

8/14/2011

-SR7-

PI Sta 23+23.72
 $\Delta = 3'00''$ 156'(LT)
 $D = 0'57''$ 17.7"
 $L = 314.61'$
 $T = 157.34'$
 $R = 6,000.00'$
 $SE = 0.02$
 RUNOFF = SEE PLANS

-Y12-

PI Sta 55+24.71
 $\Delta = 4'52''$ 28.3'(RT)
 $D = 10'11''$ 44.7"
 $L = 470.21'$
 $T = 249.85'$
 $R = 561.90'$
 SE = SEE PLANS

PI Sta 58+17.25
 $\Delta = 12'57''$ 35.0'(RT)
 $D = 9'00''$ 57.0"
 $L = 143.74'$
 $T = 72.18'$
 $R = 635.50'$
 SE = SEE PLANS

PI Sta 59+61.36
 $\Delta = 15'56''$ 05.5'(RT)
 $D = 11'03''$ 14.8"
 $L = 144.15'$
 $T = 72.55'$
 $R = 518.32'$
 SE = SEE PLANS

PIs Sta 10+80.00
 $G_s = 1'08''$ 45.3"
 $L_s = 120.00'$
 $LT = 80.00'$
 $ST = 40.00'$

PI Sta 11+96.80
 $G_s = 2'59''$ 58.9'(LT)
 $D = 1'54''$ 35.5"
 $L = 153.57'$
 $T = 76.80'$
 $R = 3,000.00'$
 $SE = 0.04$
 RUNOFF = SEE PLANS

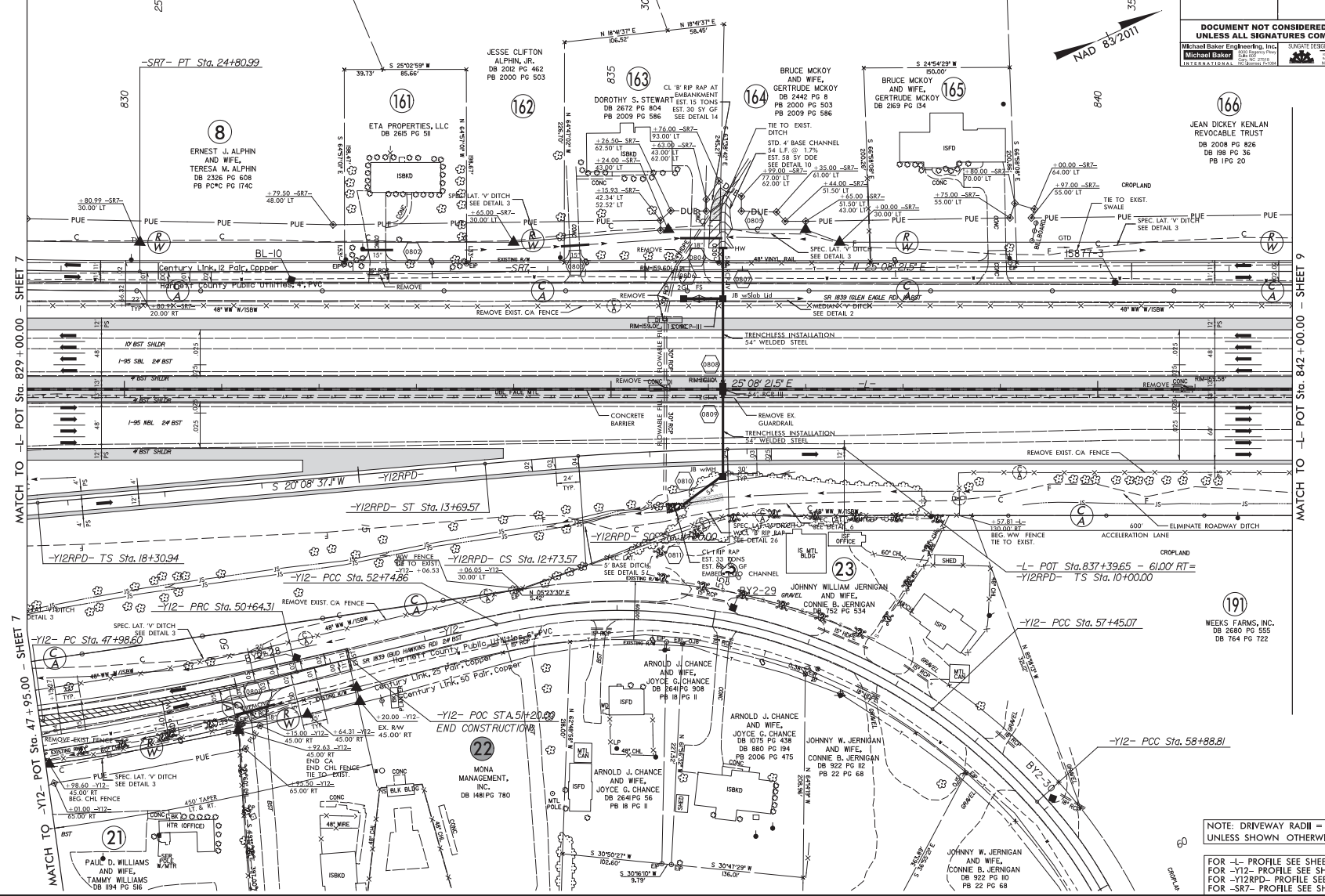
-Y12RPD-

PIs Sta 13+05.57
 $G_s = 0'59''$ 00.2"
 $L_s = 96.00'$
 $LT = 64.00'$
 $ST = 32.00'$

PIs Sta 19+36.45
 $G_s = 2'47''$ 37.7"
 $D = 1'56''$ 00.2"
 $L = 156.00'$
 $LT = 105.51'$
 $ST = 53.38'$

PI Sta 20+79.12
 $\Delta = 6'3''$ 08' 31.6'(LT)
 $D = 3'8''$ 11' 49.9"
 $L = 165.31'$
 $T = 92.18'$
 $R = 150.00'$
 $SE = 0.08$
 RUNOFF = SEE PLANS

PROJECT REFERENCE NO.	SHEET NO.
I-5877	8
R/W SHEET NO.	HYDRAULICS ENGINEER
ROADWAY DESIGN ENGINEER	
I-5877 RIGHT OF WAY PLANS PHASE II	
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED	
Michael Baker Engineering, Inc. INTERNATIONAL	Michael Baker Engineering, Inc. INTERNATIONAL



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MATCH TO -L- POT Sta. 829+00.00 - SHEET 7

MATCH TO -Y12- POT Sta. 47+95.00 - SHEET 7

MATCH TO -L- POT Sta. 842+00.00 - SHEET 9

NOTE: DRIVEWAY RADII = 10' UNLESS SHOWN OTHERWISE

FOR -L- PROFILE SEE SHEET 72
FOR -Y12- PROFILE SEE SHEET 78
FOR -SR7- PROFILE SEE SHEET 87