MAGNOLIA

ELEVATION B



3-CAR **FRONT LOAD OPTION** 4' GARAGE

EXTENSION OPTION

INCLUDED OPTIONS:

1st FLOOR **SCREENED PORCH GOURMET KITCHEN FIREPLACE** FIREPLACE W/ BUILT-INS FIXED WINDOWS @ FAMILY ROOM FLOOR RECEPTACLE @ FAMILY ROOM FLOOR RECEPTACLE @ STUDY **BOX OAK STAIRS OPEN RAIL** FRENCH DOORS @ STUDY TRAY CEILING @ DINING TRAY CEILING @ OWNERS **OWNERS DELUXE BATH** 2ND SINK @ BATH 2 TUB W/TILE ILO FG TUB @ BATH 2 **BENCH @ MUD ROOM**

2nd FLOOR **BONUS ROOM & BEDROOM 4 W/ BATH** UNFINISHED STORAGE

BASE HOUSE SQUARE FOOTAGE CALCULATIONS						TOTAL UNDER
ELEVATIONS	1st FLOOR	TOTAL FIN.	FRONT PORCH	REAR PORCH	GARAGE	ROOF
ELEV. B	2,524 s.f.	2,524 s.f.	159 s.f.	300 s.f.	396 s.f.	3,379 s.f.
OPTIONS SQUARE FOOTAGE CALCULATIONS						_
OPTIONS:						
3RD CAR GARAGE			+264 s.f.	BONUS ROOM \	+927 s.f.	
4' FRONT LOAD GARAGE EXT.			+76 s.f.	BONUS ROOM S	+124 s.f.	

GENERAL NOTES:

- SOIL BEARING CALCULATIONS BASED ON 2000 PSF MIN. REFER TO THE FOUNDATION/FOOTING SCHEDULE
- BACK FILL SHALL BE FREE FROM VEGETATION AND CONSTRUCTION DEBRIS.
- BACK FILL SHALL BE PLACED IN LIFTS AND COMPACTED IN SUCH A MANNER AS TO NOT DAMAGE THE FOUNDATION WALLS OR ANY WATERPROOFING/ DAMP PROOFING

- ALL DIMENSIONS ARE FROM FACE OF STUD TO FACE OF STUD. ALL STUDS ARE 3 1/2" UNLESS NOTED. ALL DIMENSIONS PRESENTED HERE ARE FRAME DIMENSIONS ONLY. PROVIDE 1x BLOCKING UNDER ALL EXTERIOR SLIDING DOORS.
- JOIST HANGERS, WHERE REQUIRED, SHALL BE USED WITHOUT ANGLES. INSTALL FIRE STOPPING AND/ OR DRAFT STOPPING AS REQUIRED.
- PROVIDE CUTTING, NOTCHING, NAILING REQUIREMENTS PER 2018-IRC SECTIONS R502.8 R602, R802.7.

THERMAL & MOISTURE PROTECTION

- INSTALL FIRE STOPPING AND/ OR DRAFT STOPPING AS REQUIRED.
 ATTIC VENTILATION SHALL BE PROVIDED AT 1/150th OF THE AREA OF THE SPACE VENTILATED. CROSS VENTILATION WITH HALF OF THE VENTILATED AREA SHALL BE PROVIDED BY RIDGE OR GABLE VENTS AND THE OTHER HALF BY EAVE OR CORNICE VENTS. VENTS SHALL BE PLACED SO AS TO NOT ALLOW INFILTRATION OF RAIN OR
- PROVIDE APPROVED TILE BACKER BOARD FOR ALL SHOWER AND BATH SPACE.
- PROVIDE ICE-SHIELD PER CODE.
- ROOF VENTING TO BE PROVIDED AS SHOWN. SOFFIT RIDGE AND OTHER ROOF VENTS TO BE INSTALLED AS NOTED ON THE DRAWINGS & AS PER MANUFACTURERS RECOMMENDATIONS

DOORS & WINDOW

- WINDOW CALL OUT PER PLAN. VERIFY WINDOW MANUFACTURER WITH PROJECT MANAGER
- REVIEW ALL WINDOW HEADER HEIGHTS PER PLATE HT. AND VERIFY W/ ELEVATIONS AND CORNICE DETAILS.
 TEMPERED GLASS SHALL BE USED IN ALL HAZARDOUS AREAS.
- FRONT DOOR WIDTH AS REQUIRED BY CODE.
- GARAGE DOOR AS REQUIRED BY CODE
- EMERGENCY SLEEPING ROOMS SHALL HAVE AT LEAST ONE EGRESS OPENING OF NOT LESS THAN 5.7 SF AND A CLEAR OPENING OF NOT LESS THAN 20" WIDE X 24" HIGH AND SHALL NOT BE MORE THAN 44" ABOVE THE FLOOR.

INSULATION

EXTERIOR WALLS ZONE 4: R-15 BATTS MINIMUM. VERIFY

CEILING WITH ATTIC ABOVE COMPRESSED INSULATION:

R-38 BATTS MINIMUM, VERIFY

CEILING WITH ATTIC ABOVE UNCOMPRESSED INSULATION (HEELS IN TRUSSES): R-38 BATTS MINIMUM, VERIFY

FLOOR OVER GARAGE

R-19 BATTS MINIMUM. VERIFY

ATTIC KNEEWALL:

R-19 BATTS MINIMUM. VERIFY

BUILDING CODE ANALYSIS 2018 NCRC/ 2018 IBC

APPLICABLE CODES

USER GROUP: CONSTRUCTION CLASS: HEIGHT LIMITATION: EMERGENCY ESCAPE:

SINGLE FAMILY UNPROTECTED EGRESS OR RESCUE WINDOWS FROM SLEEPING ROOM SHALL

GARAGE / HOUSE CEILING/ HOUSE ASSEMBLY: $\frac{1}{2}$ GYPSUM BD. WALL & $\frac{5}{8}$ TYPE "X" GYPSUM BD CFILING W/ 20 MINUTE

DESIGN LOAD:

SLEEPING = 30 PSF NON-SLEEPING = 40 PSF DECKS = 40 PSF

DEAD LOAD = 10 PSE BASIC WIND SPEED = 115 MPH EXPOSURE B (CHARLOTTE) STAIR LOAD = 40 PSF ROOF LIVE LOAD = 20 PSF LATERAL SOIL PRESSURE = 30 PCF

(ASSUMED)

VERIFY ALL APPLICABLE BUILDING CODES WITH STATE AND LOCAL JURISDICTION PRIOR TO CONSTRUCTION

HAVE A MINIMUM OF 5.7 SQ. FT

GARAGE/HOUSE DOOR

VENTS 128 SQ IN = (0.8889 SQ FT) 16.827 SQ FT = 60.6 VENTS REQUIRED ACTUAL CRAWL VENTS PROVIDED

NOTE: WHERE AN APPROVED VAPER BARRIER IS INSTALLED OVER GROUND SURFACE THE REQUIRED

CRAWL VENTING

2524 SQ FT OF FOUNDATION TO BE VENTED
150 SQ FT / 1 SQ FT = 16.83 SQ FT VENTILATION

THE ATTACHED PLANS & SPECIFICATIONS ARE THE SOLE PROPERTY OF DAVIDSON HOMES. ANY UNAUTHORIZED USE OF THESE PLANS WITHOUT PRIOR WRITTEN CONSENT OF DAVIDSON HOMES IS STRICTLY PROHIBITED

- MAIN STREET DESIGNS OF GEORGIA, LLC DESIGNS HOUSING AS SET FORTH BY THE FORMAT AND PROVISIONS OF THE INTERNATIONAL RESIDENTIAL CODE (IRC), AND THE NATIONAL ELECTRIC CODE (NEC).
- THESE PLANS ARE SUBJECT TO MODIFICATIONS TO MEET CODE REQUIREMENTS AND/OR TO FACILITATE MECHANICAL/ ELECTRICAL/ PLUMBING INSTALLATION AND/ OR TO
- CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL DIMENSIONS AFFECTING CONTRACTOR'S PRODUCTS, INSTALLATIONS, OR FABRICATIONS IN THE FIELD PRIOR TO EXPEDITING THE CONSTRUCTION OF SUCH WORK. FIELD VERIEVALL DIMENSIONS - DO NOT SCALE DRAWINGS. CONTRACTOR IS RESPONSIBLE FOR SURVEYING THE PROJECT AND BECOMING FAMILIAR WITH THE EXISTING CONDITIONS AND SCOPE OF WORK INCLUDING BUT NOT LIMITED TO SITE AND SOIL BEARING CONDITIONS
- ERRORS AND OMISSIONS WHICH MAY OCCUR IN THE CONTRACT DOCUMENTS SHALL BE BROUGHT TO THE ATTENTION OF MAIN STREET DESIGNS OF GEORGIA, LLC IN WRITING AND WRITTEN INSTRUCTION SHALL BE OBTAINED PRIOR TO PROCEEDING WITH CONSTRUCTION. THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY ERRORS, DISCREPANCIES, OR OMISSIONS FOR WHICH THE CONTRACTOR FAILED TO NOTIFY MAIN STREET DESIGNS OF GEORGIA, LLC PRIOR TO CONSTRUCTION AND/ OR FABRICATION OF
- FLAME SPREAD AND SMOKE DENSITY NOTES

WALLS AND CEILING

WALL AND CEILING FINISHES SHALL HAVE A FLAME - SPREAD CLASSIFICATION OF NOT GREATER THAN 200. WALL AND CEILING FINISHES SHALL HAVE A SMOKE-DEVELOPED

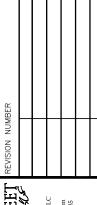
IF BATT OR BLANKET INSULATION, INCLUDING FACINGS SUCH AS VAPOR RETARDERS OR OTHER VAPOR PERMEABLE MEMBRANES ARE LEFT EXPOSED (IN AREAS LIKE UNFINISHED BASEMENTS), THE MATERIAL SHALL HAVE A FLAME SPREAD RATING OF 25 OR LESS AND A SMOKE DEVELOPMENT RATING OF 450 OR LESS. FLAME-SPREAD AND SMOKE-DEVELOPMENT LIMITATIONS DO NOT APPLY TO FACINGS THAT IS INSTALLED IN SUBSTANTIAL CONTACT WITH THE UNEXPOSED SURFACE OF THE CEILING, FLOOR, OR

EXCEPT WHERE OTHERWISE NOTED IN SECTION R314.2 ALL FOAM PLASTIC OR FOAM PLASTIC CORES IN MANUFACTURED ASSEMBLIES USED IN BUILDING CONSTRUCTION SHALL HAVE A FLAME-SPREAD RATING OF NOT MORE THAN 75 AND SHALL HAVE A SMOKE-DEVELOPMENT RATING OF NOT MORE THAN 450 WHEN TESTED IN THE MAXIMUM THICKNESS INTENDED FOR USE IN ACCORDANCE WITH ASTM E 84.

R314.1.2 THERMAL BARRIER. FOAM PLASTIC, EXCEPT WHERE OTHERWISE NOTED, SHALL BE SEPARATED FROM THE INTERIOR OF A BUILDING BY MINIMUM1/2-INCH (12.7 MM) GYPSUM BOARD OR AN APPROVED FINISH MATERIAL EQUIVALENT TO A THERMAL BARRIER TO LIMIT THE AVERAGE TEMPERATURE RISE OF THE UNEXPOSED SURFACE TO NO MORE THAN 250°F(121°C) AFTER 15MINUTES OF FIRE EXPOSURE TO THE ASTM E 119 STANDARD TIME TEMPERATURE CURVE. THE GYPSUM BOARD SHALL BE INSTALLED USING A MECHANICAL FASTENING SYSTEM IN ACCORDANCE WITH SECTIOR702.3.5. RELIANCE ON ADHESIVES TO ENSURE THAT THE GYPSUM BOARD WILL REMAIN IN PLACE WHEN EXPOSED TO FIRE SHALL BE PROHIBITED.

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BASE HOUSE SQUARE FOOTAGE CALCULATIONS					TOTAL UNDER	
ELEVATIONS	1st FLOOR	TOTAL FIN.	FRONT PORCH	REAR PORCH	GARAGE	ROOF
ELEV. B	2,524 s.f.	2,524 s.f.	159 s.f.	300 s.f.	396 s.f.	3,379 s.f.
	OPTIONS SQUARE FOOTAGE CALCULATIONS					
OPTIONS:						
3RD CAR GARAGE			+264 s.f.	BONUS ROOM W/ BEDROOM		+927 s.f.
4' FRONT LOAD GARAGE EXT.			+76 s.f.	BONUS ROOM S	STORAGE	+124 s f







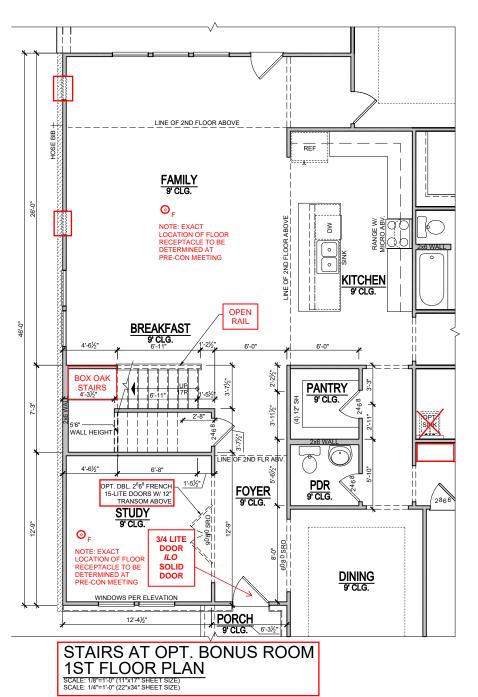
1/8"=1'-0' 60

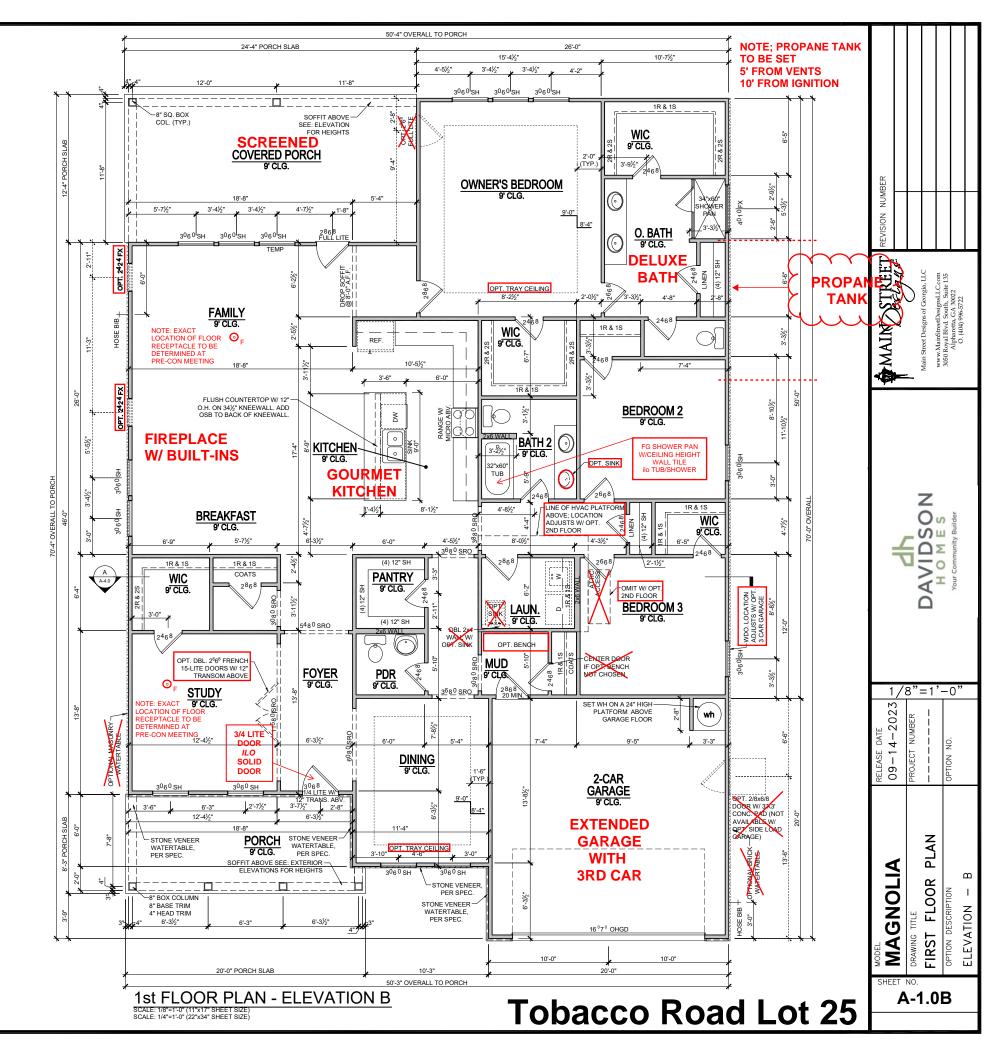
MAGNOL

COVER

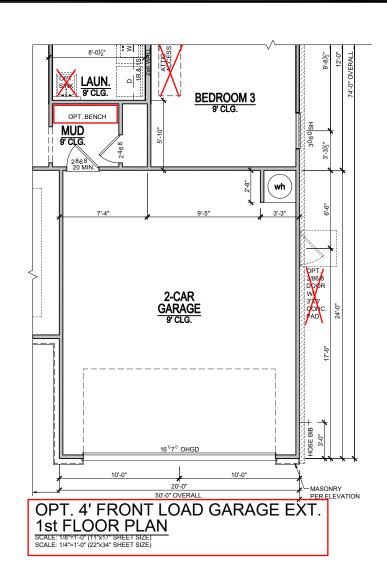
CS-1.0

SEE PAGE A-1.1B FOR OPTIONS:
SCREENED PORCH
3-CAR GARAGE
4' GARAGE EXTENSION
GOURMET KITCHEN
FIREPLACE
DELUXE BATH
&
BENCH DETAIL

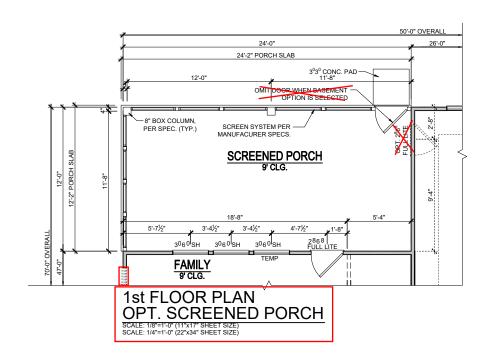


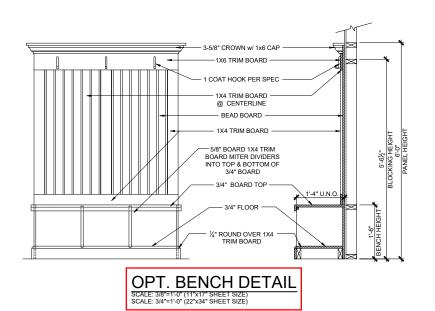


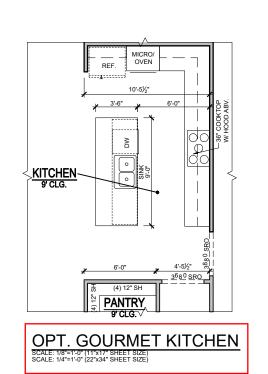
LAUN. 9' CLG. BEDROOM 3 9' CLG. OPT. BENCH MUD 9' CL.G. 3RD-CAR GARAGE 9'CLG. 8⁰7⁰ OHGD OPT. 3RD CAR GARAGE 1st FLOOR PLAN SCALE: 1/8"=1'-0" (11"x17" SHEET SIZE) SCALE: 1/4"=1'-0" (22"x34" SHEET SIZE)

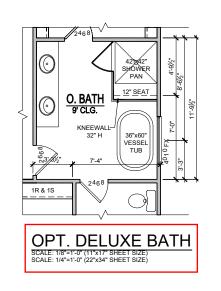


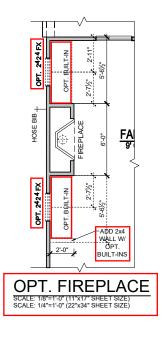
Tobacco Road Lot 25



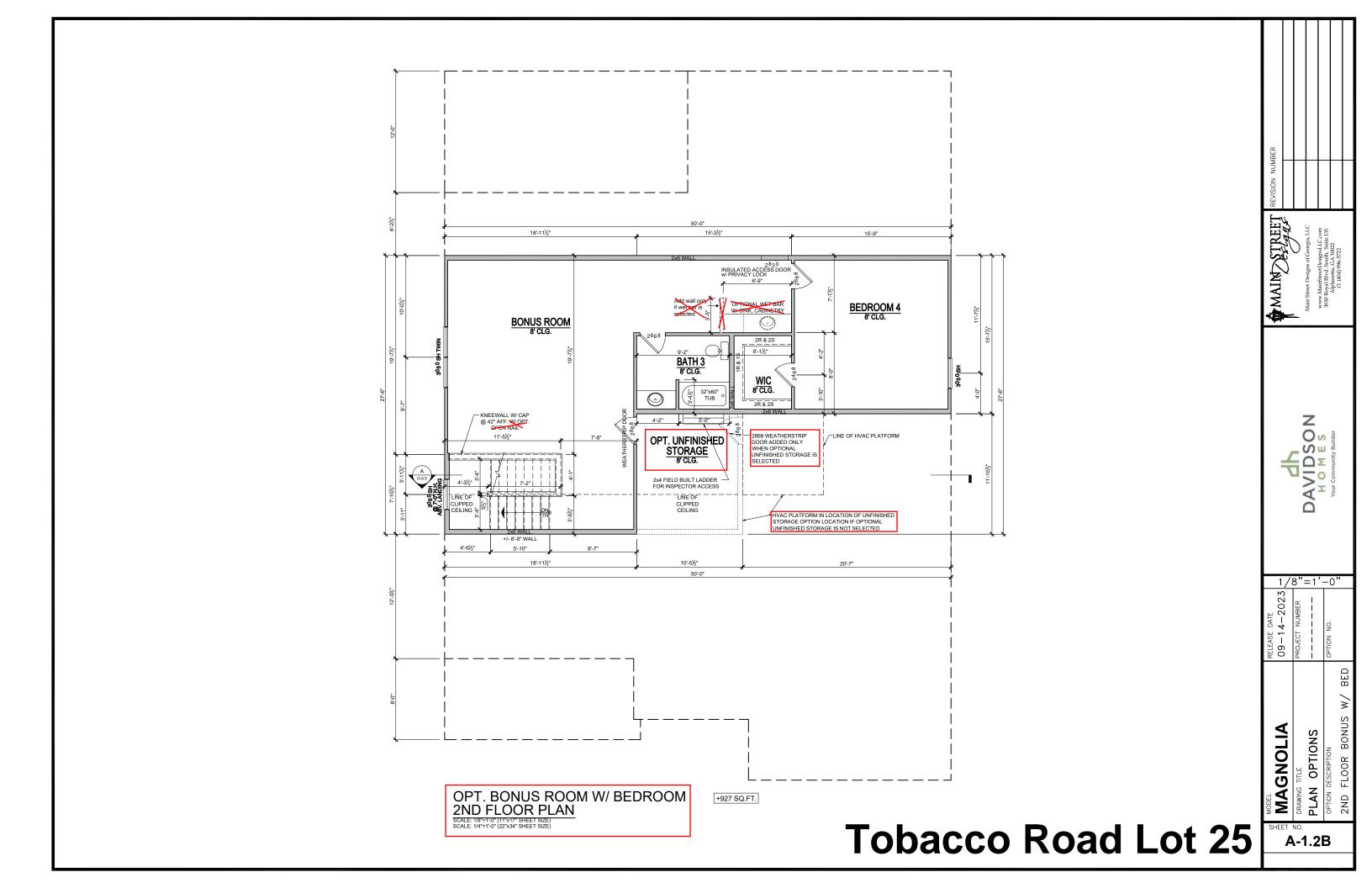


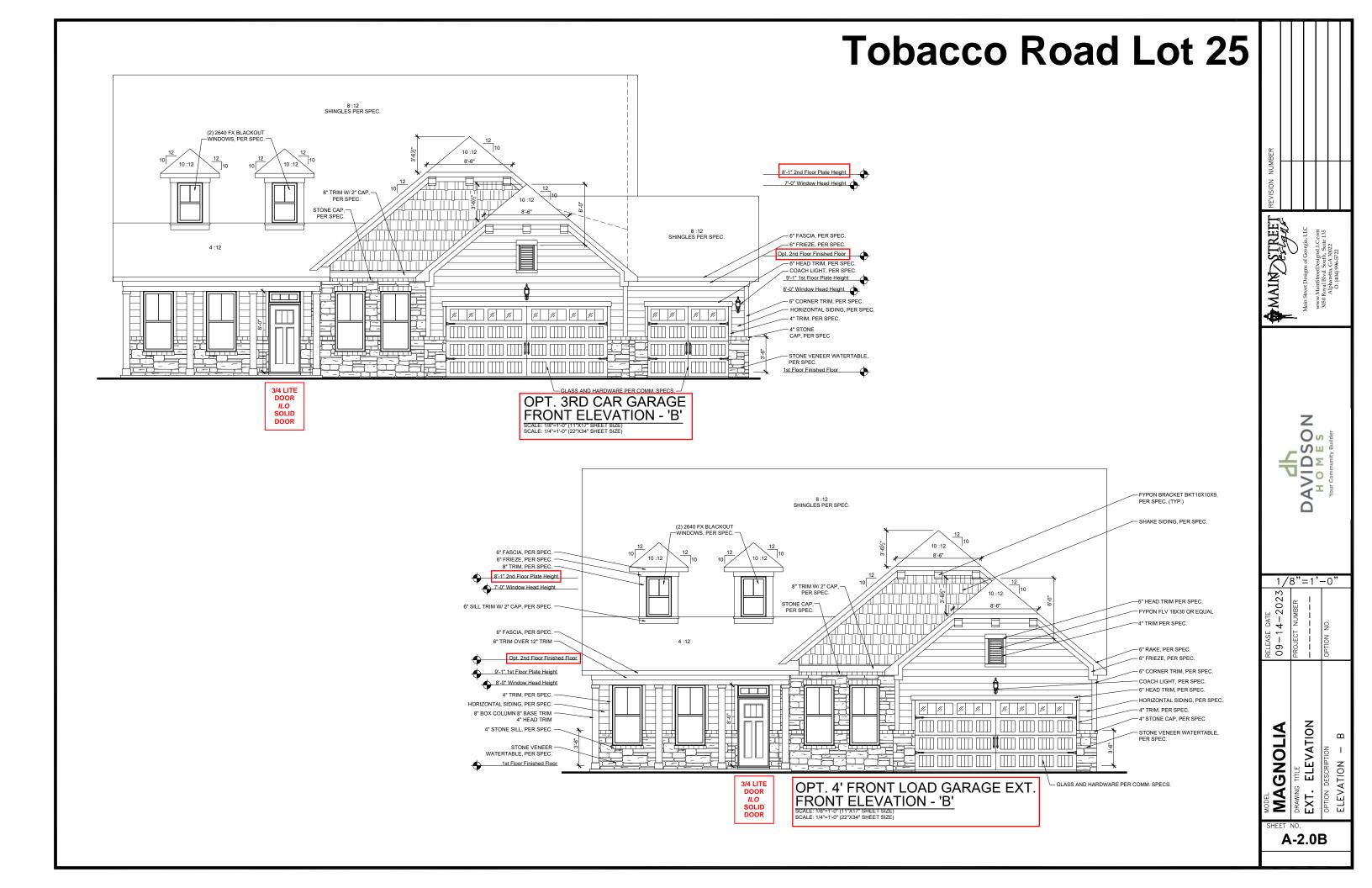






SHE	MODEL	RELEASE DATE		MAIN STREET REVISION NUMBER	REVISION NUMBER
Δ		/ 6707 +- 60		Certains	
NO.	DRAWING TITLE	PROJECT NUMBER		ک -	
	FIRST FLOOR PLAN	= 1 · · · · · · · · · · · · · · · · · ·	DAVIDSON	Main Street Designs of Georgia, LLC	
	OPTION DESCRIPTION	OPTION NO.	Your Community Builder	3050 Royal Blvd. South, Suite 135 Alpharetta, GA 30022	
	ELEVATION - B OPTS			O. (404) 996-5722	





Tobacco Road Lot 25 8 :12 SHINGLES PER SPEC. 8 :12 SHINGLES PER SPEC. 6" FASCIA, PER SPEC. -6" TRIM OVER 12" TRIM 6" RAKE, PER SPEC. — 6" FRIEZE, PER SPEC. — 9'-1" 1st Floor Plate Height 9'-1" 1st Floor Plate Height 8'-0" Window Head Height 8'-0" Window Head Height - SCREEN SYSTEM PER MANUFACURER SPECS HORIZONTAL SIDING, PER SPEC. -4" CORNER TRIM, PER SPEC. — - 8" BOX COLUMN, PER SPEC. (TYP.) 1st Floor Finished Floor 1st Floor Finished Floor

OPT. 3RD CAR GARAGE REAR ELEVATION

SCALE: 1/8"=1'-0" (11"X17" SHEET SIZE) SCALE: 1/4"=1'-0" (22"X34" SHEET SIZE) SCREENED PORCH REAR ELEVATION SCALE: 1/8"=1'-0" (11"X17" SHEET SIZE) SCALE: 1/4"=1'-0" (22"X34" SHEET SIZE) MAIN STREET DAVIDSON HOMES HOMES 1/8"=1'-0" RELEASE DATE

09-14-2023 - PROJECT NUMBER DRAWING TITLE

EXT. ELEVATION MAGNOLIA A-2.1B

10:12

ELEVATION -B- ROOF PLAN

SCALE: 1/16"=1'-0" (11"X17" SHEET SIZE) SCALE: 1/8"=1'-0" (22"X34" SHEET SIZE)

ATTIC VENT CALCULATIONS

- GENERAL CONTRACTOR SHALL VERIEV THE NET FREE VENTILATION OF THE VENT PRODUCT SELECTED BY OWNER VENILATION OF THE VENT PRODUCT SELECTED BY OWNER
 PERFY WITH MANUFACTURER OF HIGH AND LOW VENTS
 TO BE USED FOR MINIMUM CALCULATED VENTS REQUIRED.
 THE REQUIRED VENTILATION SHALL BE MAINTAINED.
 PROVIDE INSULATION STOP SUCH THAT INSULATION
 DOES NOT OBSTRUCT FREE AIR MOVEMENT AS REQUIRED
 BY THE BUILDING OFFICIAL.

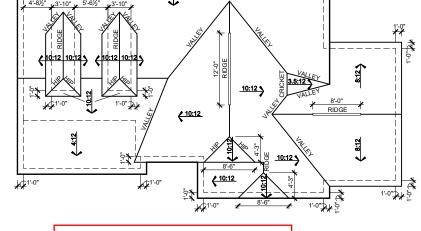
 ALL DIVERSIDA EDAMED FOOCA ADEAS SHALL HAVE
- ALL OVERLAP FRAMED ROOF AREAS SHALL HAVE
- OPENINGS BETWEEN THE ADJACENT ATTICS IN THE ROOF SHEATHING (AS ALLOWED BY THE STRUCTURAL ENGINEER) TO ALLOW PASSAGE AND ATTIC VENTILATION BETWEEN THE TWO OR ISOLATED ATTIC SPACES SHALL BE VENTED INDEPENDENTLY TO CBC REQUIREMENTS.
- PER DEVELOPER, AT ALL CANTILEVERED FLOORS, CANTILEVERED ARCHITECTURAL POP-OUTS, AND ANY DOUBLE FRAMING PROJECTIONS THAT ARE SEPARATED FROM THE VENTING CALCULATIONS SHOWN ABOVE, PROVIDE A CONTINUOUS 2" CORROSION RESISTANT SOFFIT VENT AT UNDERSIDE OF FRAMED ELEMENT.
- ALL ROOF DRAINAGE SHALL BE PIPED TO STREET OR APPROVED DRAINAGE FACILITY.
- DASHED LINES INDICATE WALL BELOW.
- LOCATE GUTTER AND DOWNSPOUTS PER BUILDER.
- PITCHED ROOES AS NOTED
- TRUSS MANUFACTURER SHALL SUBMIT STRUCTURAL CALCS AND SHOP DRAWINGS TO THE BUILDER'S GENERAL CONTRACTOR AND BUILDING DEPARTMENT FOR REVIEW PRIOR TO FABRICATIONS
- ALL PLUMBING VENTS SHALL BE COMBINED INTO A MINIMUM AMOUNT OF ROOF PENETRATIONS. ALL ROOF PENETRATIONS SHALL OCCUR TO THE REAR OF THE MAIN RIDGE

MAIN ROOF

3359 SQ FT UNDER ROOF ATTIC
300 SQ FT / 1 SQ FT = 11.20 SQ FT VENTILATION

5.598 SQ FT 0.125 SQ FT SOFFIT VENT

FACADE PERCEN	TAGE	S		
MATERIALS	S.F.	%		
IDING	186	45		
HAKE	85	21		
OARD & BATT	0	0		
IARDI BOARD	0	0		
TONE VENEER	142	34		
RICK VENEER	0	0		
TOTAL= 413 100				
MASONRY % = 34				



OPT. 3RD CAR GARAGE **ELEVATION B - ROOF PLAN** SCALE: 1/16"=1'-0" (11"X17" SHEET SIZE SCALE: 1/8"=1'-0" (22"X34" SHEET SIZE)

ATTIC VENT CALCULATIONS

MAIN ROOF

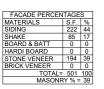
3623 SQ FT UNDER ROOF ATTIC
300 SQ FT / 1 SQ FT = 12.08 SQ FT VENTILATION

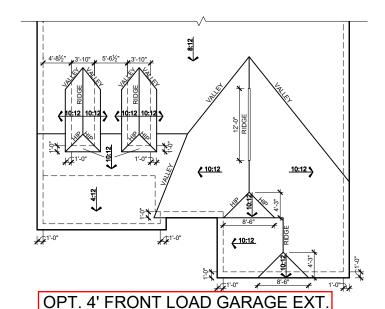
RIDGE VENTS 18 SQ IN = (.125 SQ FT) SOFFIT VENTS 9 SQ IN = (.0625 SQ FT) BOX VENTS 50 SQ IN = (.347 SQ FT)

RIDGE VENT 6.038 SQ FT = 48.3 FEET OF RIDGE VENT

6.038 SQ FT = 96.6 FEET OF SOFFIT VENT

ACTUAL RIDGE VENT PROVIDED ACTUAL SOFFIT VENT PROVIDED NUMBER OF BOX VENTS NEEDED (REQ - ACTUAL x .347) -2.7 COUNT (NEGATIVE = 0)





Tobacco Road Lot 25

ATTIC VENT CALCULATIONS

ELEVATION B - ROOF PLAN

MAIN ROOF

3439 SQ FT UNDER ROOF ATTIC 300 SQ FT / 1 SQ FT = 11.46 SQ FT VENTILATION

RIDGE VENTS 18 SQ IN = (.125 SQ FT) SOFFIT VENTS 9 SQ IN = (.0625 SQ FT) BOX VENTS 50 SQ IN = (.347 SQ FT)

SCALE: 1/16"=1'-0" (11"X17" SHEET SIZE SCALE: 1/8"=1'-0" (22"X34" SHEET SIZE)

11.46 SQ FT x 50% 5.732 SQ FT OF RIDGE 11.46 SQ FT x 50% 5.732 SQ FT OF SOFFIT

RIDGE VENT

5.732 SQ FT = 45.9 FEET OF RIDGE VENT

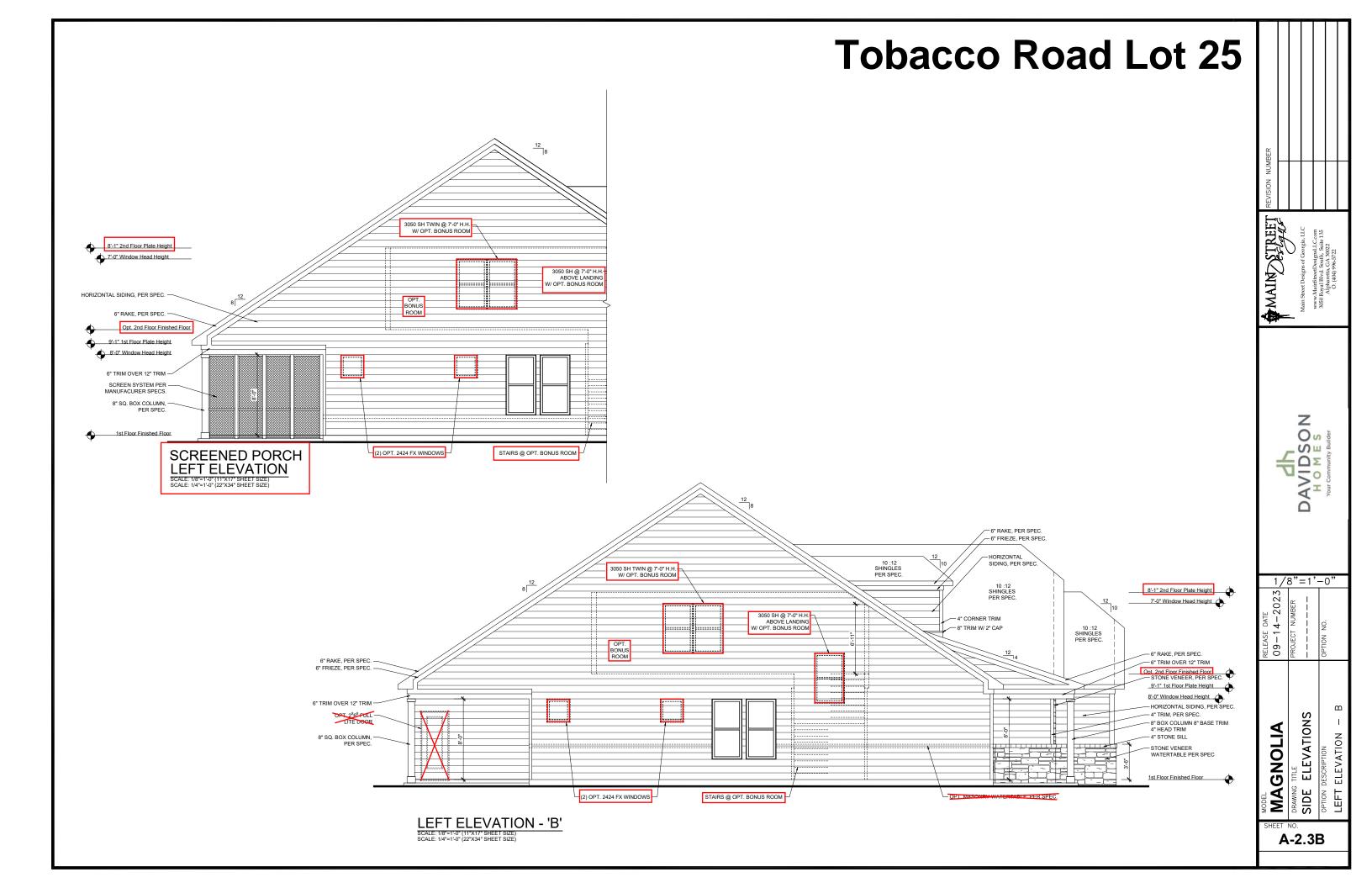
0.125 SQ FT

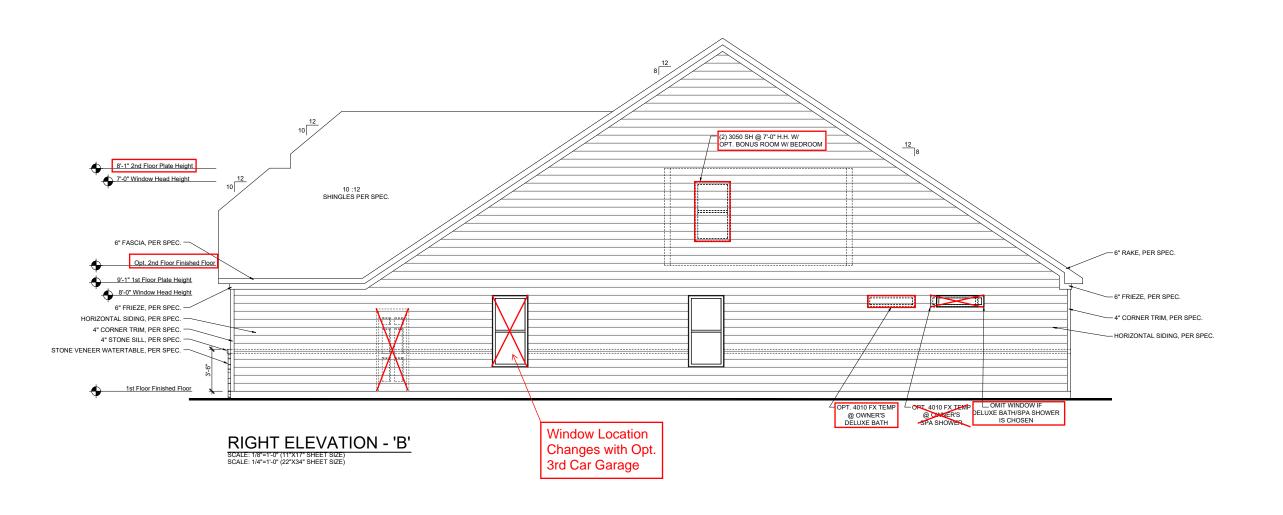
ACTUAL RIDGE VENT PROVIDED ACTUAL SOFFIT VENT PROVIDED NUMBER OF BOX VENTS NEEDED (REQ - ACTUAL x .347)

FACADE PERCENTAGES					
ATERIALS	S.F.	%			
DING	186	45			
HAKE	85	21			
DARD & BATT	0	0			
ARDI BOARD	0	0			
TONE VENEER	142	34			
RICK VENEER	0	0			
TOTAL= 413 100					
MASONRY % = 34					

Z Ou SI ΩΣ 1/8"=1'-0"960 09 MAGNOLIA PLAN ROOF

A-2.2B

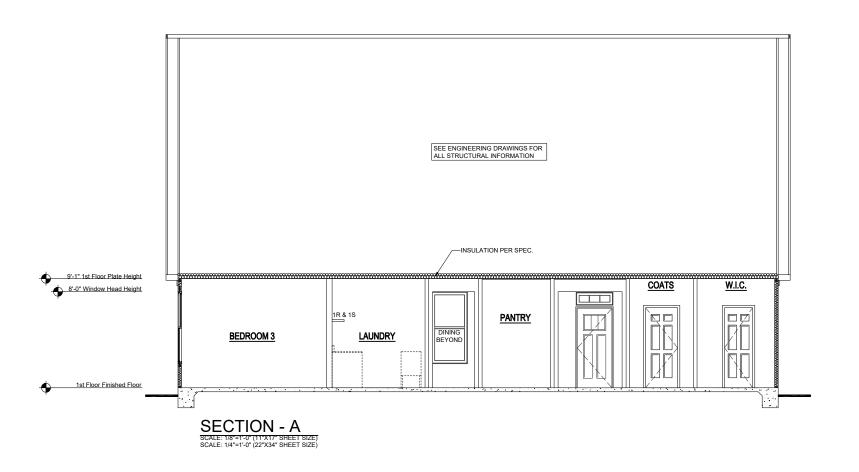




MAIN STREET DAVIDSON HOMES 1/8"=1'-0" RELEASE DATE 09-14-2023 SIDE ELEVATIONS MAGNOLIA

A-2.4B

Tobacco Road Lot 25 10 :12 SHINGLES PER SPEC. (2) 3050 SH @ 7'-0" H.H. W/ OPT. BONUS ROOM W/ BEDRO 8'-1" 2nd Floor Plate Height MAIN STREET 6" FASCIA, PER SPEC. -Opt. 2nd Floor Finished Floor 8'-0" Window Head Height 6" FRIEZE, PER SPEC. -HORIZONTAL SIDING, PER SPEC. -4" CORNER TRIM, PER SPEC. 4" STONE SILL, PER SPEC. STONE VENEER WATERTABLE, PER SPEC. OPT. 3RD CAR GARAGE RIGHT ELEVATION - 'B' Window Location DAVIDSON HOMES Changes with Opt. SCALE: 1/8"=1'-0" (11"X17" SHEET SIZE) SCALE: 1/4"=1'-0" (22"X34" SHEET SIZE) 3rd Car Garage (2) 3050 SH @ 7'-0" H.H. W/ OPT. BONUS ROOM W/ BEDRO 8'-1" 2nd Floor Plate Height 1/8"=1'-0" RELEASE DATE 09-14-2023 7'-0" Window Head Height 10 :12 SHINGLES PER SPEC. 6" FASCIA, PER SPEC. -Opt. 2nd Floor Finished Floo 9'-1" 1st Floor Plate Height 8'-0" Window Head Height 6" FRIEZE, PER SPEC. HORIZONTAL SIDING, PER SPEC. ELEVATIONS 4" CORNER TRIM, PER SPEC. MAGNOLIA 4" STONE SILL, PER SPEC. STONE VENEER WATERTABLE, PER SPEC. 1st Floor Finished Floor SIDE OPT. 4' FRONT LOAD GARAGE EXT. RIGHT ELEVATION - 'B' Window Location Changes with Opt. 3rd Car Garage A-2.5B





MAGNOLIA

A-3.0B

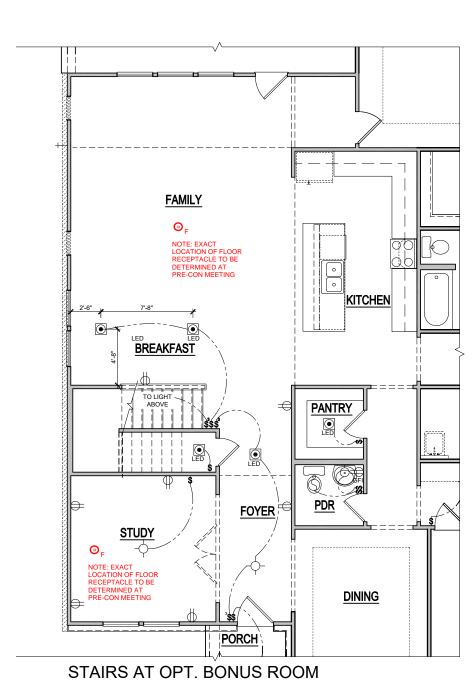


MAINDSTREET DAVIDSON HOMES

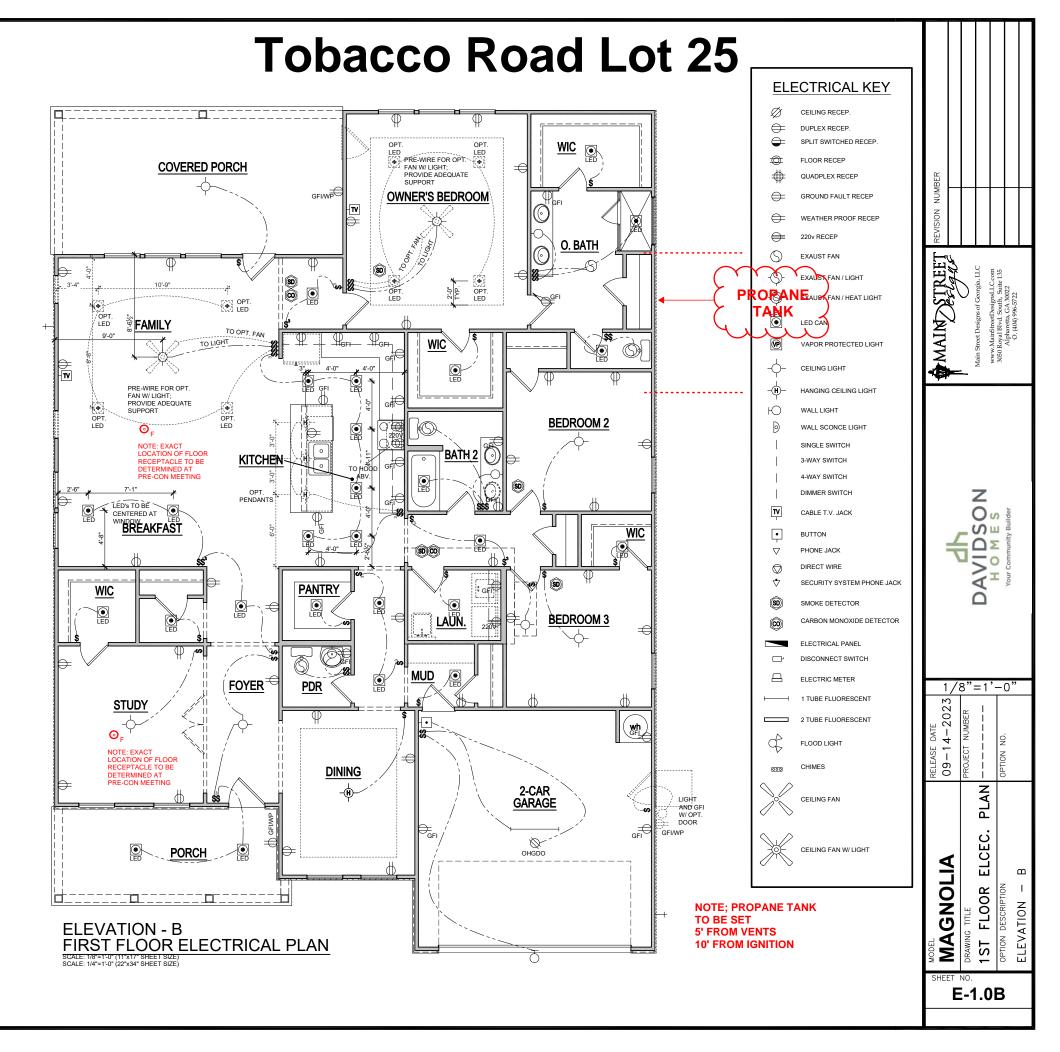
MAGNOLIA

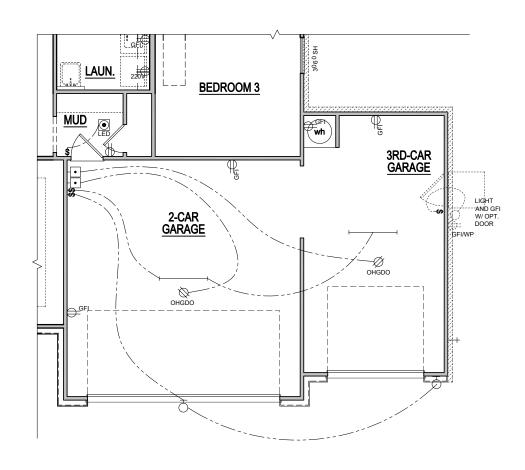
SECTION

A-3.1B

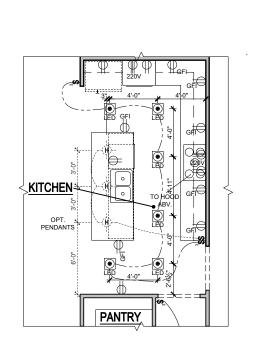


STAIRS AT OPT. BONUS ROOM ELECTRICAL PLAN
SCALE: 1/8°=1'-0' (11*x17' SHEET SIZE)
SCALE: 1/4*-1'-0' (22*-44' SHEET SIZE)

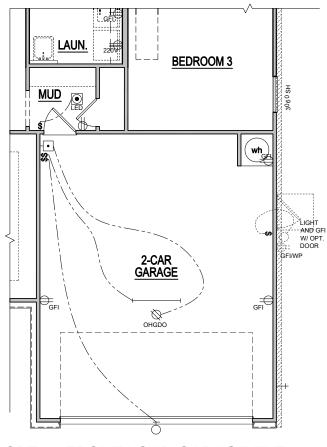




OPT. 3RD CAR GARAGE
1st FLOOR ELECTRICAL PLAN
SCALE: 18"=1-0" (12"X94" SHEET SIZE)



OPT. GOURMET KITCHEN
SCALE: 1/8"=1-0" (12"x1/" SHEET SIZE)
SCALE: 1/4"=1-0" (22"x34" SHEET SIZE)

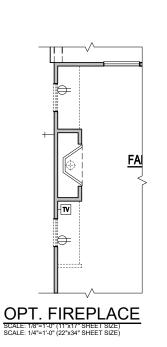


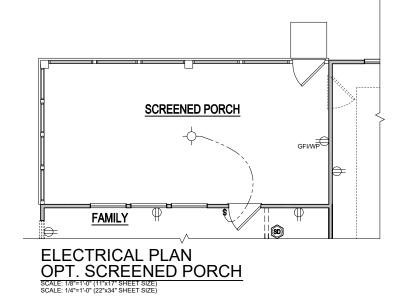
OPT. 4' FRONT LOAD GARAGE EXT.

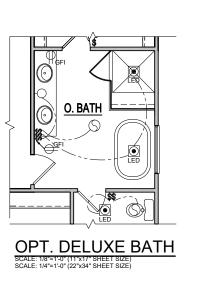
1st FLOOR ELECTRICAL PLAN

SCALE: 1/8"=1"-0" (22"x34" SHEET SIZE)

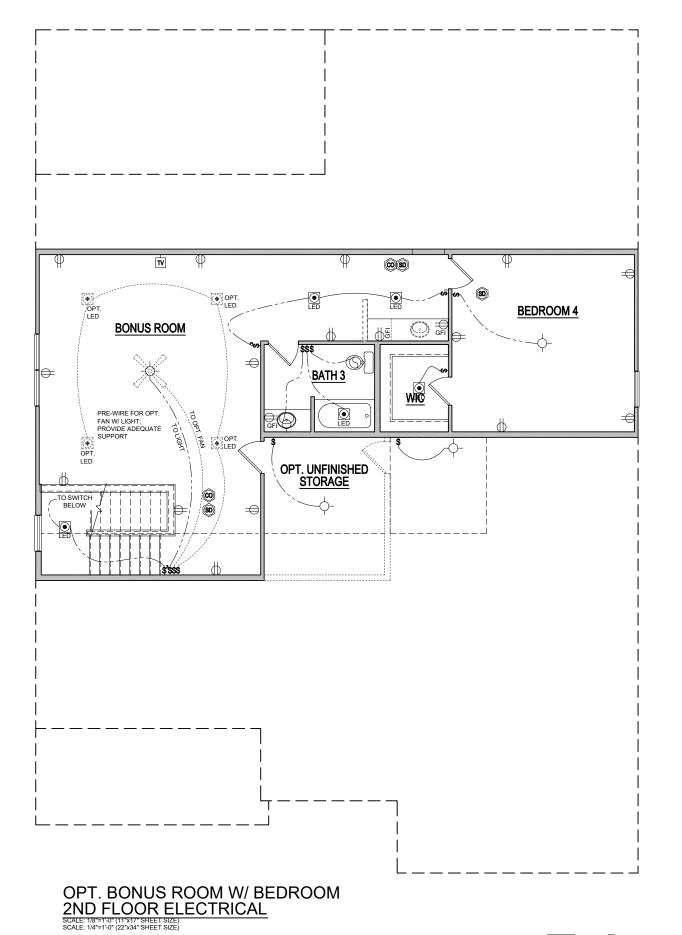
SCALE: 1/4"=1"-0" (22"x34" SHEET SIZE)





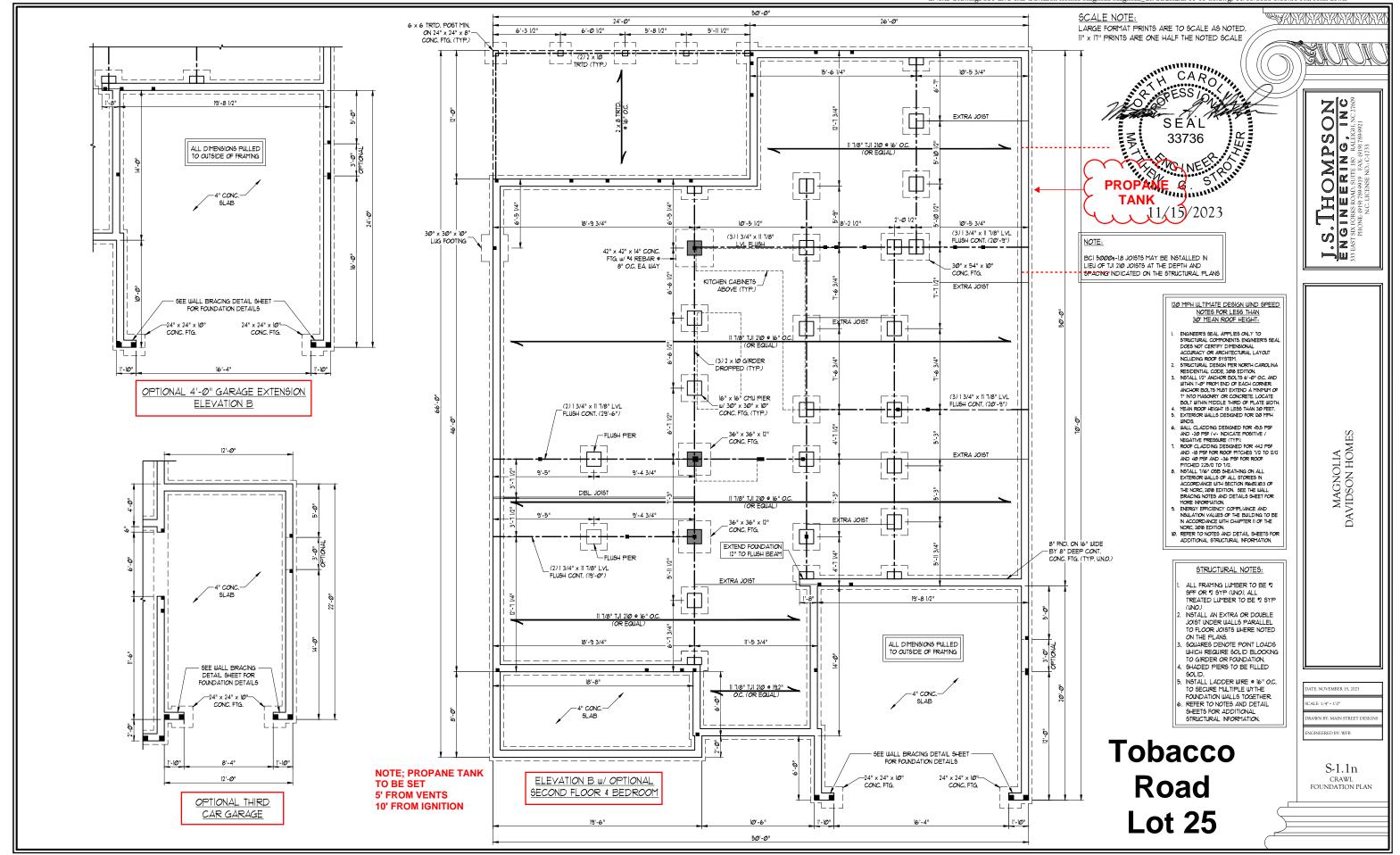


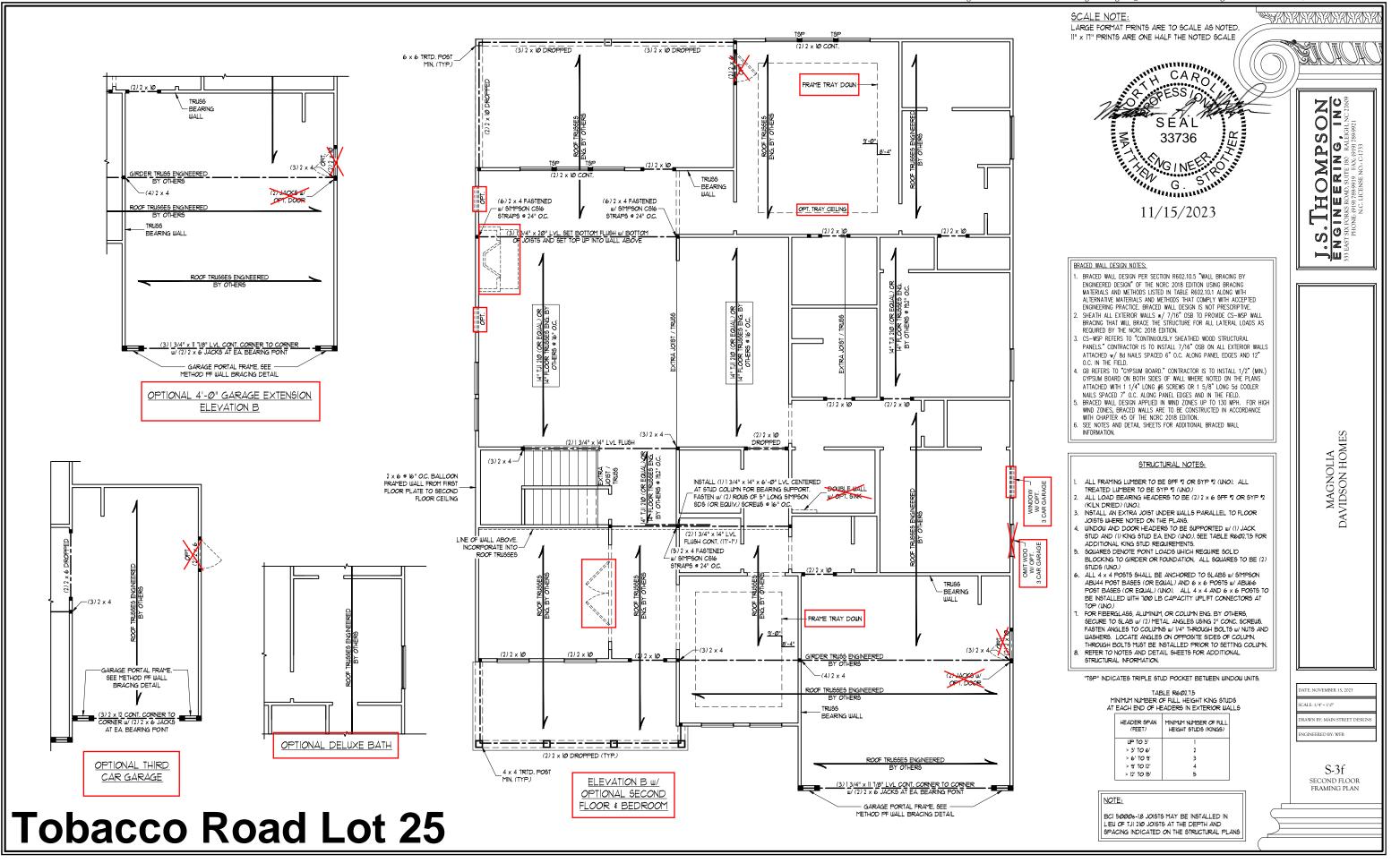


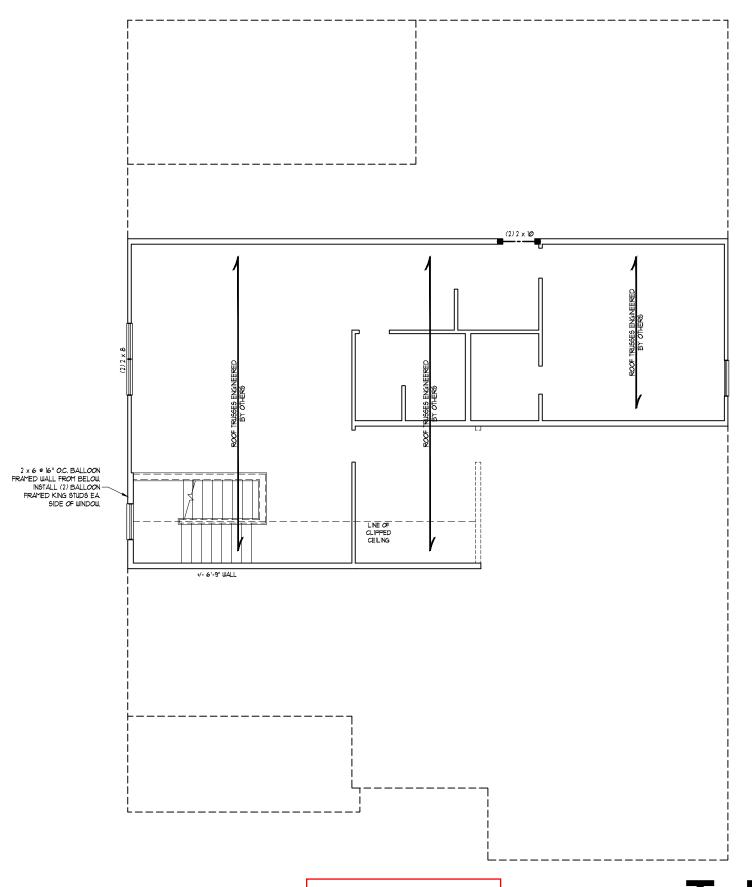


MAINDSTREET DAVIDSON HOMES MAGNOLIA DRAWING TITLE
PLAN OPTIONS SHEET NO.
E-1.2B

Tobacco Road Lot 25







OPTIONAL 2ND FLOOR W/ BEDROOM

SCALE NOTE:

LARGE FORMAT PRINTS ARE TO SCALE AS NOTED.

II" X IT" PRINTS ARE ONE HALF THE NOTED SCALE

SEAL
33736

SOUNCE SOUND

11/15/2023

THOMPSON
MYDERING, INC

S U

BRACED WALL DESIGN NOTES:

- BRACED WALL DESIGN PER SECTION R602.10.5 "WALL BRACING BY ENGINEERED DESIGN" OF THE NCRC 2018 EDITION USING BRACING MATERIALS AND METHODS LISTED IN TABLE R602.10.1 ALONG WITH ALTERNATIVE MATERIALS AND METHODS THAT COMPLY WITH ACCEPTED ENGINEERING PRACTICE. BRACED WALL DESIGN IS NOT PRESCRIPTIVE.
- SHEATH ALL EXTERIOR WALLS w/ 7/16* OSB TO PROVIDE CS-WSP WALL BRACING THAT WILL BRACE THE STRUCTURE FOR ALL LATERAL LOADS AS REQUIRED BY THE NCRC 2018 EDITION.
- CS-WSP REFERS TO "CONTINUOUSLY SHEATHED WOOD STRUCTURAL PARLS." CONTRACTOR IS TO INSTALL 7/16" OSB ON ALL EXTERIOR WALLS ATTACHED W/ 84 NAILS SPACED 6" O.C. ALONG PANEL EDGES AND 12" O.C. IN THE FIELD.
- 4. GB REFERS TO "GYPSUM BOARD." CONTRACTOR IS TO INSTALL 1/2" (MIN.) GYPSUM BOARD ON BOTH SIDES OF WALL WHERE NOTED ON THE PLANS ATTACHED WITH 1 1/4" LONG #6 SCREWS OR 1 5/8" LONG 5d COOLER NAILS SPACED 7" O.C. ALONG PANEL EDGES AND IN THE FIELD.
- BRACED WALL DESIGN APPLIED IN WIND ZONES UP TO 130 MPH. FOR HIGH WIND ZONES, BRACED WALLS ARE TO BE CONSTRUCTED IN ACCORDANCE WITH CHAPTER 45 OF THE NCRC 2018 EDITION.
- 6. SEE NOTES AND DETAIL SHEETS FOR ADDITIONAL BRACED WALL INFORMATION.

STRUCTURAL NOTES:

- I. ALL FRAMING LUMBER TO BE *2 SPF
- OR 2 SYP (UNO).

 ALL LOAD BEARING HEADERS TO BE
- (2) 2 x 6 (UNO).

 3. WINDOW AND DOOR HEADERS TO BE
- S. UNINDOW AND DOOR HEADERS TO BE SUPPORTED W/ (1) JACK STUD AND (1) KING STUD EA, END (UNO.), SEE TABLE RE02.15 FOR ADDITIONAL KING STUD REQUIREMENTS.
- 4. SQUARES DENOTE POINT LOADS
 WHICH REQUIRE SOLID BLOCKING TO
 GIRDER OR FOUNDATION. SQUARES
 TO BE (2) STUDS (UNO.)
- REFER TO NOTES AND DETAIL SHEETS FOR ADDITIONAL STRUCTURAL INFORMATION.

TABLE R602.7.5 MINIMUM NUMBER OF FULL HEIGHT KING \$TUD\$ AT EACH END OF HEADERS IN EXTERIOR WALLS

HEADER SPAN (FEET)	MINIMUM NUMBER OF FU HEIGHT STUDS (KINGS
UP TO 3'	1
> 3' TO 6'	2
> 6' TO 9'	3
> 9' TO 12'	4
> 12' TO 15'	5

DATE: NOVEMBER 15, 2023

SCALE: 1/4* – 1'-0*

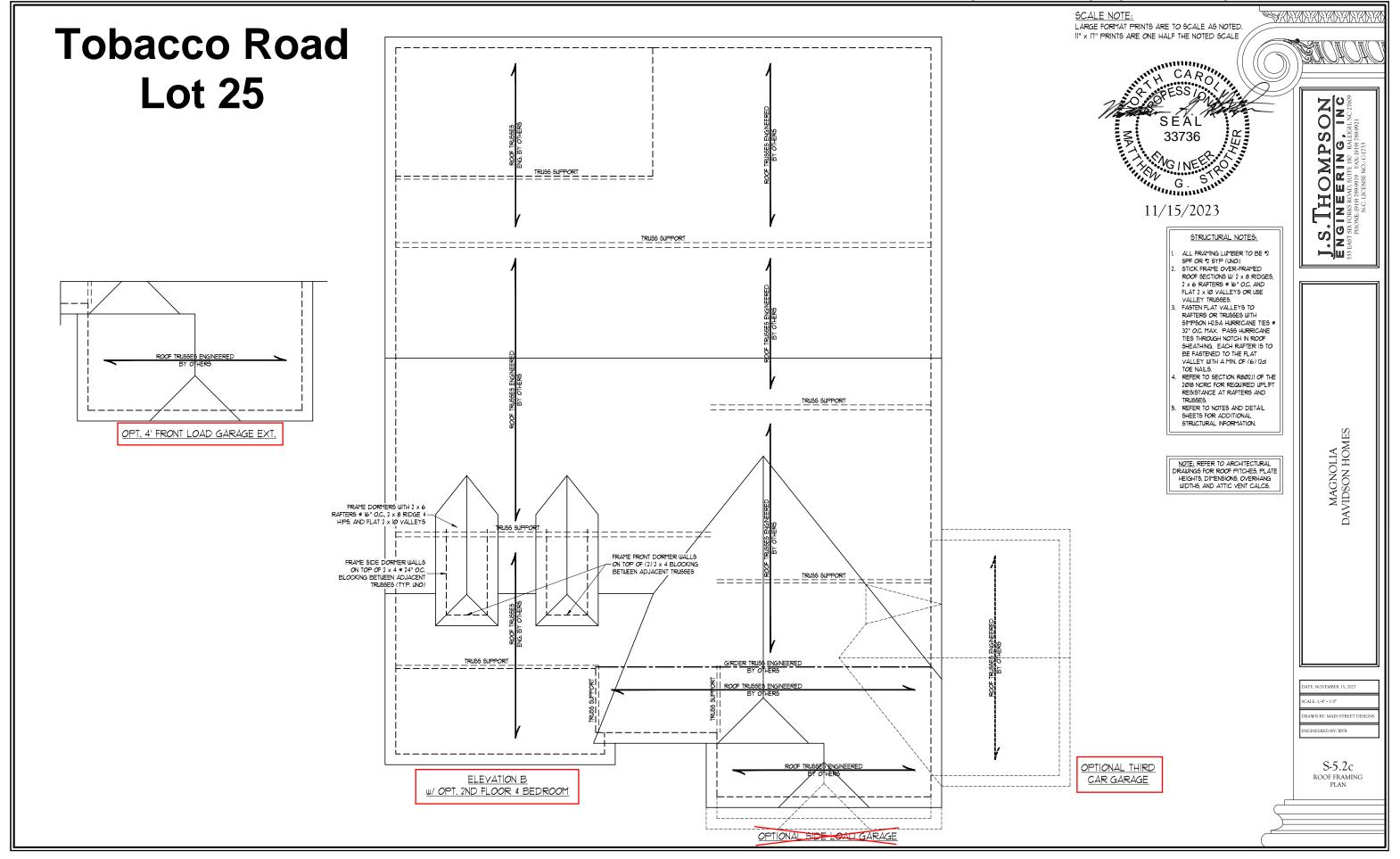
DRAWN BY: MAIN STREET DESIGNS

ENGINEERED BY: WFB

S-4b CEILING FRAMING PLAN

Tobacco Road Lot 25

MAGNOLIA DAVIDSON HOMI



SCALE NOTE:

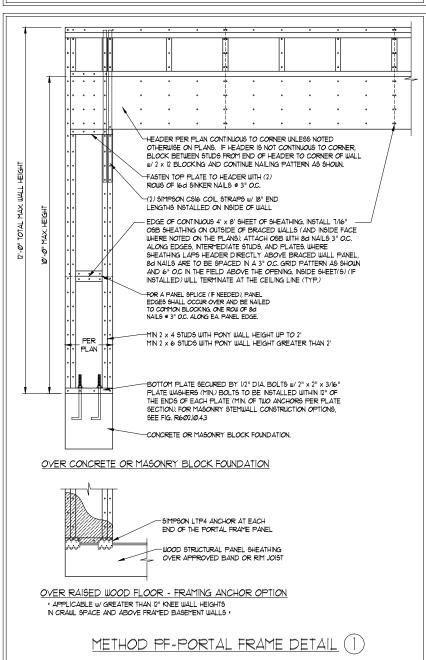
GENERAL WALL BRACING NOTES:

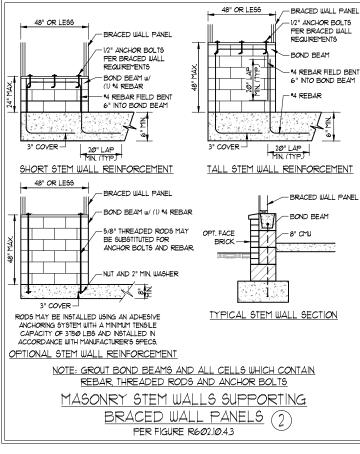
WALL BRACING DESIGNED IN ACCORDANCE WITH CHAPTER 6 OF THE 2018 NC RESIDENTIAL BUILDING CODE (NCRC). TABLES AND FIGURES REFERENCED ARE FROM THE 2018 NCRC.

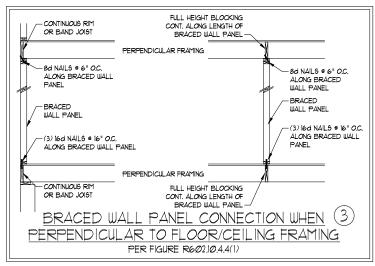
SEE THIS SHEET FOR GENERAL DETAILS, REFER TO THE 2018 NCRC FOR ADDITIONAL INFORMATION AS NEEDED.

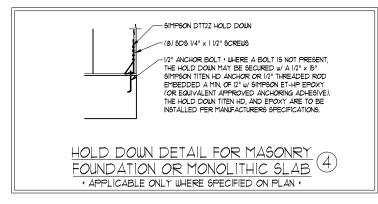
AND SHEAR FORCES IN ACCORDANCE WITH ACCEPTED ENGINEERED PRACTICE.

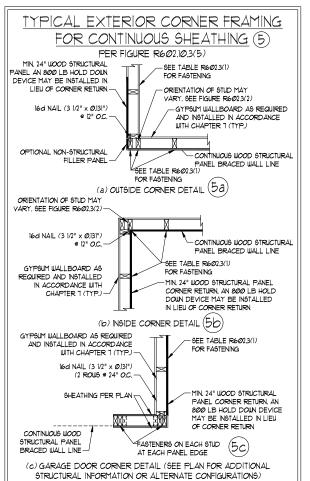
- 2. SEE HIG SHEET FOR GENERAL DETAILS. REFER TO HE 2008 ROKE TO RADITIONAL INFORTATION AND REDEED.
 3. BRACED EXTERIOR WALLS SUPPORTING ROOF TRUSSES AND RAFTERS, INCLUDING STORRES BELOW THE TOP FLOOR, HAVE BEEN DESIGNED FOR REØ2.35 (3), WALL SHEATHING AND FASTENERS HAVE BEEN DESIGNED TO RESIST COMBINED UPLIFT
- 4. SEE STRUCTURAL SHEETS FOR BRACED WALL LOCATIONS, DIMENSIONS, HOLD DOWN TYPE AND LOCATIONS, BRACED WALL LINE KEY WITH WALL DESIGN SWMARY OF REQUIRED/PROVIDED TOTALS FOR EACH WALL LINE AND ANY SPECIAL NOTES OR REQUIREMENTS.
- ALL EXTERIOR WALLS ARE TO BE SHEATHED WITH C9-WSP IN ACCORDANCE WITH SECTION R602:103 UNLESS NOTED OTHERWISE.
- 6. ALL EXTERIOR AND INTERIOR WALLS TO HAVE 1/2" GYPSUM INSTALLED, WHEN NOT USING METHOD "GB", GYPSUM TO BE FASTENED PER TABLE R102.35. METHOD GB TO BE FASTENED PER TABLE R602.10.1
- I. CS-WSP REFERS TO THE "CONTINUOUS SHEATHING WOOD STRUCTURAL PANELS" WALL BRACING METHOD. 1/16" OSB SHEATHING IS TO BE INSTALLED ON ALL EXTERIOR WALLS ATTACHED W 6d COMMON NAILS OR 8d (2 1/2" LONG X Ø.113" DIAMTETER NAILS SPACED 6" OC. AUNG PANEL EDGES AND 18" OC. IN THE FIELD (UN.O.).
- 8. GB REFERS TO THE "GYPSUM BOARD" WALL BRACING METHOD. 1/2" (MIN.) GYPSUM WALL BOARD IS TO BE INSTALLED ON BOTH SIDES OF THE BRACED WALL FASTENED WITH 11/4" SCREWS OR 1 5/8" NAILS SPACED 1" O.C. ALONG PANEL EDGES INCLUDING TOP AND BOTTOM PLATES AND INTERMEDIATE SUPPORTS (UN.O.). VERIFY ALL FASTENER OPTIONS FOR 1/2" AND 5/8" GYPSUM PRIOR TO CONSTRUCTION. FOR INTERIOR FASTENER OPTIONS SEE TABLE R60/23(1). EXTERIOR GB TO BE INSTALLED VERTICALLY.
- REQUIRED BRACED WALL LENGTH FOR EACH SIDE OF THE CIRCUMSCRIBED RECTANGLE ARE INTERPOLATED PER TABLE
 R602. 103. METHOD C6-WBP CONTRIBUTES ITS ACTUAL LENGTH, METHOD GB CONTRIBUTES 5 ITS ACTUAL LENGTH, AND
 METHOD PF CONTRIBUTES IS TIMES ITS ACTUAL LENGTH.

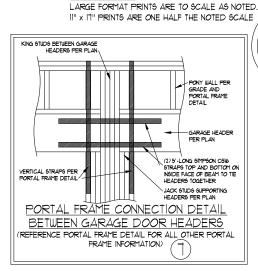


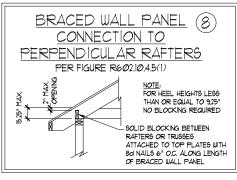


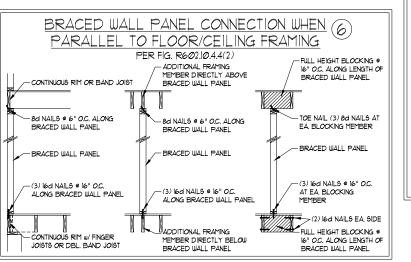


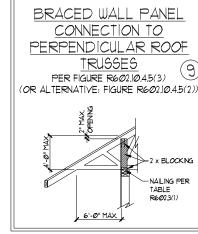


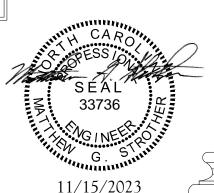












Tobacco Road Lot 25

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IGINEERING, INC

MAGNOLIA DAVIDSON HOMES

DATE: NOVEMBER 15, 2023

SCALE: 1/4* - 1'.0*

DRAWN BY: MAIN STREET DE:
ENGINEERED BY: WFB

D-4 WALL BRACING NOTES AND DETAILS

GENERAL NOTES

- I. ENGINEER'S SEAL APPLIES ONLY TO STRUCTURAL COMPONENTS INCLUDING ROOF RAFTERS, HIPS, VALLEYS, RIDGES, FLOORS, WALLS, BEAMS, HEADERS, COLUMNS, CANTILEVERS, OFFSET LOAD BEARING WALLS, PIERS, GIRDER SYSTEM AND FOOTING. ENGINEER'S SEAL DOES NOT CERTIFY DIMENSIONAL ACCURACY OF ARCHITECTURAL LAYOUT INCLUDING ROOF. ENGINEER'S SEAL DOES NOT APPLY TO 1-JOIST OR FLOOR/ROOF TRUSS LAYOUT DESIGN AND ACCURACY.
- 2. ALL CONSTRUCTION SHALL CONFORM TO THE LATEST REQUIREMENTS OF THE NORTH CAROLINA RESIDENTIAL CODE (NCRC), 2018 EDITION, PLUS ALL LOCAL CODES AND REGULATIONS. THE STRUCTURAL ENGINEER IS NOT RESPONSIBLE FOR, AND WILL NOT HAVE CONTROL OF, CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, OR SAFETY PRECAUTIONS AND PROGRAMS IN CONNECTION WITH THE CONSTRUCTION WORK. NOR WILL THE ENGINEER BE RESPONSIBLE FOR THE CONTRACTORS FAILURE TO CARRY OUT THE CONSTRUCTION WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
- 3. STRUCTURAL DESIGN BASED ON THE PROVISIONS OF THE NCRC, 2018 EDITION (R301.4 R301.7)

DESIGN CRITERIA:	LIVE LOAD (PSF)	DEAD LOAD (PSF)	DEFLECTION (IN)
ATTIC WITH LIMITED STORAGE	2Ø	10	L/240 (L/360 w/ BRITTLE FINISHES)
ATTIC WITHOUT STORAGE	10	10	L/360
DECK\$	40	10	L/360
EXTERIOR BALCONIES	40	10	L/360
FIRE ESCAPES	40	10	L/360
HANDRAILS/GUARDRAILS	200 LB OR 50 (PLF)	10	L/360
PASSENGER VEHICLE GARAGE	50	10	L/360
ROOMS OTHER THAN SLEEPING ROOM	40	10	L/360
SLEEPING ROOMS	3Ø	10	L/360
STAIRS	40	10	L/360
WIND LOAD	(BASED ON TABLE R3Ø1.20	4) WIND ZONE AND EXPOSURE)
GROUND SNOW LOAD: Pa	2Ø (PSF)		

- I-JOIST SYSTEMS DESIGNED WITH 12 PSF DEAD LOAD AND DEFLECTION (IN) OF L/480
- FLOOR TRUSS SYSTEMS DESIGNED WITH 15 PSF DEAD LOAD
- 4. FOR 115 AND 120 MPH WIND ZONES, FOUNDATION ANCHORAGE 15 TO COMPLY WITH SECTION R403.16 OF THE NCRC, 2018 EDITION. FOR 130 MPH, 140 MPH, AND 150 MPH WIND ZONES, FOUNDATION ANCHORAGE 15 TO COMPLY WITH SECTION 4504 OF THE NCRC, 2018 EDITION.
- 5. ENERGY EFFICIENCY COMPLIANCE AND INSULATION VALUES OF THE BUILDING TO BE IN ACCORDANCE WITH CHAPTER II OF THE NCRC, 2018 EDITION.

FOOTING AND FOUNDATION NOTES

- 1. FOUNDATION DESIGN BASED ON A MINIMUM ALLOWABLE BEARING CAPACITY OF 2000 PSF. CONTACT GEOTECHNICAL ENGINEER IF BEARING CAPACITY IS NOT ACHIEVED.
- 2. FOR ALL CONCRETE \$LAB\$ AND FOOTING\$, THE AREA WITHIN THE PERIMETER OF THE BUILDING ENVELOPE \$HALL HAVE ALL VEGETATION, TOP \$OIL AND FOREIGN MATERIAL REMOVED. FILL MATERIAL, \$HALL BE FREE OF VEGETATION AND FOREIGN MATERIAL. THE FILL \$HALL BE COMPACTED TO A\$\$URE UNIFORM \$UPPORT OF THE \$LAB, AND EXCEPT WHERE APPROVED, THE FILL DEPTH\$ \$HALL NOT EXCEED 24" FOR CLEAN \$AND OR GRAVEL. A 4" THICK BA\$ED COURSE CONSISTING OF CLEAN GRADED \$AND OR GRAVEL \$HALL BE PLACED. A BA\$E COURSE IS NOT REQUIRED WHERE A CONCRETE \$LAB IS INSTALLED ON WELL-DRAINED OR \$AND-GRAVEL MIXTURE \$OIL\$ CLA\$SIFIED AS GROUP I, ACCORDING TO THE UNITED \$OIL CLA\$SIFICATION \$Y\$TEM IN ACCORDANCE WITH TABLE R4\$5.1 OF THE NCRC, 2018 EDITION.
- 3. PROPERLY DEWATER EXCAVATION PRIOR TO POURING CONCRETE WHEN BOTTOM OF CONCRETE 6LAB 15 AT OR BELOW WATER TABLE. IF APPLICABLE, 3/4" 1" DEEP CONTROL JOINTS ARE TO BE SAWED WITHIN 4 TO 12 HOURS OF CONCRETE FINISHING AND WALL LOCATIONS HAVE BEEN MARKED. ADJUST WHERE NECESSARY.
- 4. CONCRETE SHALL CONFORM TO SECTION R4022 OF THE NCRC, 2018 EDITION. CONCRETE REINFORCING STEEL TO BE ASTM A615 GRADE 60. WELDED WIRE FABRIC TO BE ASTM A185. MAINTAIN A MINIMUM CONCRETE COVER AROUND REINFORCING STEEL OF 3" IN FOOTINGS AND 1 1/2" IN SLABS, FOR POURED CONCRETE WALLS, CONCRETE COVER FOR REINFORCING STEEL MEASURED FROM THE INSIDE FACE OF THE WALL SHALL NOT BE LESS THAN 3/4". CONCRETE COVER FOR REINFORCING STEEL MEASURED FROM THE OUTSIDE FACE OF THE WALL SHALL NOT BE LESS THAN 1/2" FOR 5 BARS OR SMALLER, AND NOT LESS THAN 2" FOR 16 BARS OR LARGER.
- 5. MASONRY UNITS TO CONFORM TO ACE 530/A6CE 5/TMS 402. MORTAR SHALL CONFORM TO ASTM C270.
- 6. THE UNSUPPORTED HEIGHT OF MASONRY PIERS SHALL NOT EXCEED FOUR TIMES THEIR LEAST DIMENSION FOR UNFILLED HOLLOW CONCRETE MASONRY UNITS AND TEN TIMES THEIR LEAST DIMENSION FOR SOLID OR SOLID FILLED PIERS, PERS MAY BE FILLED SOLID WITH CONCRETE OR TYPE M OR S MORTAR PIERS AND WALLS SHALL BE CAPPED WITH 8" OF SOLID MASONRY.
- 1. THE CENTER OF EACH OF THE PIERS SHALL BEAR IN THE MIDDLE THIRD OF ITS RESPECTIVE FOOTING, EACH GIRDER SHALL BEAR IN THE MIDDLE THIRD OF THE PIERS.
- 8. ALL CONCRETE AND MASONRY FOUNDATION WALLS ARE TO BE CONSTRUCTED IN ACCORDANCE WITH THE PROVISIONS OF SECTION R404 OF THE NCRC, 2018 EDITION OR IN ACCORDANCE WITH ACI 318, ACI 332, NCMA TR68-A OR ACE 530/ASCE 5/TMS 402. MASONRY FOUNDATION WALLS ARE TO BE REINFORCED PER TABLE R404.I.I(1), R404.I.I(2), R404.I.I(3), OR R404.I.I(4) OF THE NCRC, 2018 EDITION. CONCRETE FOUNDATION WALLS ARE TO BE REINFORCED PER TABLE R404.I.I(15) OF THE NCRC, 2018 EDITION. STEP CONCRETE FOUNDATION WALLS AT 16" OC WHERE GRADE PERMITS (UNO)

Tobacco Road Lot 25

FRAMING NOTES

- I. ALL FRAMING LUMBER SHALL BE 12 SPF (Fb = 815 P6), Fv = 315 P6), E = 1600000 P6) OR 12 SYP (Fb = 915 P6), Fv = 115 P6), E = 16000000 P6) MINIMUM UNLESS NOTED OTHERWISE (UNO). ALL TREATED LUMBER SHALL BE 12 SYP MINIMUM UNLESS NOTED OTHERWISE (UNO).
- 2. LAMINATED VENEER LUMBER (LYL) SHALL HAVE THE FOLLOWING MINIMUM PROPERTIES: Fb = 2600 PSI, Fv = 285 PSI, E = 1900000 PSI. LAMINATED STRAND LUMBER (LSL) SHALL HAVE THE FOLLOWING MINIMUM PROPERTIES: Fb = 2325 PSI, Fv = 310 PSI, E = 15500000 PSI. PARALLEL STRAND LUMBER (PSL) UP TO I DEPTH SHALL HAVE THE FOLLOWING MINIMUM PROPERTIES: Fc = 2500 PSI, E = 18000000 PSI. PARALLEL STRAND LUMBER (PSL) MORE THAN 1" DEPTH SHALL HAVE THE FOLLOWING MINIMUM PROPERTIES: Fc = 2900 PSI, E = 20000000 PSI. INSTALL ALL CONNECTIONS PER MANUFACTURER'S SPECIFICATIONS.
- 3. STRUCTURAL STEEL SHALL CONFORM TO THE FOLLOWING ASTM SPECIFICATIONS

 A.
 W AND WT 9HAPE9:
 A9TM A992

 B.
 CHANNELS AND ANGLES:
 ASTM A36

 C.
 PLATES AND BARS:
 ASTM A36

). HOLLOW STRUCTURAL SECTIONS: ASTM A500 GRADE B

E. STEEL PIPE: ASTM A53, GRADE B, TYPE E OR S

4. STEEL BEAMS SHALL BE SUPPORTED AT EACH END WITH A MINIMUM BEARING LENGTH OF 3 1/2" AND FULL FLANGE WIDTH (UNO). PROVIDE SOLID BEARING FROM BEAM SUPPORT TO FOUNDATION. BEAMS SHALL BE ATTACHED AT THE BOTTOM FLANGE TO EACH SUPPORT AS FOLLOWS (UNO):

A. WOOD FRAMING

(2) 1/2" DIA. x 4" LONG LAG SCREWS

B. CONCRETE

(2) 1/2" DIA. x 4" WEDGE ANCHORS

C. MASONRY (FULLY GROUTED)

(2) 1/2" DIA. x 4" LONG SIMPSON TITEN HD ANCHORS

LATERAL SUPPORT IS CONSIDERED ADEQUATE PROVIDING THE JOISTS ARE TOE NAILED TO THE 2x NAILER ON TOP OF THE STEEL BEAM, AND THE 2x NAILER IS SECURED TO THE TOP OF THE STEEL BEAM W/ (2) ROUS OF SELF TAPPING SCREUS @ IG!" O.C. OR (2) ROUS OF I/2" DIAMETER BOLTS @ IG" O.C. IF I/2" BOLTS ARE USED TO FASTEN THE NAILER, THE STEEL BEAM SHALL BE FABRICATED W/ (2) ROUS OF 9/16" DIAMETER HOLES @ IG!" O.C.

- SQUARES DENOTE POINT LOADS WHICH REQUIRE SOLID BLOCKING TO GIRDER OR FOUNDATION. SHADED SQUARES DENOTE POINT LOADS
 FROM ABOVE WHICH REQUIRE SOLID BLOCKING TO SUPPORTING MEMBER BELOW.
- 6. ALL LOAD BEARING HEADERS TO CONFORM TO TABLE R602.7(1) AND R602.7(2) OF THE NCRC, 2018 EDITION OR BE (2) 2 x 6 WITH (1) JACK AND (1) KING STUD EACH END (UNO), WHICHEVER IS GREATER ALL HEADERS TO BE SECURED TO EACH JACK STUD WITH (4) 8d NAILS. ALL BEAMS TO BE SUPPORTED WITH (2) STUDS AT EACH BEARING POINT (UNO). INSTALL KING STUDS PER SECTION R602.7.5 OF THE NORTH CAROLINA RESIDENTIAL CODE, 2018 EDITION.
- 1. ALL BEAMS, HEADERS, OR GIRDER TRUSSES PARALLEL TO WALL ARE TO BEAR FULLY ON (1) JACK OR (2) STUDS MINIMUM OR THE NUMBER OF JACKS OR STUDS NOTED. ALL BEAMS OR GIRDER TRUSSES PERPENDICULAR TO WALL AND SUPPORTED BY (3) STUDS OR LESS ARE TO HAVE 1 1/2" MINIMUM BEARING (NO.). ALL BEAMS OR GIRDER TRUSSES PERPENDICULAR TO WALL AND SUPPORTED BY MORE THAN (3) STUDS OR OTHER NOTED COLUMN ARE TO BEAR FULLY ON SUPPORT COLUMN FOR ENTIRE WALL DEPTH (UNO.). BEAM ENDS THAT BUTT INTO ONE ANOTHER ARE TO EACH BEAR EQUIAL LENGTHS (UNO.).
- 8. FLITCH BEAMS SHALL BE BOLTED TOGETHER USING 1/2" DIAMETER BOLTS (ASTM A3Ø1) WITH WASHERS PLACED AT THREADED END OF BOLT. BOLTS SHALL BE SPACED AT 24" CENTERS (MAXIMUM), AND STAGGERED AT TOP AND BOTTOM OF BEAM (2" EDGE DISTANCE), WITH (2) BOLTS LOCATED AT 6" FROM EACH END (UNO).
- 9. ALL 1-JOIST OR TRUSS LAYOUTS ARE TO BE IN COMPLIANCE WITH THE OVERALL DESIGN SPECIFIED ON THE PLANS. ALL DEVIATIONS ARE TO BE BROUGHT TO THE ATTENTION OF THE ENGINEER OF RECORD PRIOR TO INSTALLATION.
- IØ. BRACED WALL PANELS SHALL BE CONSTRUCTED ACCORDING TO THE NORTH CAROLINA RESIDENTIAL CODE 2018 EDITION WALL BRACING CRITERIA. THE AMOUNT, LENGTH, AND LOCATION OF BRACING SHALL COMPLY WITH ALL APPLICABLE TABLES IN SECTION R602.10.
- II. PROVIDE DOUBLE JOIST UNDER ALL WALLS PARALLEL TO FLOOR JOISTS, PROVIDE SUPPORT UNDER ALL WALLS PARALLEL TO FLOOR TRUSSES OR I-JOISTS PER MANUFACTURER'S SPECIFICATIONS. INSTALL BLOCKING BETWEEN JOISTS OR TRUSSES FOR POINT LOAD SUPPORT FOR ALL POINT LOADS ALONG OFFSET LOAD LINES.
- 12. FOR ALL HEADERS SUPPORTING BRICK VENEER THAT ARE LESS THAN 8'-0" IN LENGTH, REST A 6" x 4" x 5/16" STEEL ANGLE WITH 6" MINIMUM EMBEDMENT AT SIDES FOR BRICK SUPPORT (UN.O.). FOR ALL HEADERS 8'-0" AND GREATER IN LENGTH, BOLT A 6" x 4" x 5/16" STEEL ANGLE TO HEADER WITH 1/2" LAG SCREWS AT 12" O.C. STAGGERED FOR BRICK SUPPORT. FOR ALL BRICK SUPPORT AT ROOF LINES, BOLT A 6" x 4" x 5/16" STEEL ANGLE TO (2) 2 x 10 BLOCKING INSTALLED w/ (4) 12d NAILS EA. PLY BETWEEN WALL STUDS WITH (2) ROWS OF 1/2" LAG SCREWS AT 12" O.C. STAGGERED AND IN ACCORDANCE WITH SECTION RT03.8.2.1 OF THE NCRC, 2018 EDITION.
- 13. FOR STICK FRAMED ROOFS: CIRCLES DENOTE (3) 2 x 4 POSTS FOR ROOF MEMBER SUPPORT. HIP SPLICES ARE TO BE SPACED A MINIMUM OF 8'-0". FASTEN MEMBERS WITH THREE ROUS OF 12d NAILS AT 16" O.C. FRAME DORMER WALLS ON TOP OF DOUBLE OR TRIPLE RAFTERS AS SHOUN (UNO)
- 14. FOR TRUSSED ROOFS: FRAME DORMER WALLS ON TOP OF 2 × 4 LADDER FRAMING AT 24" O.C. BETWEEN ADJACENT ROOF TRUSSES. STICK FRAME OVER-FRAMED ROOF SECTIONS WITH 2 × 8 RIDGES, 2 × 6 RAFTERS AT 16" O.C. AND FLAT 2 × 10 VALLEYS (UNO).
- IB. ALL 4 x 4 AND 6 x 6 POSTS TO BE INSTALLED WITH 1000 LB CAPACITY UPLIFT CONNECTORS TOP AND BOTTOM (UNO.) POSTS MAY BE SECURED USING ONE SIMPSON H6 OR LTS12 UPLIFT CONNECTOR FASTENED TO THE BAND AT THE BOTTOM AND THE BEAM AT THE TOP OF EACH POST. ONE 16" SECTION OF SIMPSON CS16 COIL STRAPPING WITH (8) 8d HDG NAILS AT EACH END MAY BE USED IN LIEU OF EACH TWIST STRAP IF DESIRED. FOR MASONRY OR CONCRETE FOUNDATION USE SIMPSON POST BASE.

SCALE NOTE:

LARGE FORMAT PRINTS ARE TO SCALE AS NOTED.

III" x ITI" PRINTS ARE ONE HALF THE NOTED SCALE

CARO

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11/15/2023

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MAGNOLIA DAVIDSON HOMES

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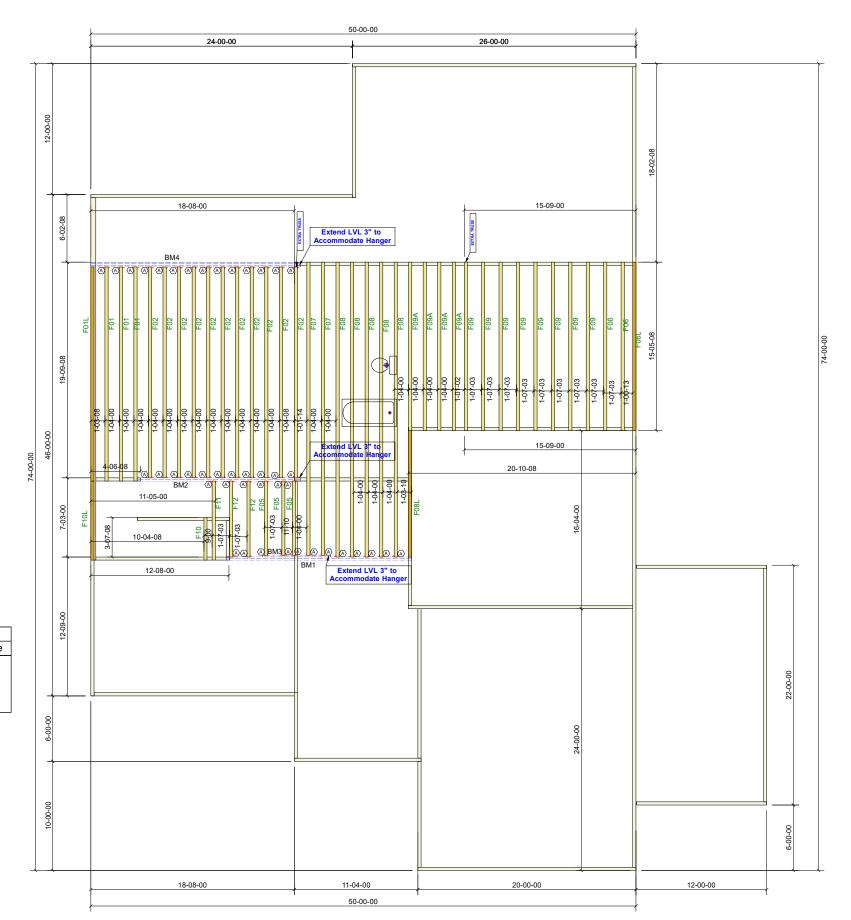
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DRAWN BY: MAIN STREET D

ENGINEERED BY: WFB

D-5 STANDARD STRUCTURAL NOTES

THIS LAYOUT IS INTENDED FOR THE PURPOSE OF TRUSS LOCATION AND PLACEMENT ONLY. REFER TO THE BUILDING PLANS FOR ACTUAL BUILDING CONSTRUCTION.



DEDICATED TO QUALITY AND EXCELLENCE 200 EMMETT ROAD DUNN, NORTH CAROLINA 28334 PHONE: 910-892-8400

TOBACCO ROAD

25

Davidson Homes

Floor

4

w/Brdm

 \Box

Magnolia

TOP LIVE LOAD:

TOP DEAD LOAD:

BOTTOM LIVE LOAD:

BOTTOM DEAD LOAD:

- DO NOT CUT OR MODIFY TRUSSES

- TRUSSES ARE SPACED 19.2" ON CENTER UNLESS OTHERWISE NOTED

- REFER TO THE INDIVIDUAL TRUSS DESIGN DRAWINGS FOR THE LOCATION

- OF LATERAL BRACING AND MULTI-PLY CONNECTION REQUIREMENTS.

- PER ANSI TPI 1-2002 THE TRUSS ENGINEER IS RESPONSIBILE FOR TRUSS

- TO TRUSS CONNECTIONS AND TRUSS PLY TO PLY CONNECTIONS. THIS TRUSS PLATES TO BEARING CONNECTIONS AND TRUSS TO BEARING CONNECTIONS AND TRUSS TO BEAM CONNECTIONS WHICH SHALL BE REVIEWED BY THE BUILDING DESIGNER. IT IS THE RESPONSIBILITY OF THE BUILDING DESIGNER.

TO RESOLVE ALL ROOF FORCES ADEQUATELY TO THE FOUNDATION.

N.T.S

BES

DRAWN BY

PRINT DATE: 4/21/2024

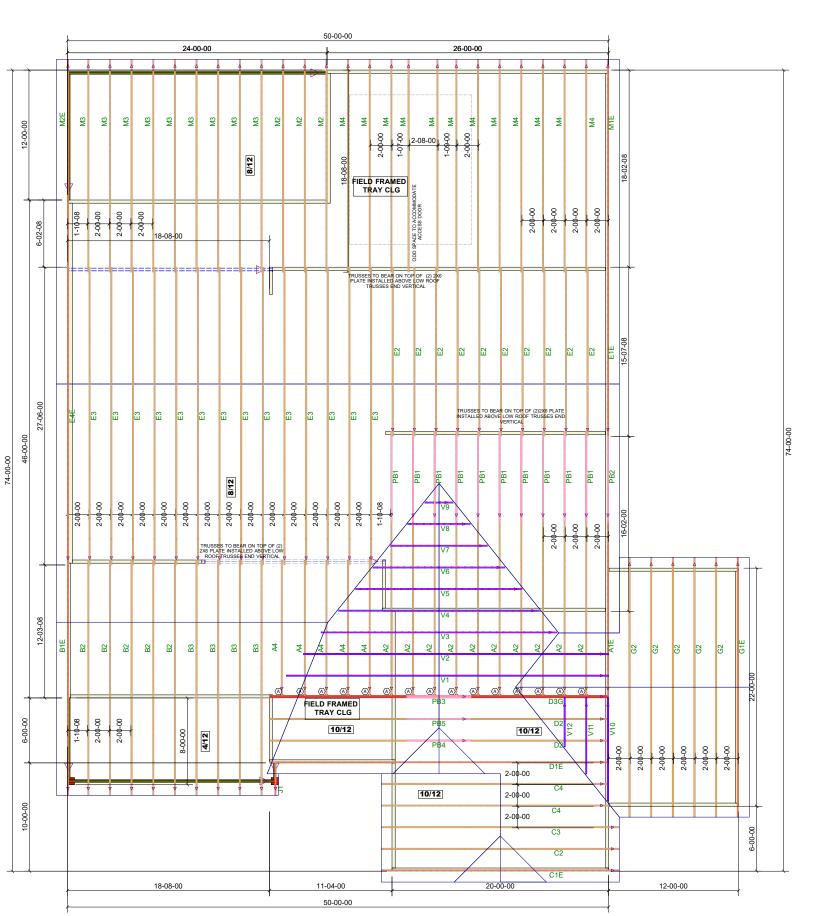
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Α	LUS410	43			
В	-	-			
С		-			

		Products			
PlotID	Length	Product	Plies	Net Qty	Fab Type
BM1	18-00-00	1-3/4" x 14" VERSA-LAM® LVL 2.1E 3100 SP	2	2	MFD
BM2	16-00-00	1-3/4" x 14" VERSA-LAM® LVL 2.1E 3100 SP	2	2	MFD
BM3	8-00-00	1-3/4" x 14" VERSA-LAM® LVL 2.1E 3100 SP	1	1	MFD
BM4	20-00-00	1-3/4" x 20" VERSA-LAM® LVL 2.1E 3100 SP	3	3	MFD

rawl Level Floor Area 1st Level Floor Area 2nd Level Floor Area 0 1122.25 0

THIS LAYOUT IS INTENDED FOR THE PURPOSE OF TRUSS LOCATION AND PLACEMENT ONLY. REFER TO THE BUILDING PLANS FOR ACTUAL BUILDING CONSTRUCTION.



DEDICATED TO QUALITY AND EXCELLENCE 200 EMMETT ROAD DUNN, NORTH CAROLINA 28334 PHONE: 910-892-8400 w/BonusR-3CGg N.T.S **TOBACCO ROAD** avidson Homes DRAWN BY: BES \Box 4/21/2024 MAGNOLIA 25 **岁**街396-1101 ² 2 TOP LIVE LOAD: 20 TOP DEAD LOAD: 20 BOTTOM LIVE LOAD: 0 BOTTOM DEAD LOAD: 10 - DO NOT CUT OR MODIFY TRUSSES
- TRUSSES ARE SPACED 24" ON CENTER UNLESS OTHERWISE NOTED
- REFER TO THE INDIVIDUAL TRUSS DESIGN DRAWINGS FOR THE LOCATION
OF LATERAL BRACING AND MULTI-PLY CONNECTION REQUIREMENTS.
- PER ANSI TPI 1-2002 THE TRUSS ENGINEER IS RESPONSIBILE FOR TRUSS
TO TRUSS CONNECTIONS AND TRUSS PLY TO PLY CONNECTIONS THIS ITRUS PLACEMENT PLAN RECOMMENDS TRUSS TO BEARM CONNECTIONS AND TRUSS TO BEAM CONNECTIONS WHICH SHALL BE REVIEWED BY THE BUILDING DESIGNER. IT IS THE RESPONSIBILITY OF THE BUILDING DESIGNER.
TO RESOLVE ALL ROOF FORCES ADEQUATELY TO THE FOUNDATION.

HANGER LIST

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