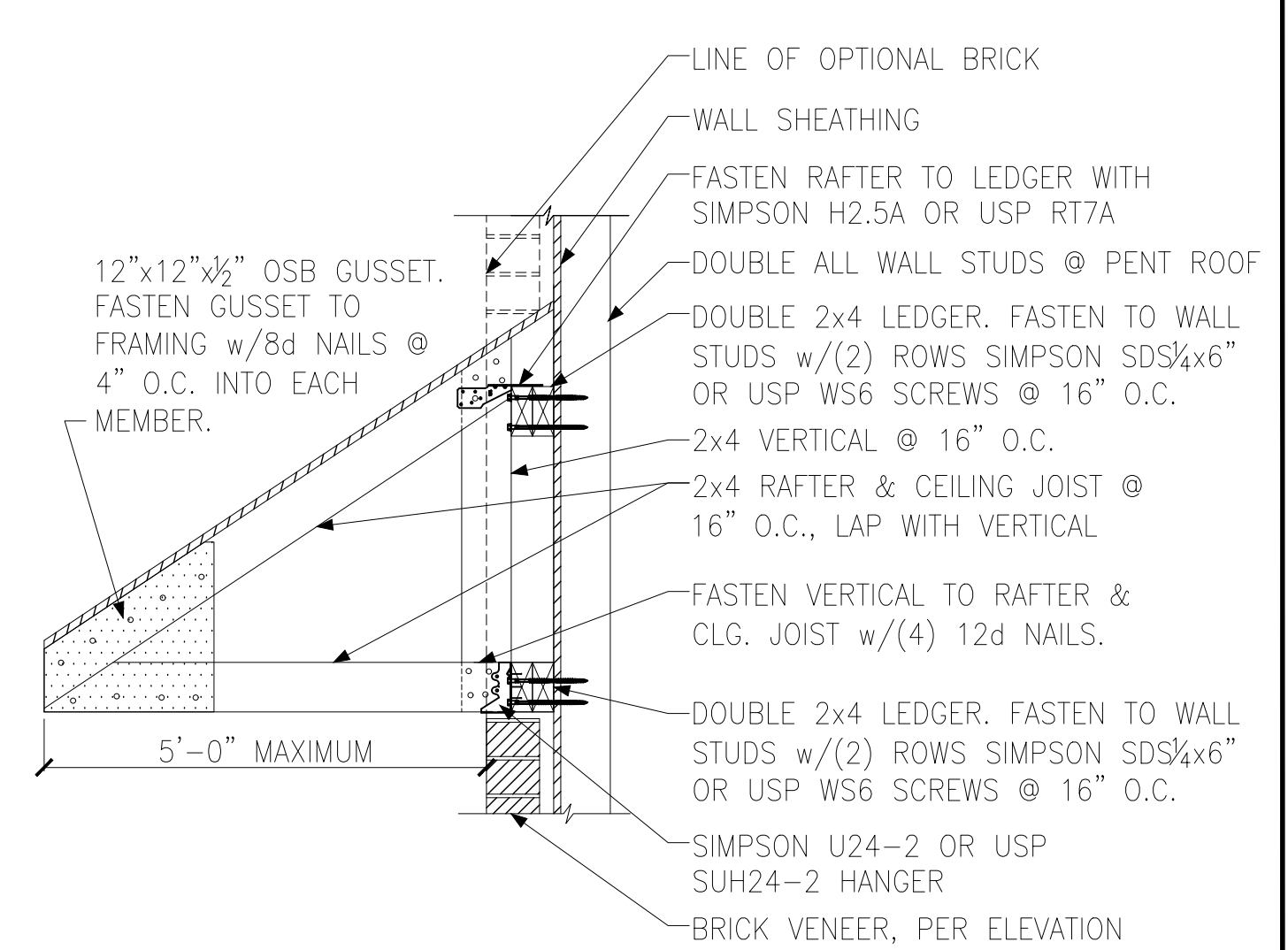




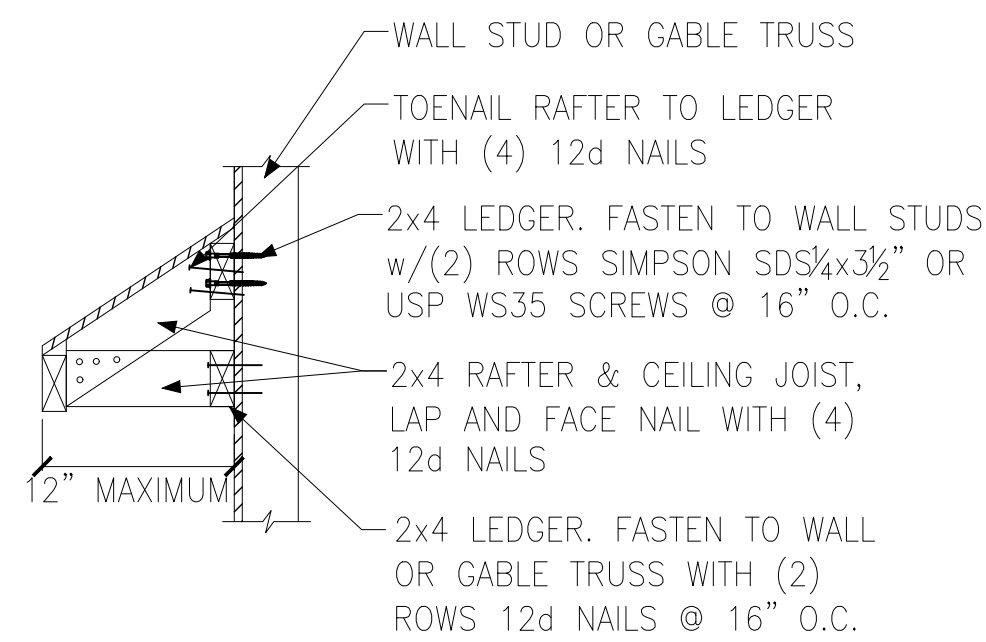
A PENT ROOF DETAIL
CURVED ROOF



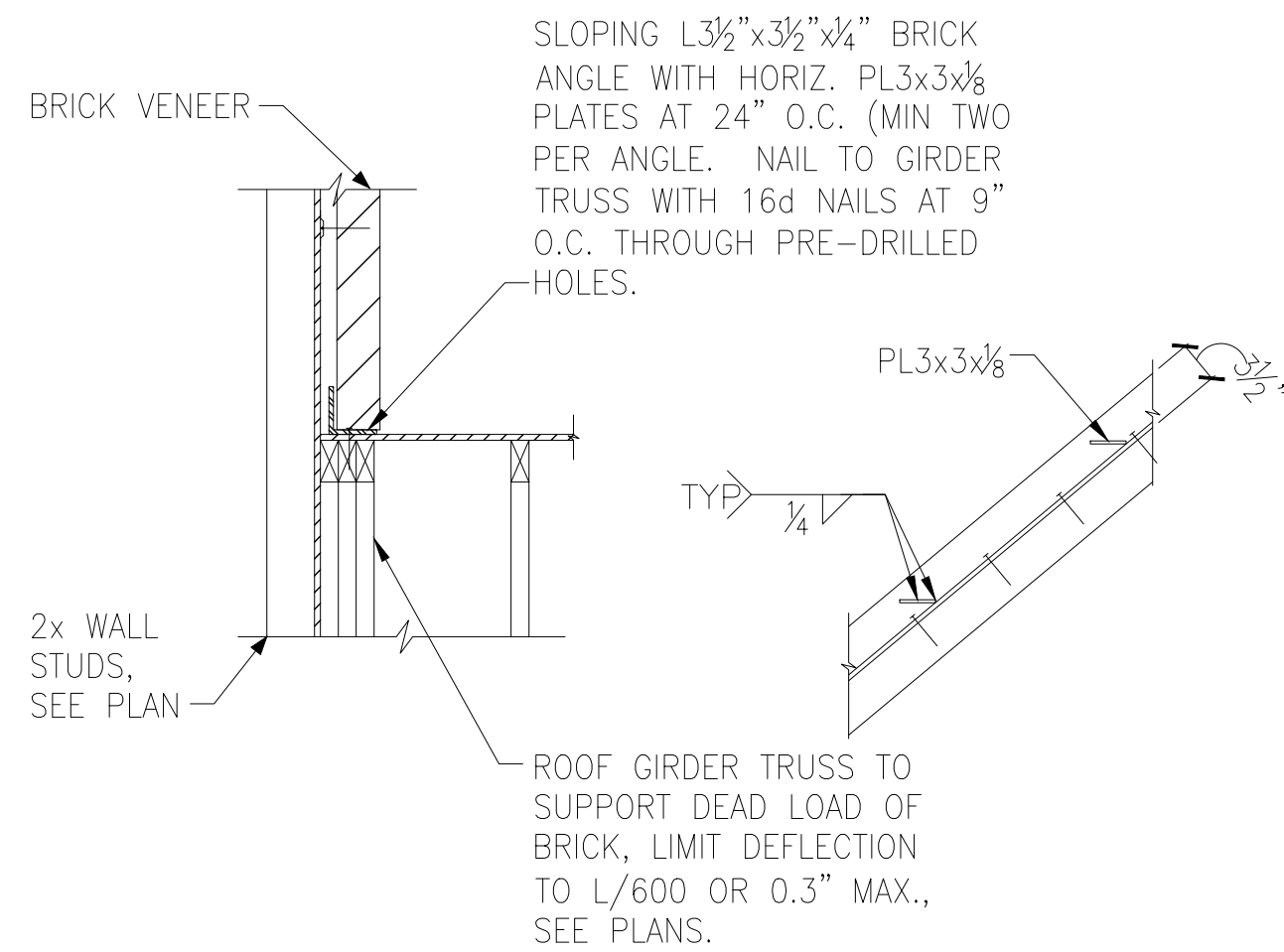
B PENT ROOF DETAIL
STRAIGHT ROOF



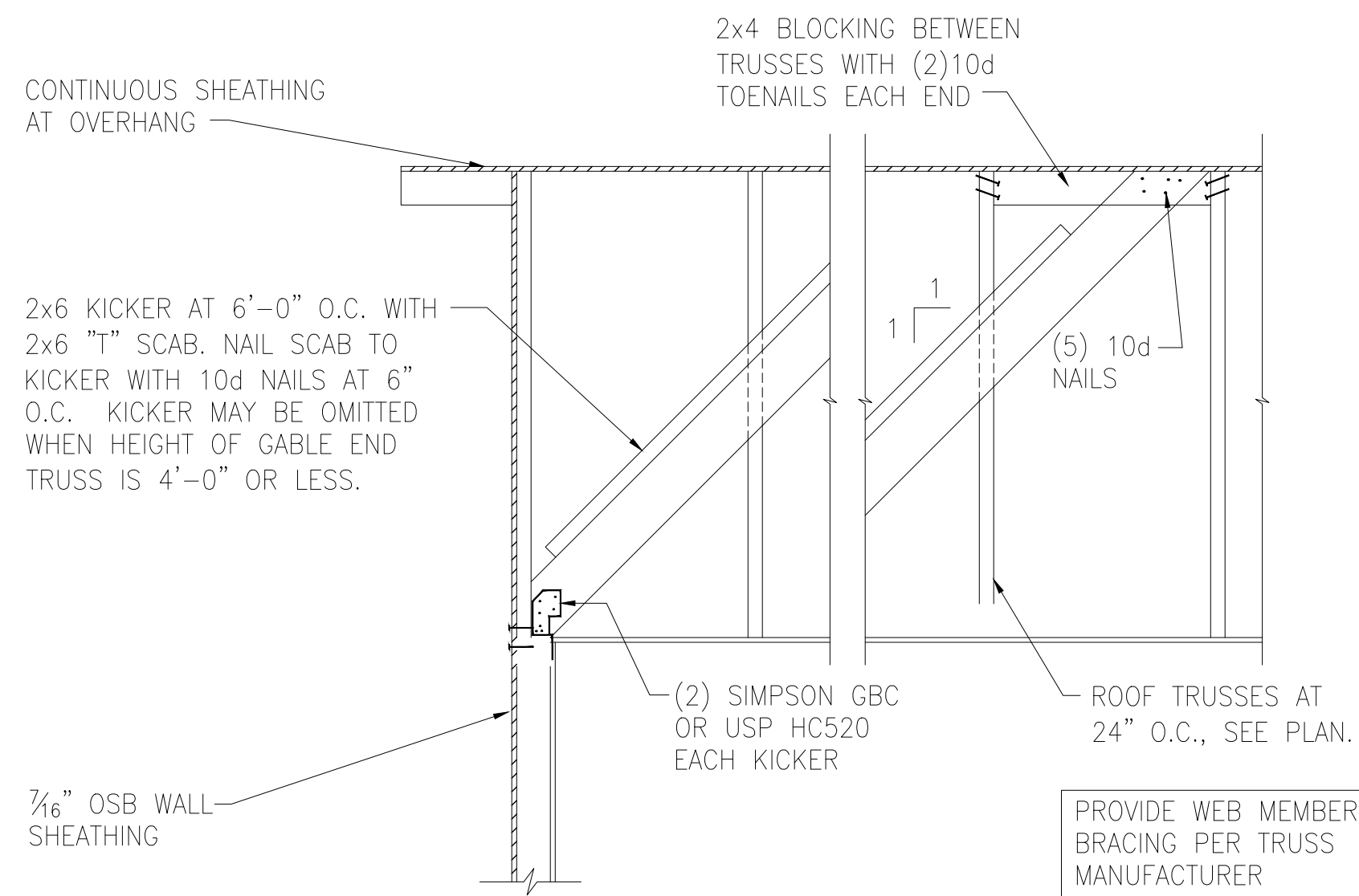
B PENT ROOF DETAIL
STRAIGHT ROOF 5'-0" MAXIMUM SPAN



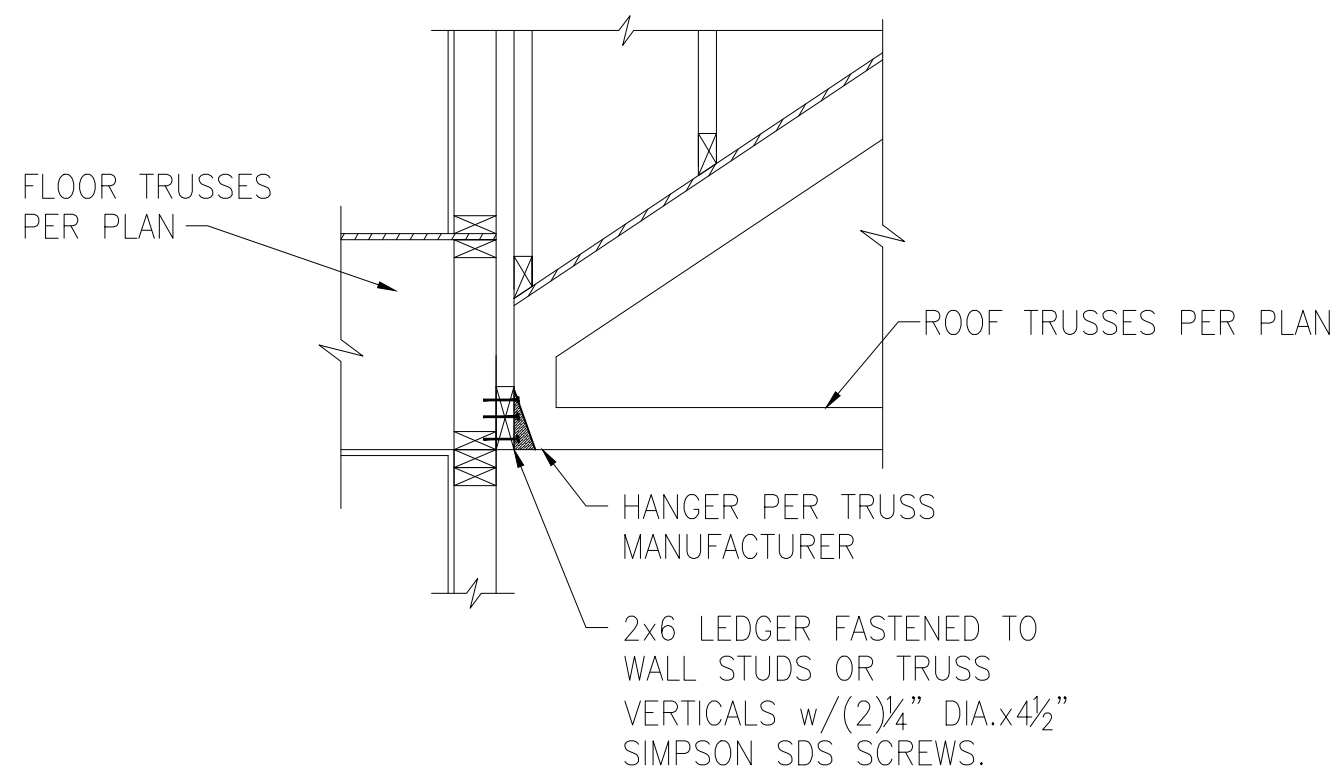
C EYEBROW ROOF DETAIL
STRAIGHT ROOF



D TRUSS DETAIL



E GABLE END WALL DETAIL



F ROOF TRUSS LEDGER DETAIL

Tobacco Road Lot 124



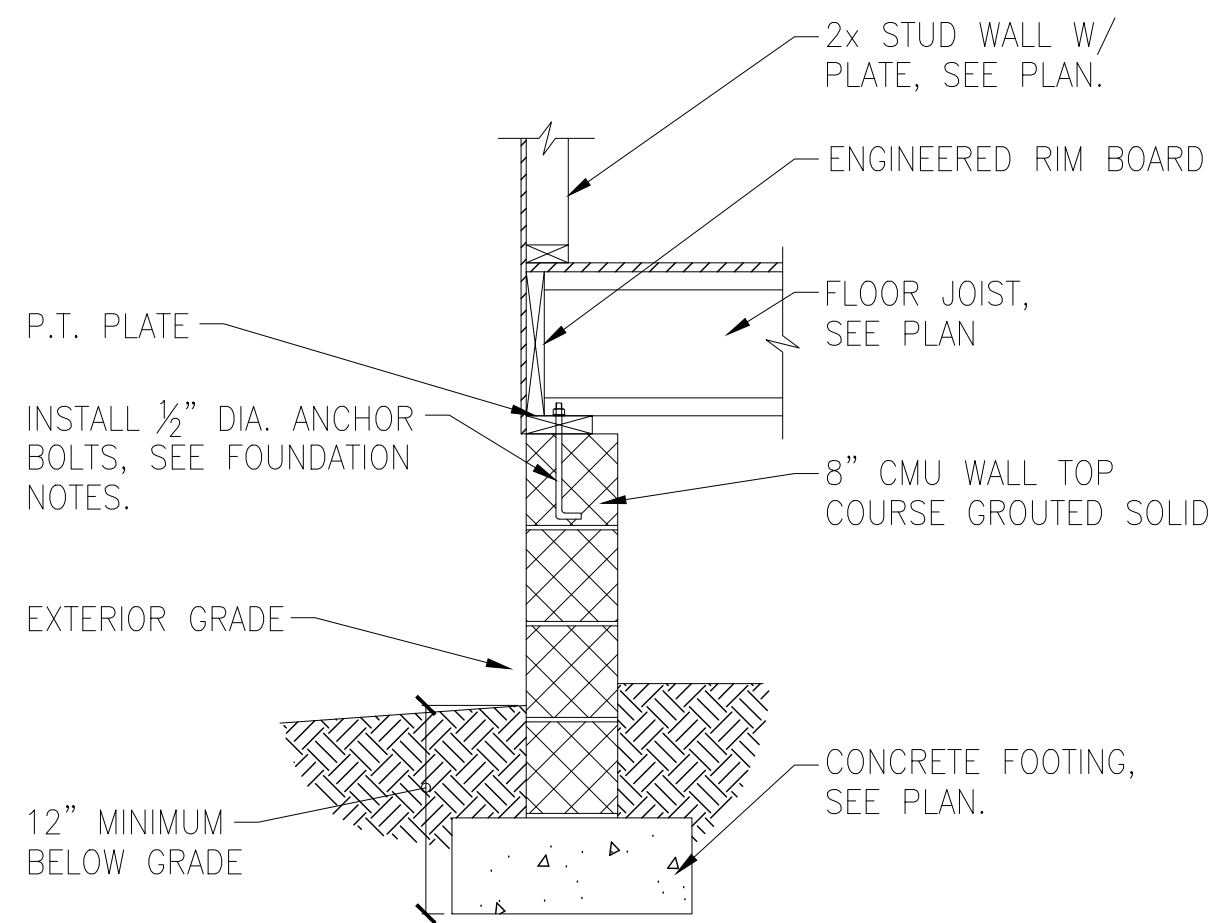
Miscellaneous Framing Details



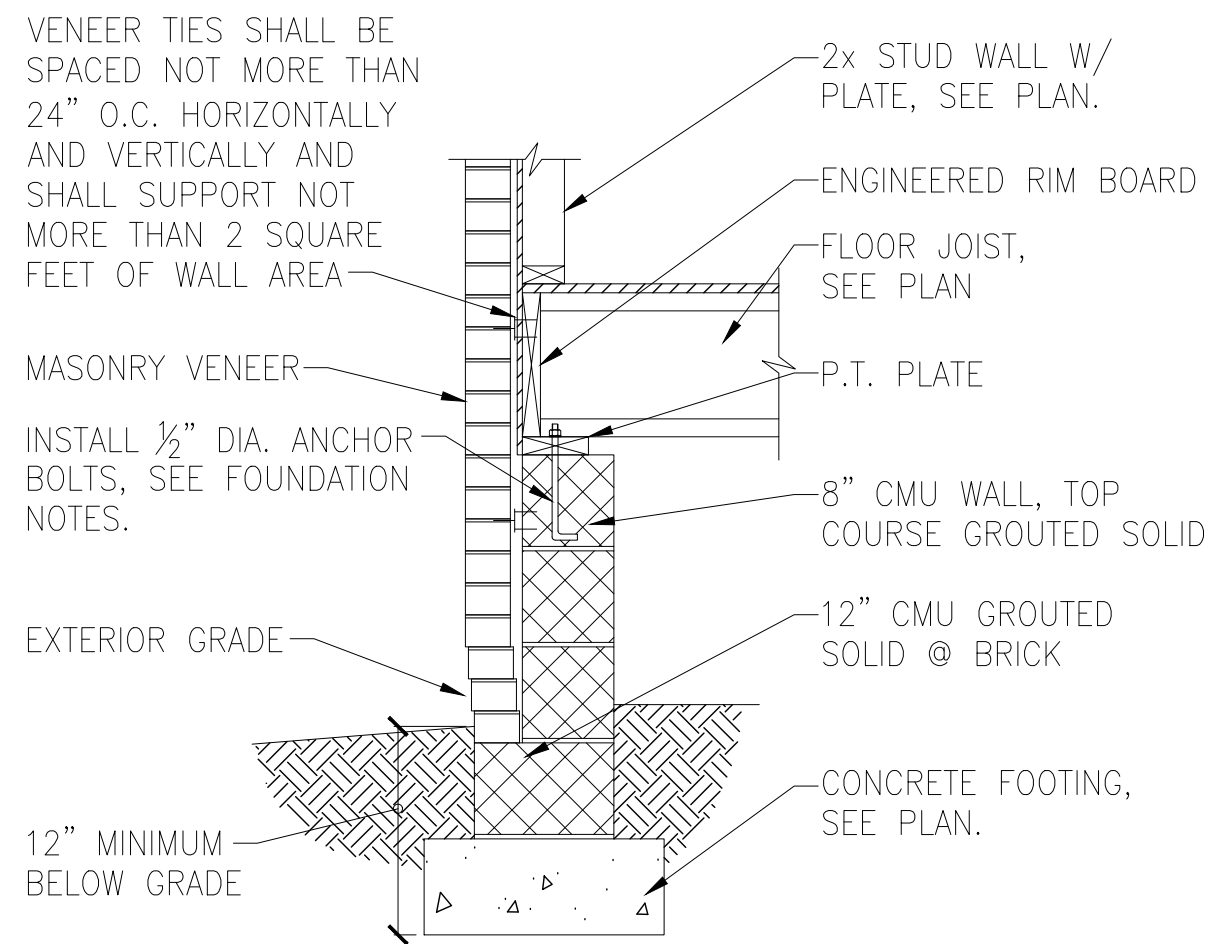
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Designed By: KRK
Checked By: AAM
Issue Date: 9/1/20
Re-Issue:
Scale: Not To Scale

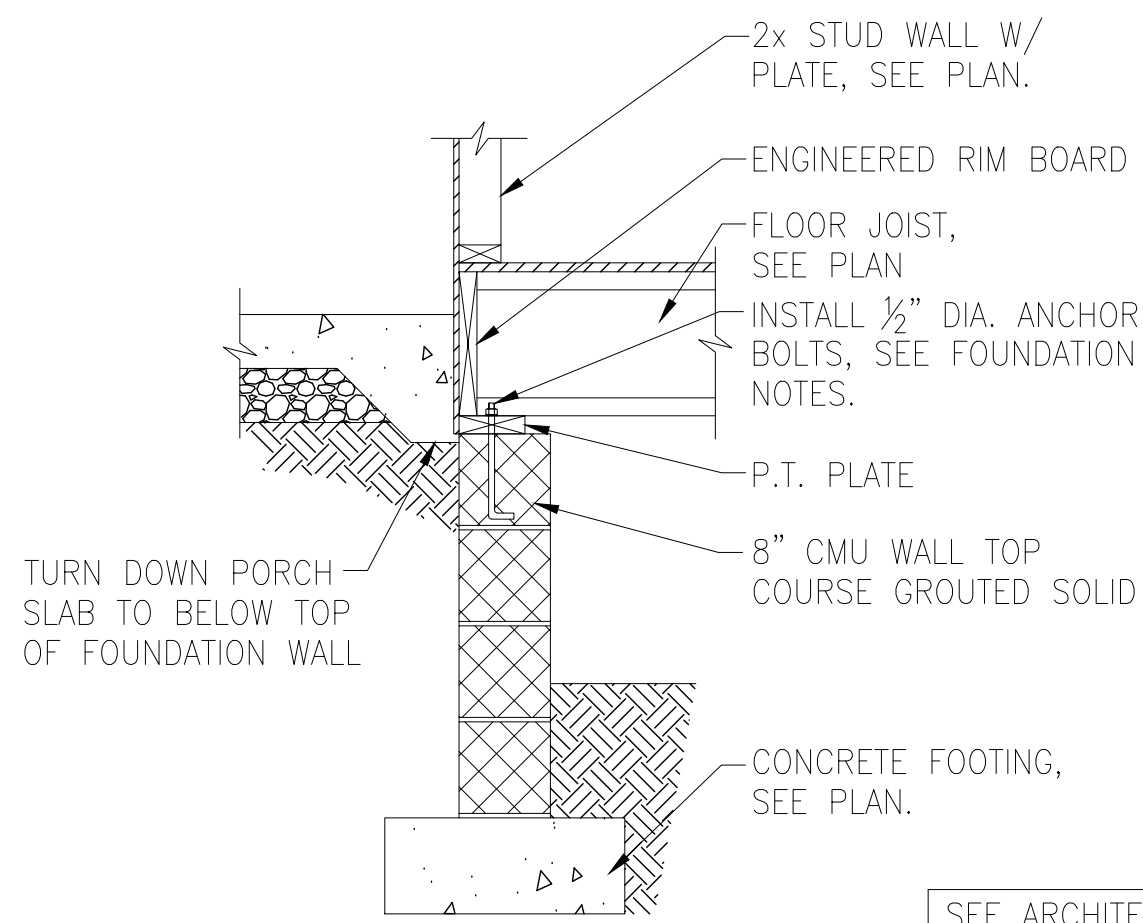
SD-6



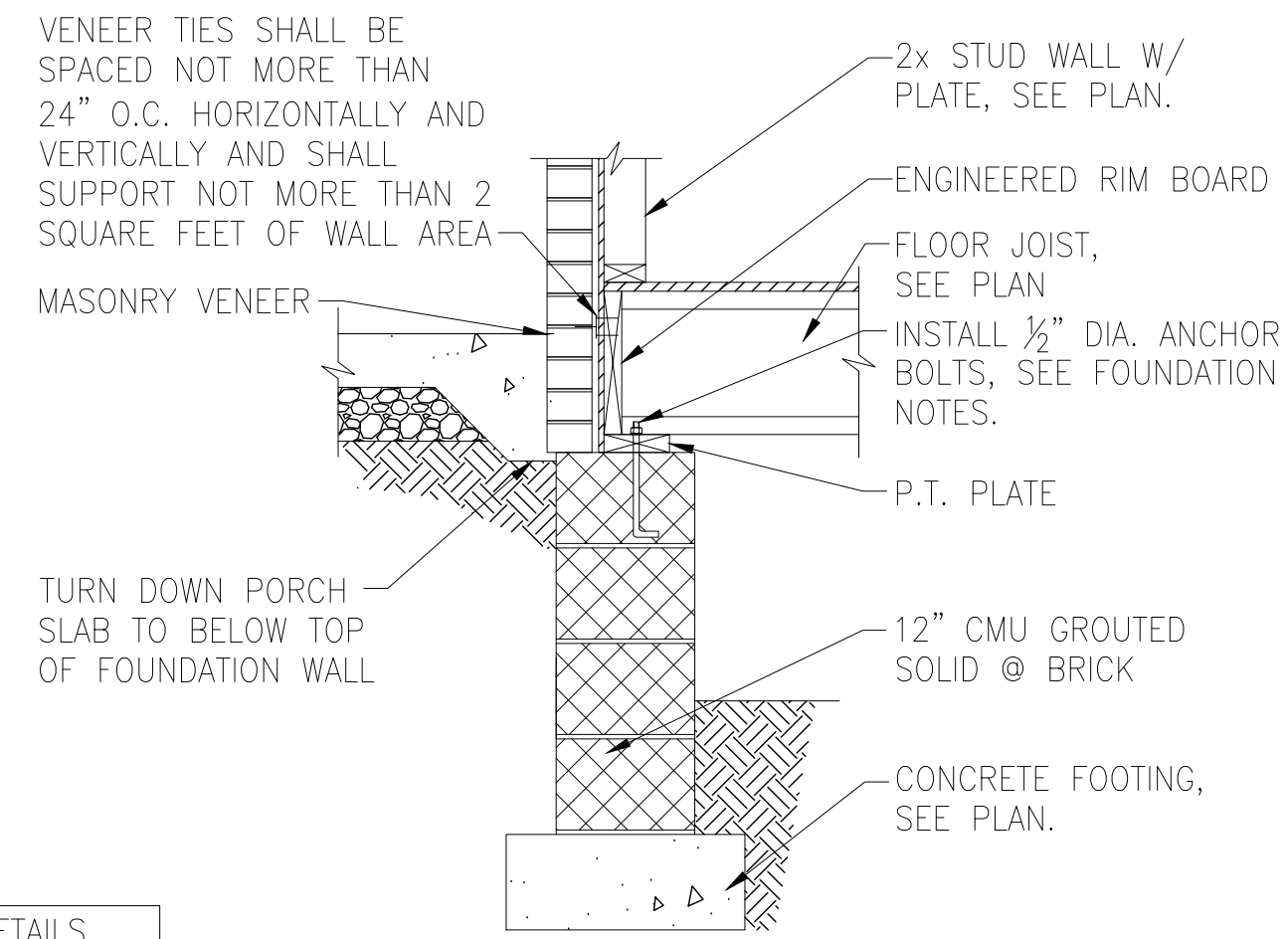
A FOUNDATION SECTION
EXTERIOR WALL



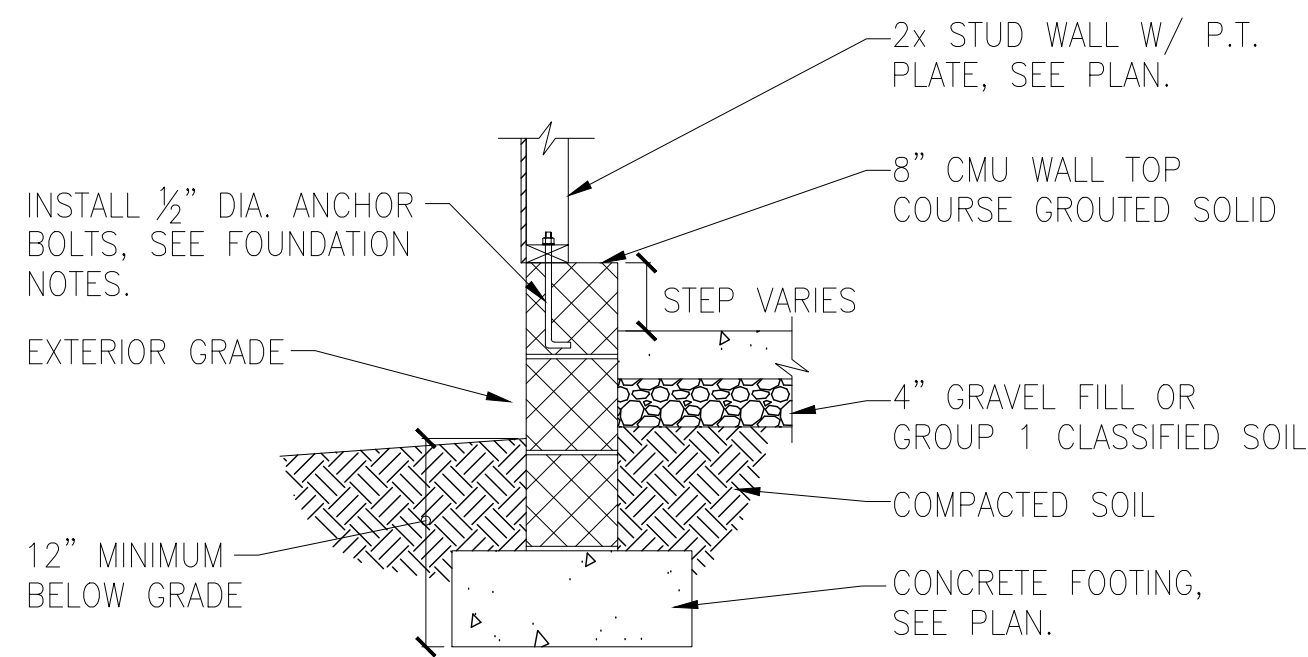
B FOUNDATION SECTION
EXTERIOR WALL @ MASONRY
VENEER



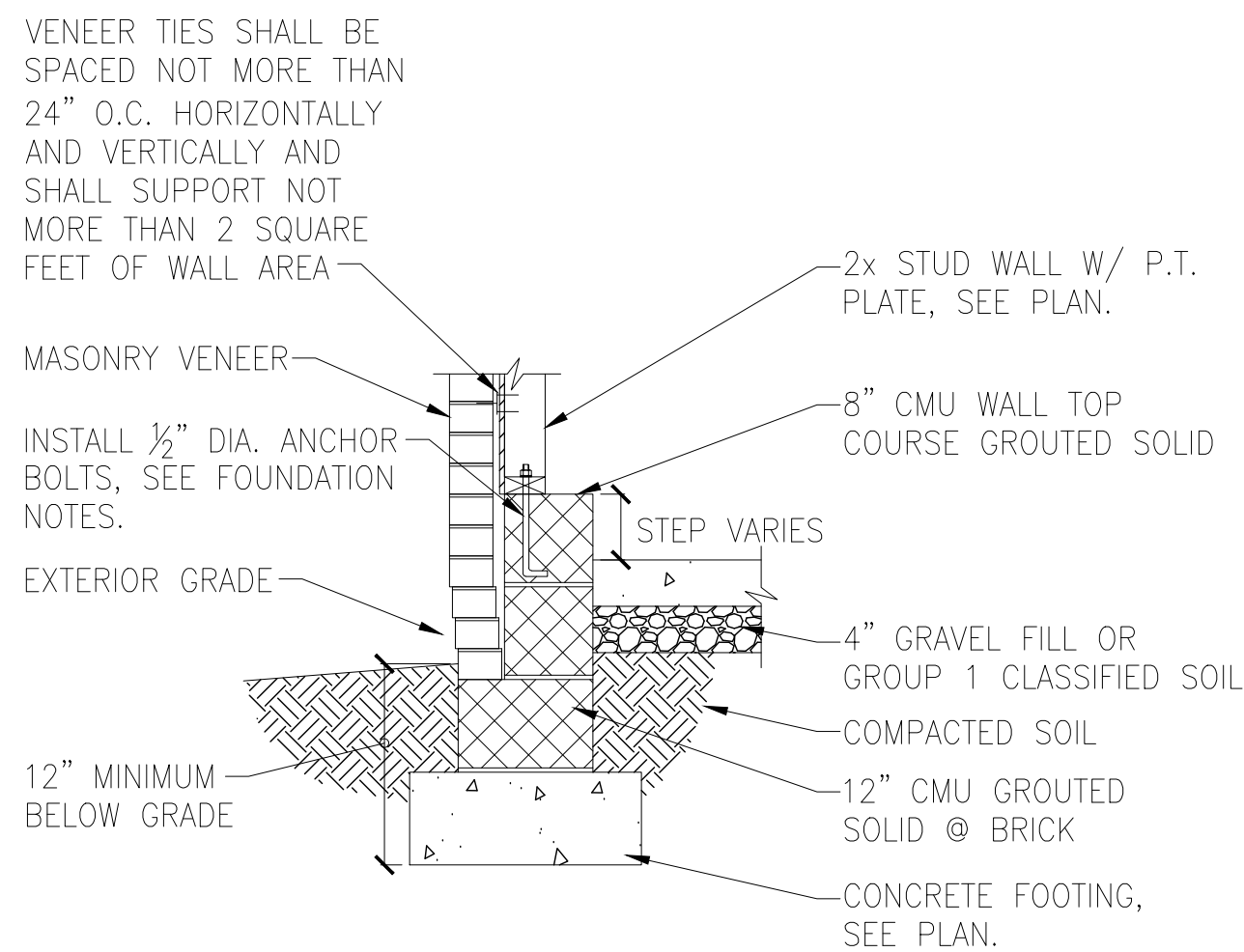
C FOUNDATION SECTION
EXTERIOR WALL AT PORCH



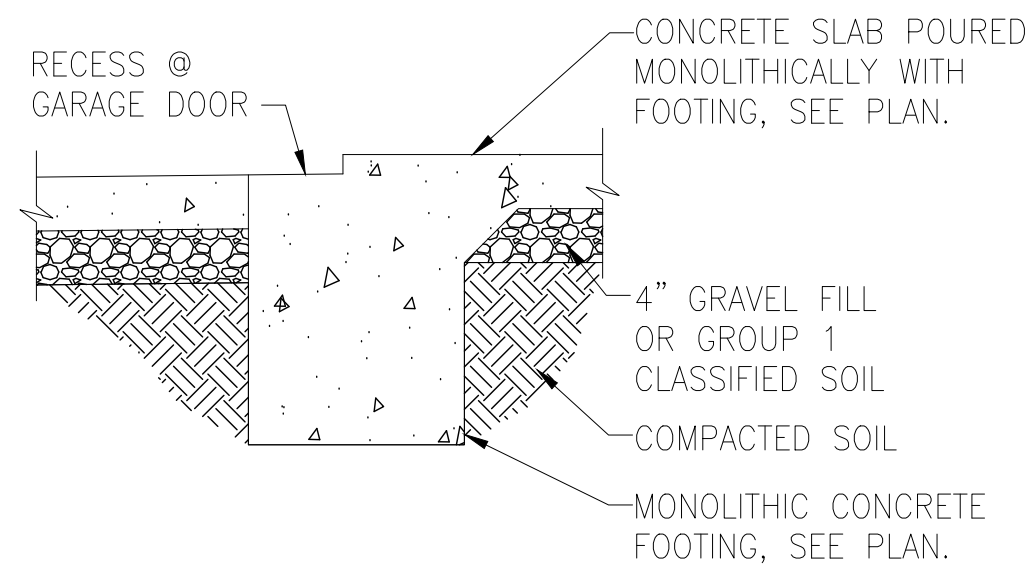
D FOUNDATION SECTION
EXTERIOR WALL AT PORCH W/ MASONRY
VENEER



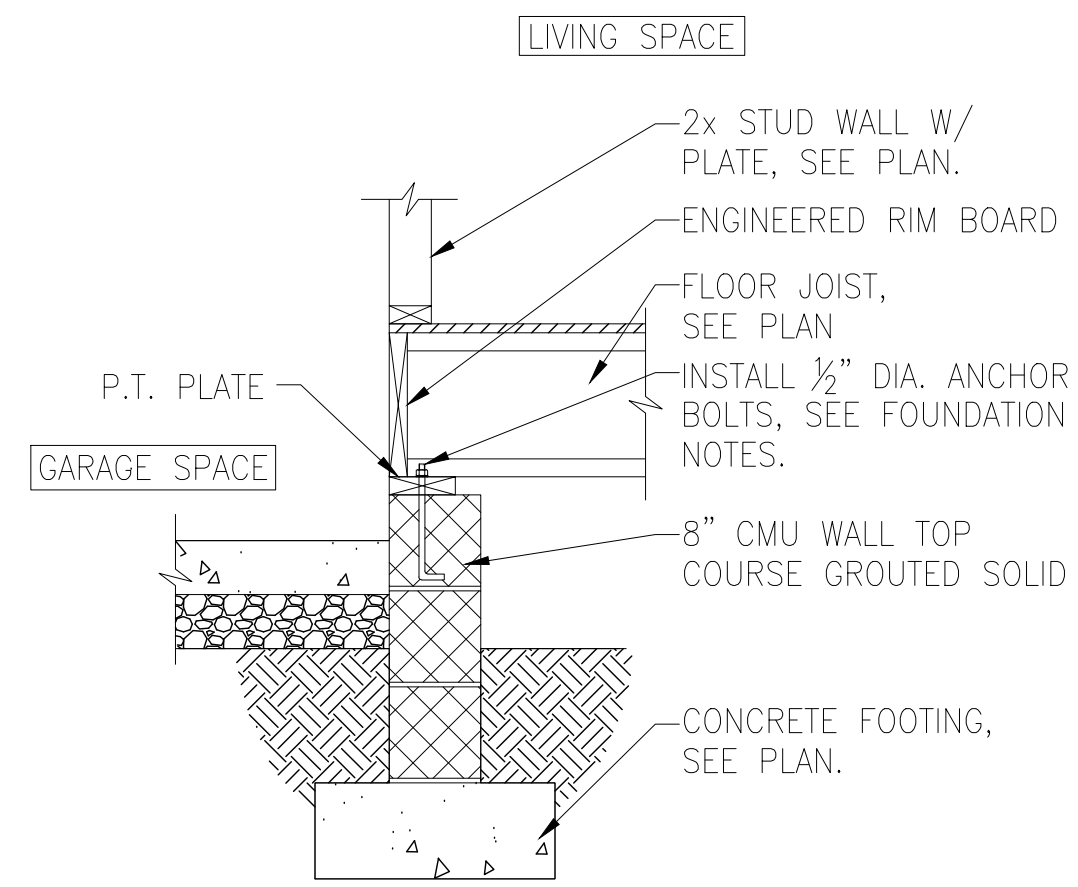
E FOUNDATION SECTION
EXTERIOR GARAGE WALL



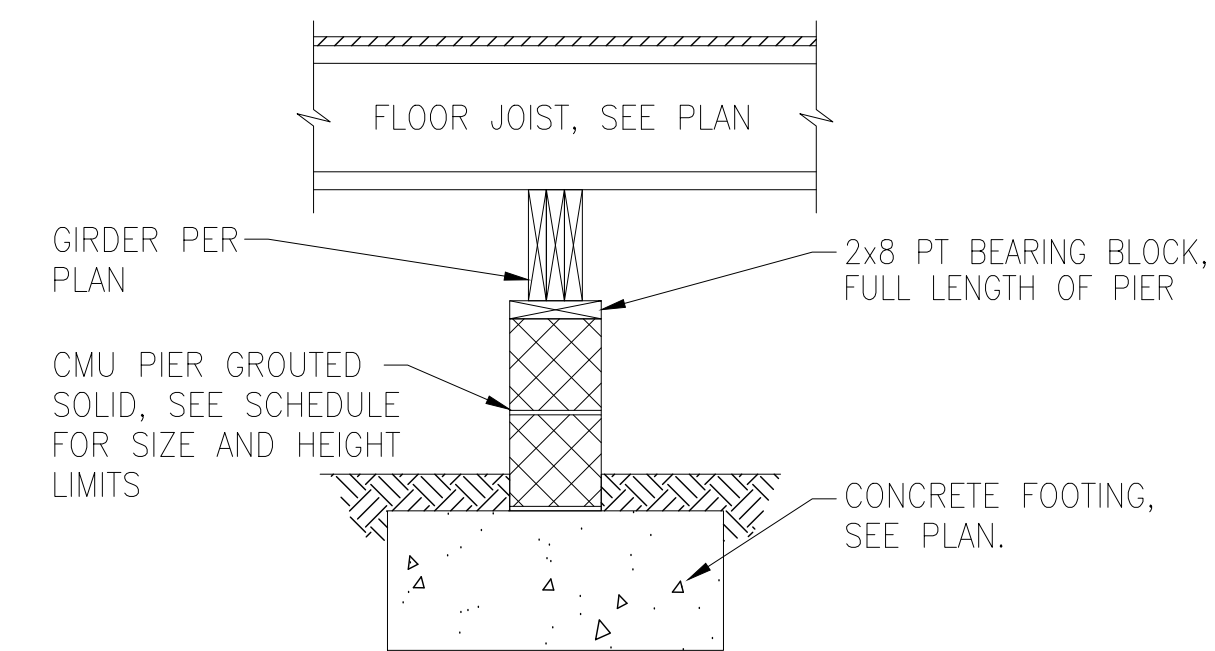
F FOUNDATION SECTION
EXTERIOR GARAGE WALL @ MASONRY
VENEER



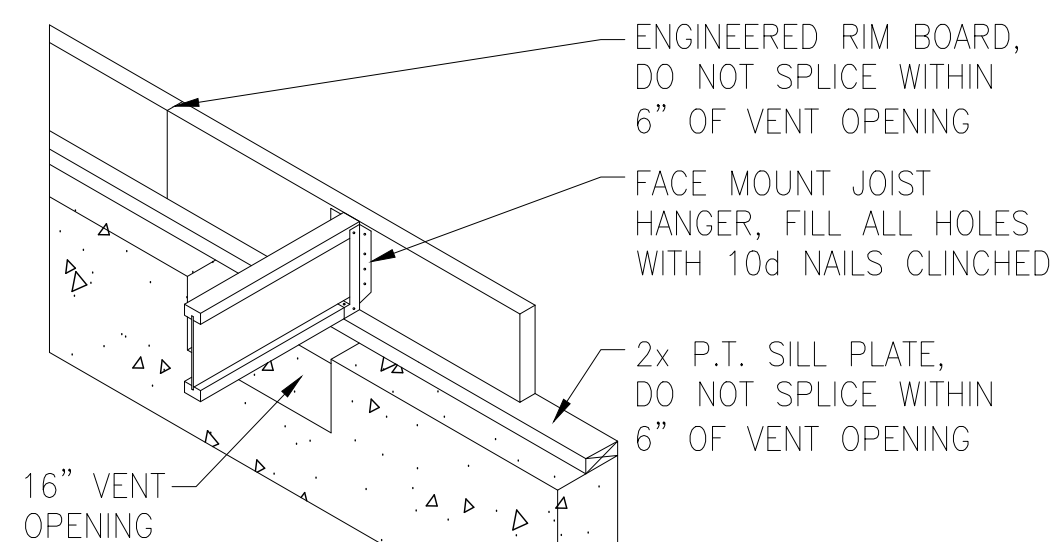
G FOUNDATION SECTION
GARAGE DOOR



H FOUNDATION SECTION
INTERIOR GARAGE WALL



J FOUNDATION SECTION
INTERIOR PIER

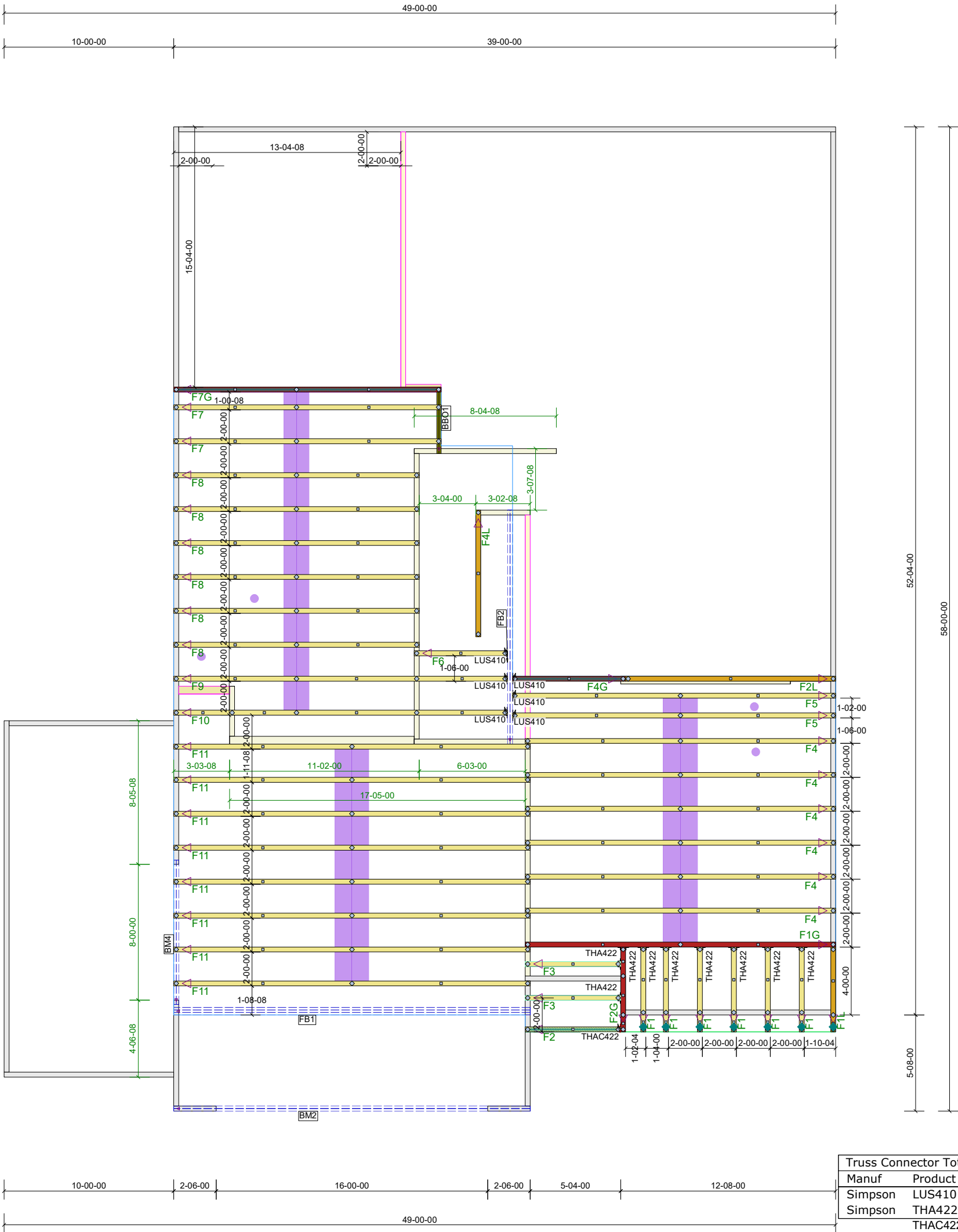


K CRAWL SPACE VENT DETAIL

PIER AND FOOTING SCHEDULE		
PIER HEIGHT	PIER SIZE	MIN. FOOTING SIZE
UP TO 2'-8"	8" x 16"	24" x 24" x 12" U.N.O.
UP TO 5'-4"	16" x 16"	24" x 24" x 12" U.N.O.
UP TO 8'-0"	16" x 16"	30" x 30" x 12" U.N.O.
NOTE: PIERS SHALL BE CAPPED WITH 8" OF SOLID MASONRY OR CONCRETE OR TOP COURSE FILLED SOLID WITH CONCRETE/MORTAR. PIERS OVER 5'-4" SHALL BE BE FILLED SOLIDLY WITH CONCRETE OR TYPE M OR S MORTAR. FOR PIERS OVER 8'-0" CONTACT KSE ENGINEERING FOR PIER AND FOOTING DESIGN.		

Tobacco Road Lot 124





Tobacco Road Lot 124

DESIGNED DATE

10/22/2025

NOT TO SCALE

Floor Truss Placement Plan

Sheet # 1 of 1

TRUSS INSTALLATION REQUIRES TEMPORARY AND PERMANENT BRACING. GENERAL GUIDANCE IS PROVIDED IN SBGA DOC's B-1 and B-3. THESE ARE INCLUDED WITH EACH JOB IN YOUR TRUSS PACKET.

DO NOT CUT, NOTCH, OR BORE HOLES UNLESS SPECIFIC WRITTEN PERMISSION IS PROVIDED BY AN AUTHORIZED REPRESENTATIVE OF 84 LUMBER.

Designer Gary Goodman

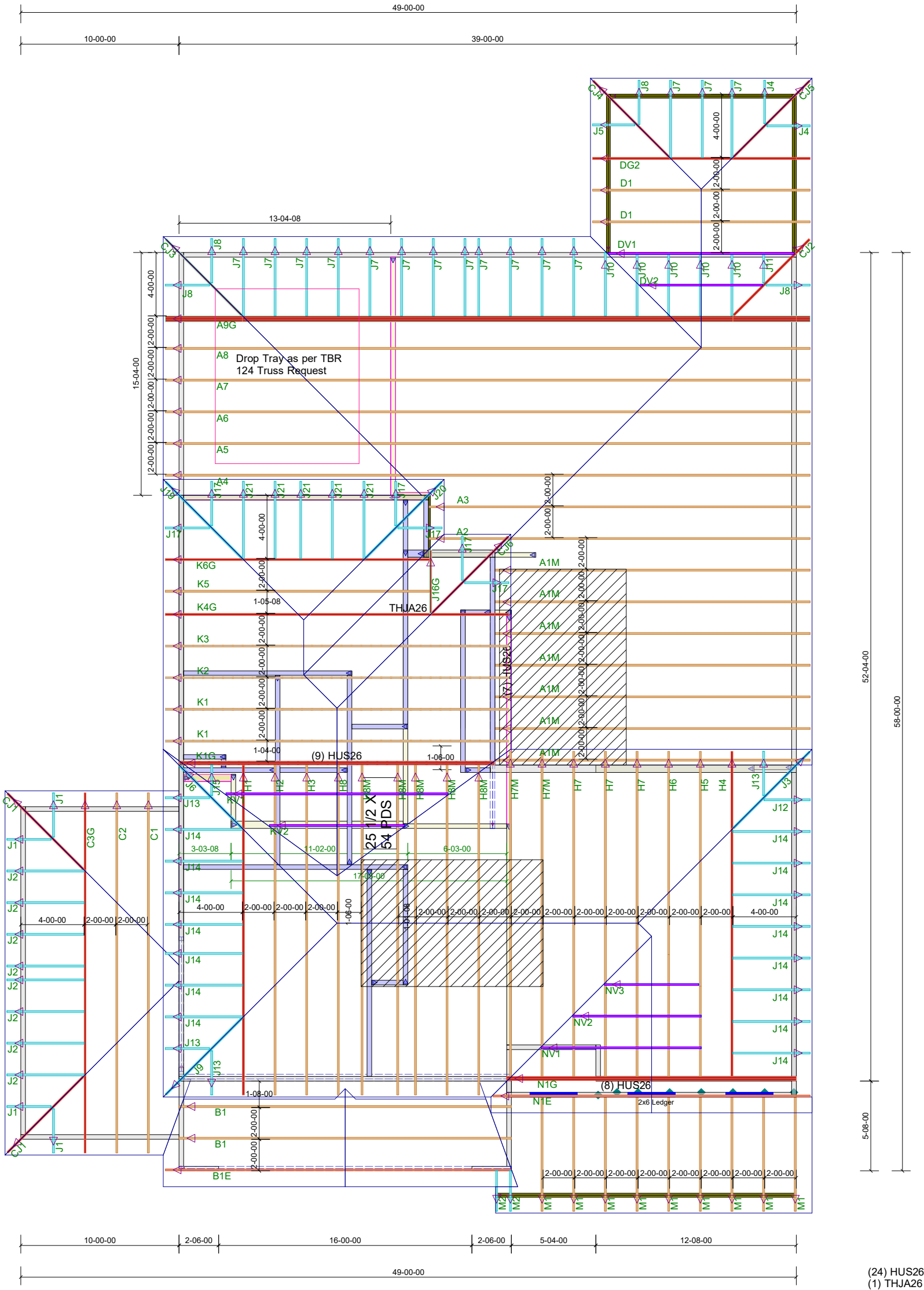
Davidson Homes

124 Tobacco Road


124 Tobacco Road - FLOOR

Job# - P05505-30627

84 Components
200 Emmett Rd
Dunn NC 28334
United States
Office: (910) 892-8400



Tobacco Road Lot 124

<div></div>		<div>84 Components 200 Emmett Rd Dunn NC 28334 United States Office: (910) 892-8400</div>	
<div>Davidson Homes</div> <div>124 Tobacco Road</div> <div>124 Tobacco Road - ROOF</div> <div>Job# - P05505-30626</div>			
Location	2383-Dunn		
Designer	Gary Goodman	<div>DO NOT CUT, NOTCH, OR BORE HOLES UNLESS SPECIFIC, WRITTEN PERMISSION IS PROVIDED BY AN AUTHORIZED REPRESENTATIVE OF 84 LUMBER.</div> <div>TRUSS INSTALLATION REQUIRES TEMPORARY AND PERMANENT BRACING. GENERAL GUIDANCE IS PROVIDED IN SBGA DOCS B-1 and B-3. THESE ARE INCLUDED WITH EACH JOB IN YOUR TRUSS PACKET.</div>	
Sheet # 1 of 1			
Roof Truss Placement Plan			
NOT TO SCALE			
DESIGNED DATE			
10/23/2025			