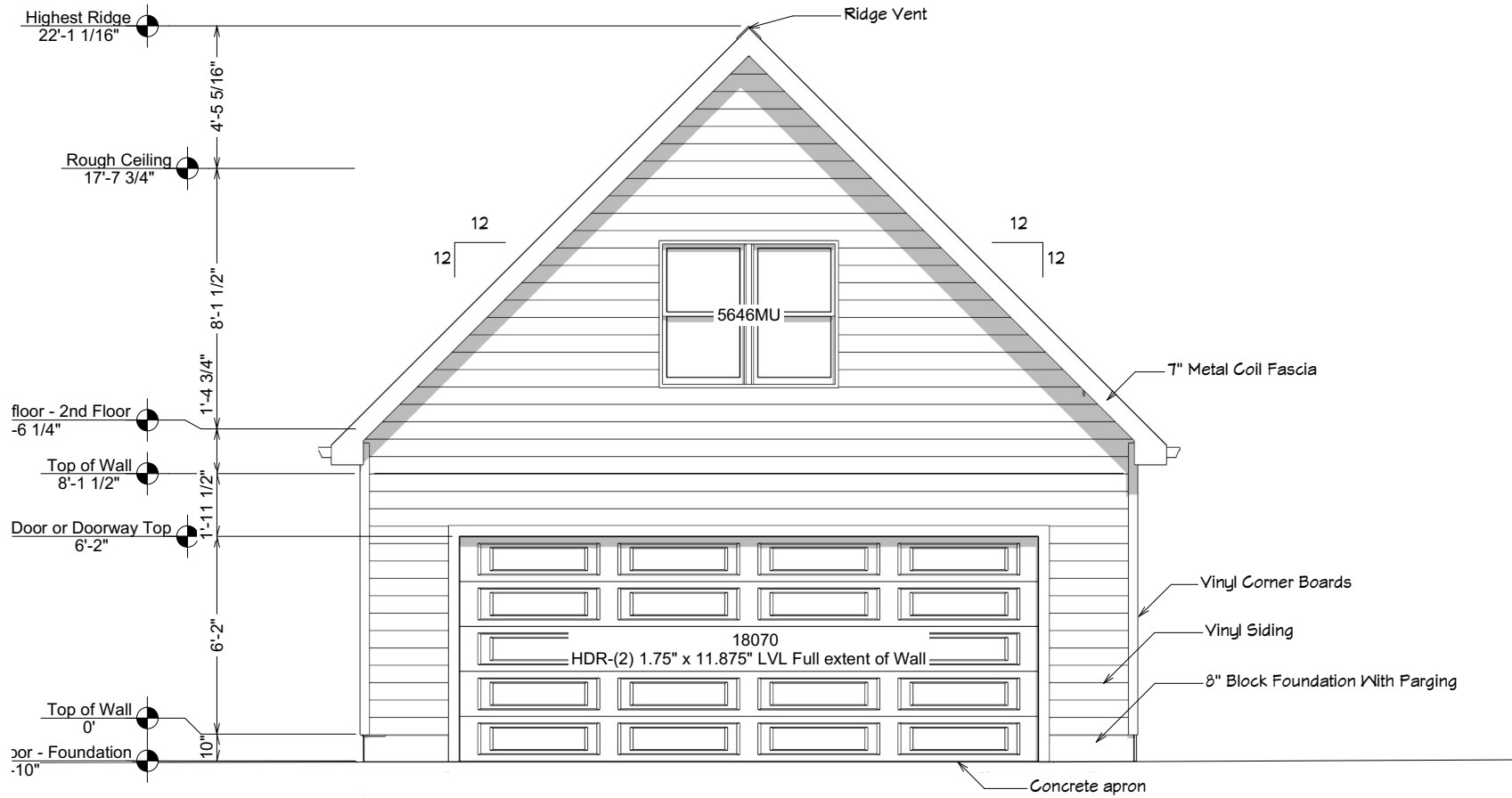
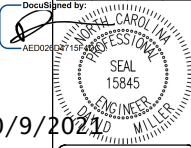


SHEET #
1

PROJECT NUMBER:
21DDJ-1007A
DATE:
10/9/2021
DRAWN BY:
DAJ
CHECKED BY:
DM



EXTERIOR ELEVATION FRONT

SCALE: 3/16 in = 1 ft

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6300 CREEDMOOR ROAD, STE 170 #363
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Will Crocker



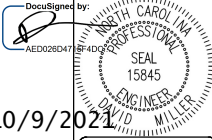


EXTERIOR ELEVATION RIGHT

SCALE: 3/16 in = 1 ft

SHEET #
2

PROJECT NUMBER:
21DDJ-1007A
10/9/2021
DAJ
Checked By: DM



10/9/2021

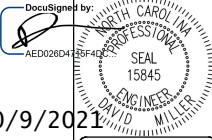
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Will Crocker



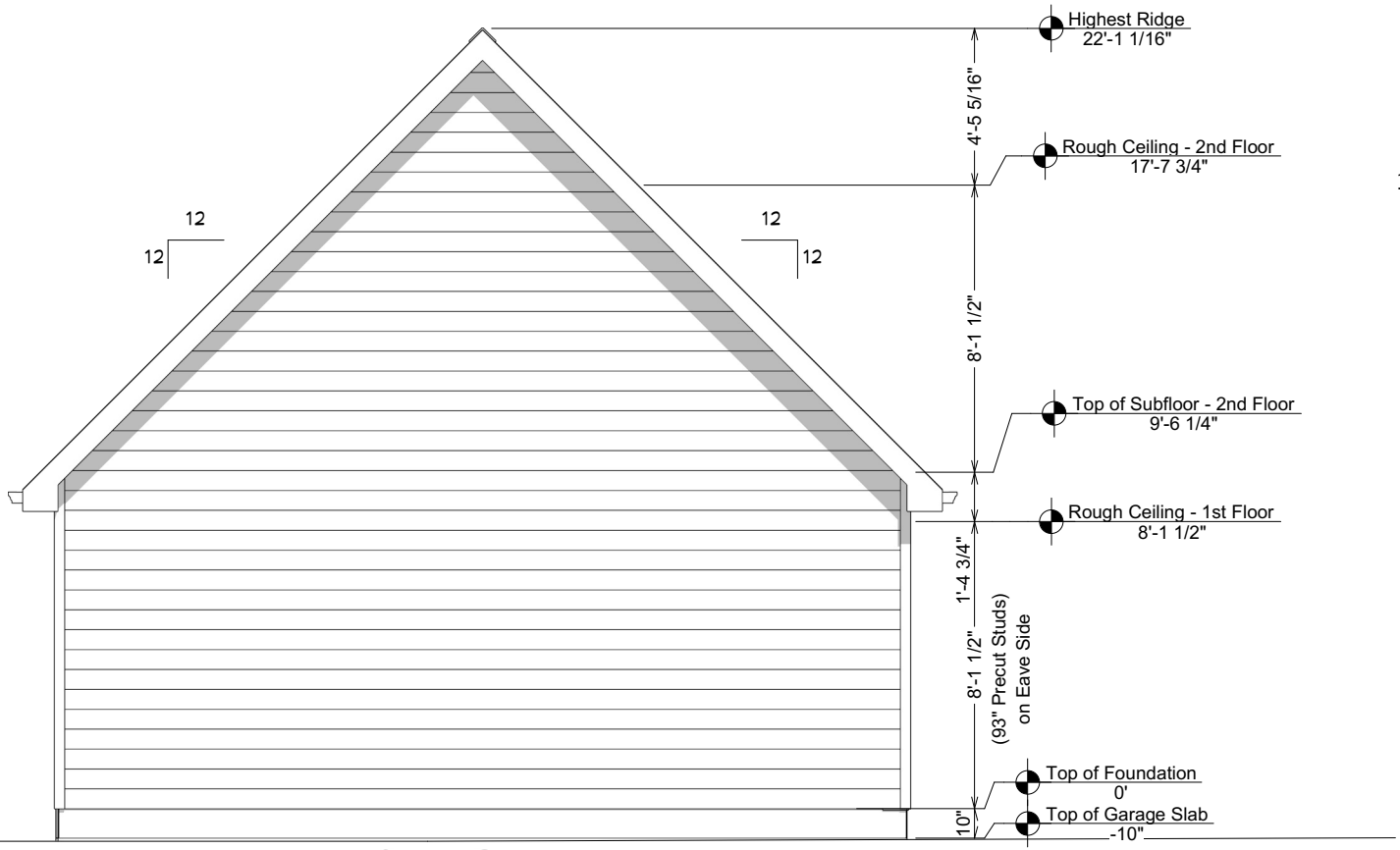
SHEET #
3

PROJECT NUMBER:
21DDJ-1007A
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10/9/2021

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EXTERIOR ELEVATION BACK

SCALE: 3/16 in = 1 ft

Will Crocker



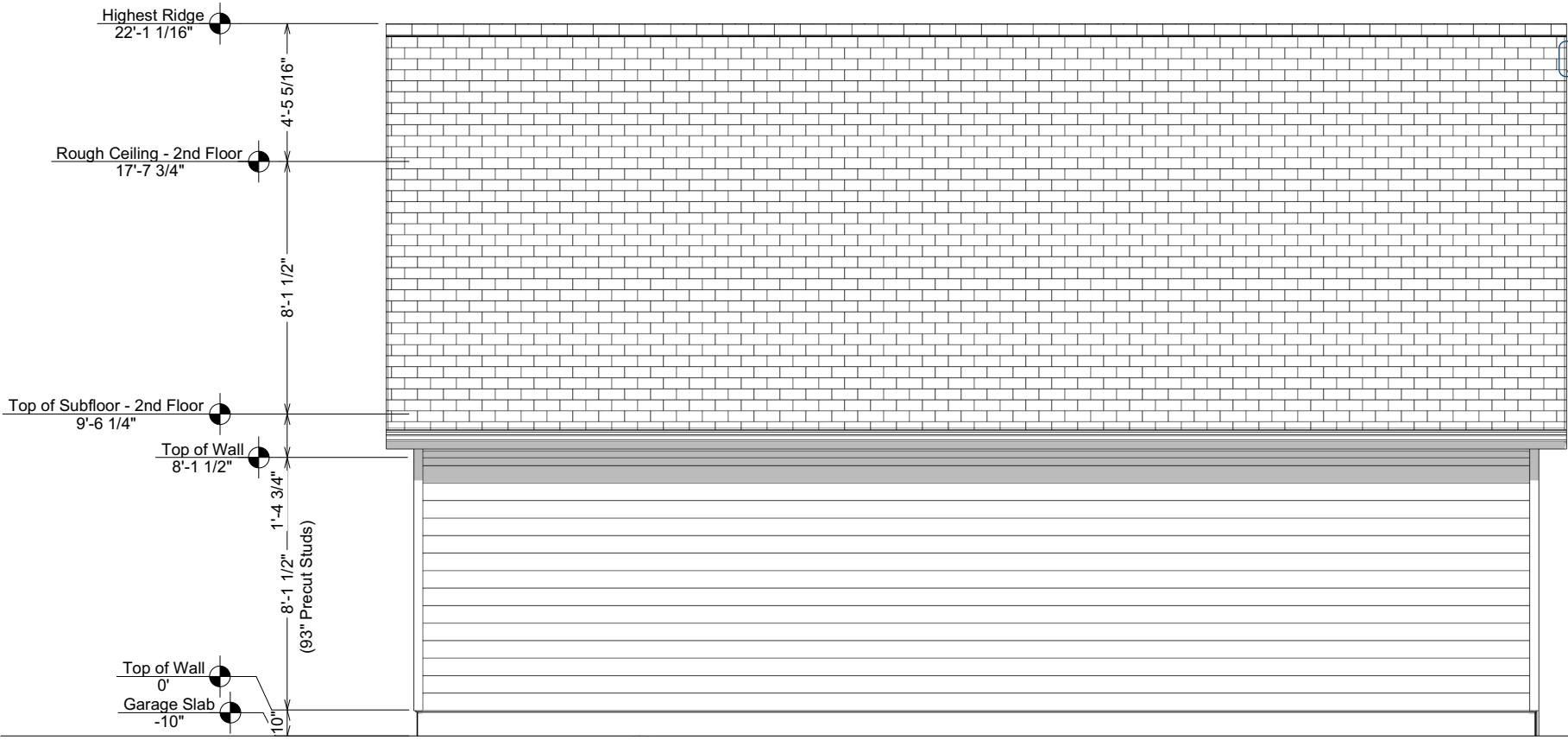
SHEET #
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PROJECT NUMBER:
21DDJ-1007A
10/9/2021
DAJ
Checked By: DM



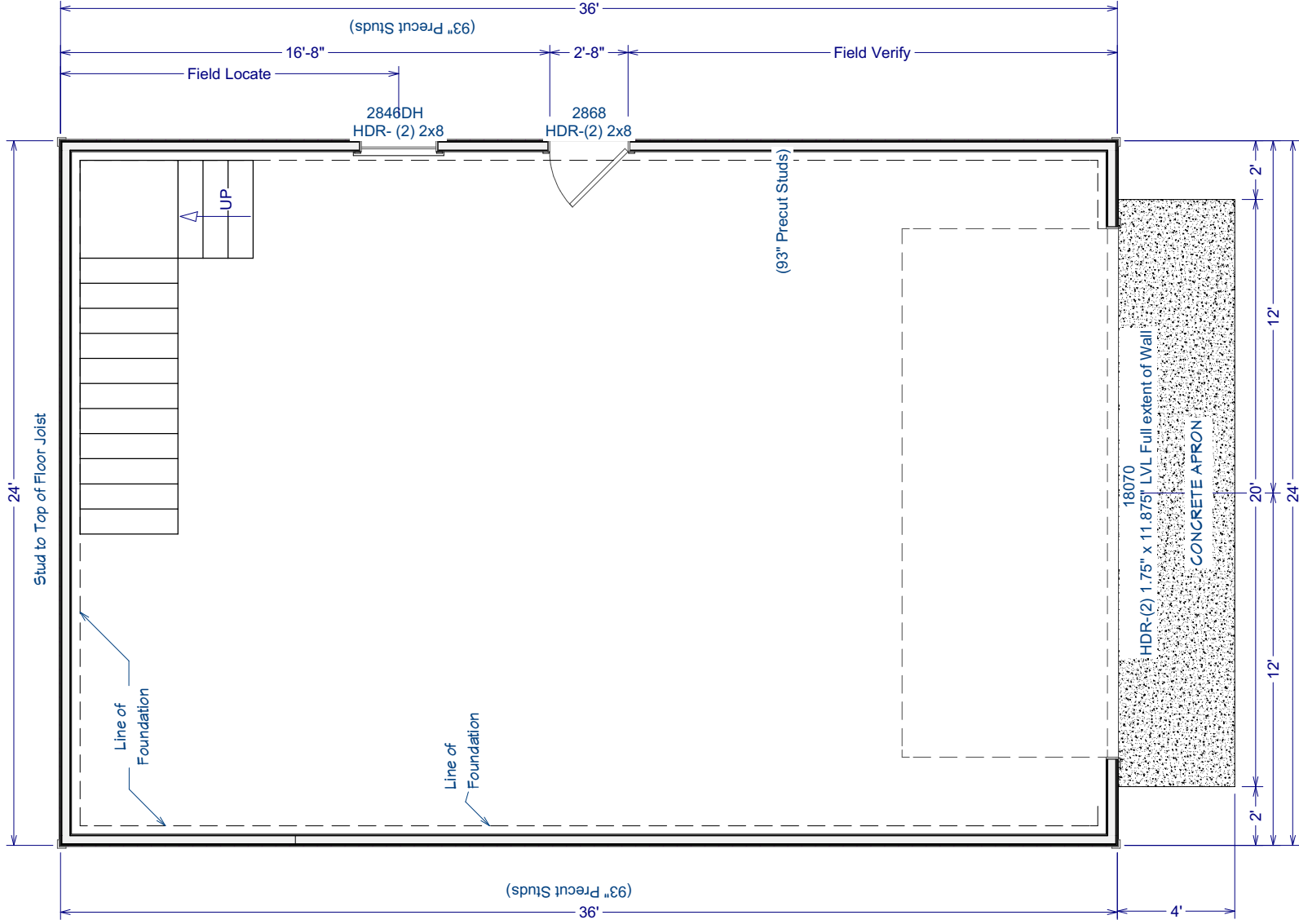
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EXTERIOR ELEVATION LEFT

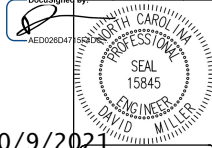
SCALE: 3/16 in = 1 ft



MAIN FLOOR PLAN
SCALE: 3/16 in = 1 ft

SHEET #
7

PROJECT NUMBER:
21DDJ-1007A
10/9/2021
DAJ
Checked By: DM

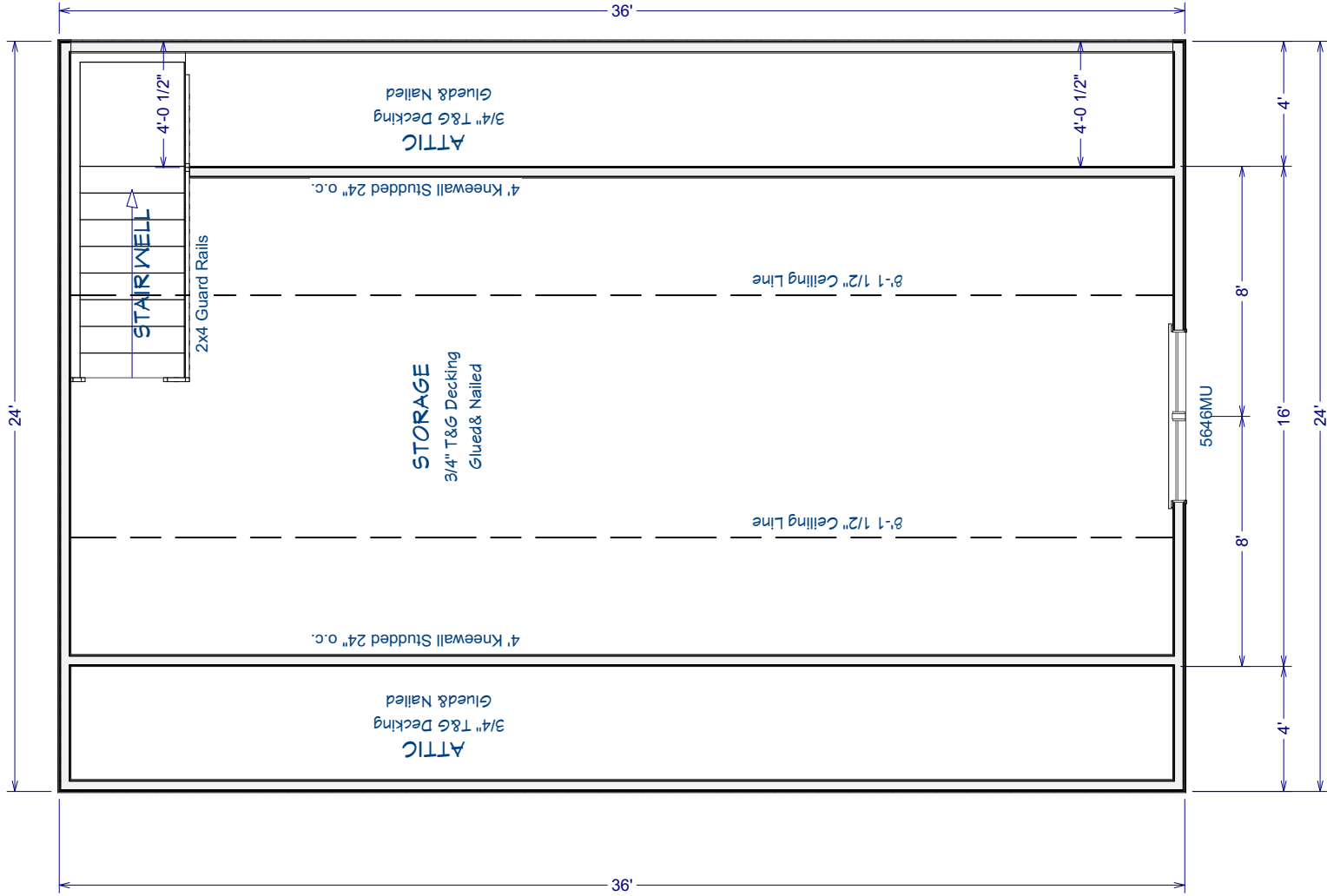


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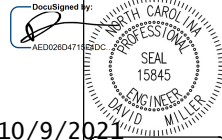




UPPER FLOOR PLAN

SCALE: 3/16 in = 1 ft

10/9/2021

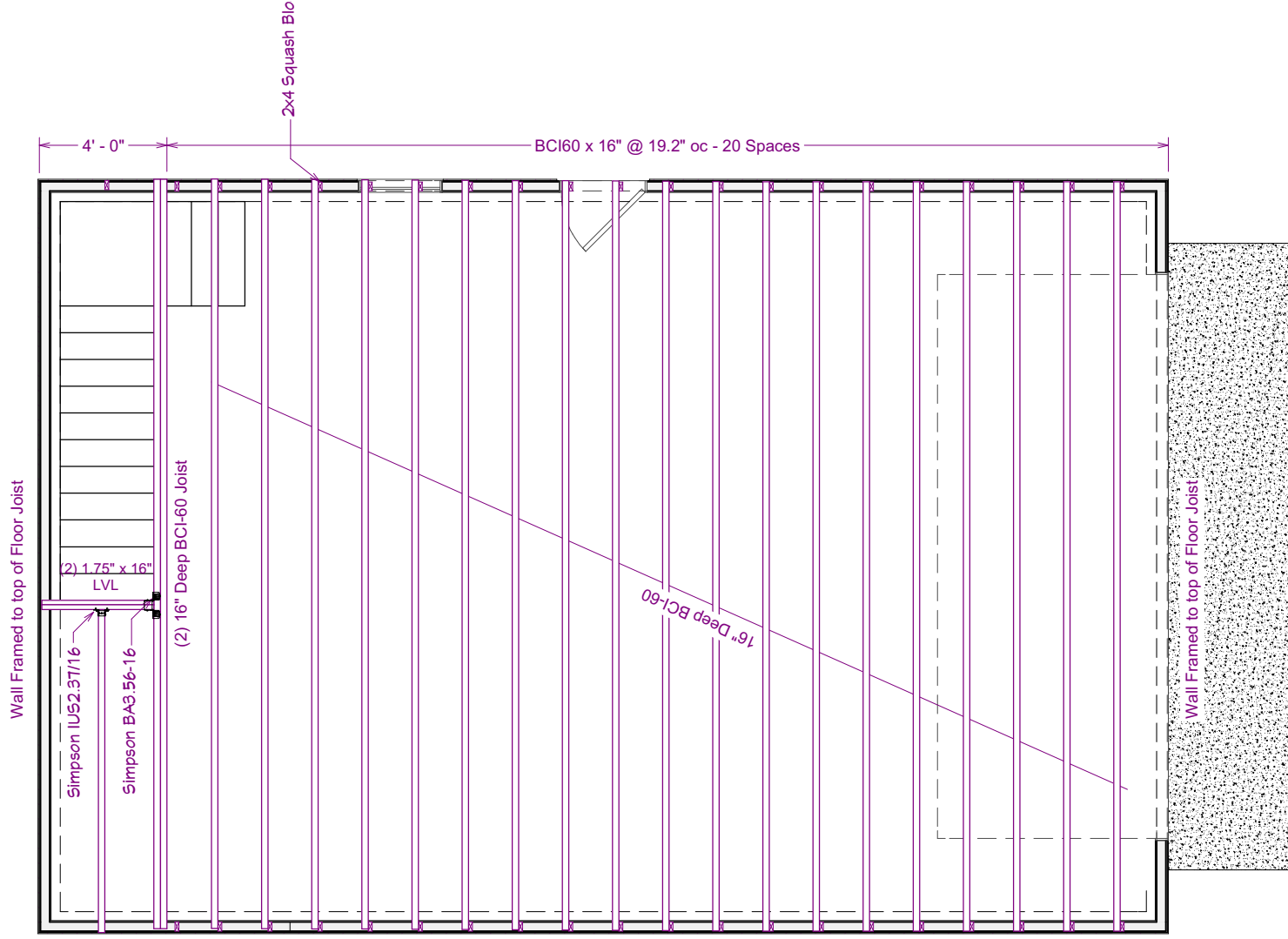


PROJECT NUMBER: 21DDJ-1007A	SHEET # 8
DATE: 10/9/2021	Checked By: DAJ
	Checked By: DM

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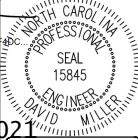
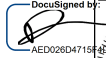
FLOOR FRAMING PLAN
 SCALE: 3/16 in = 1 ft



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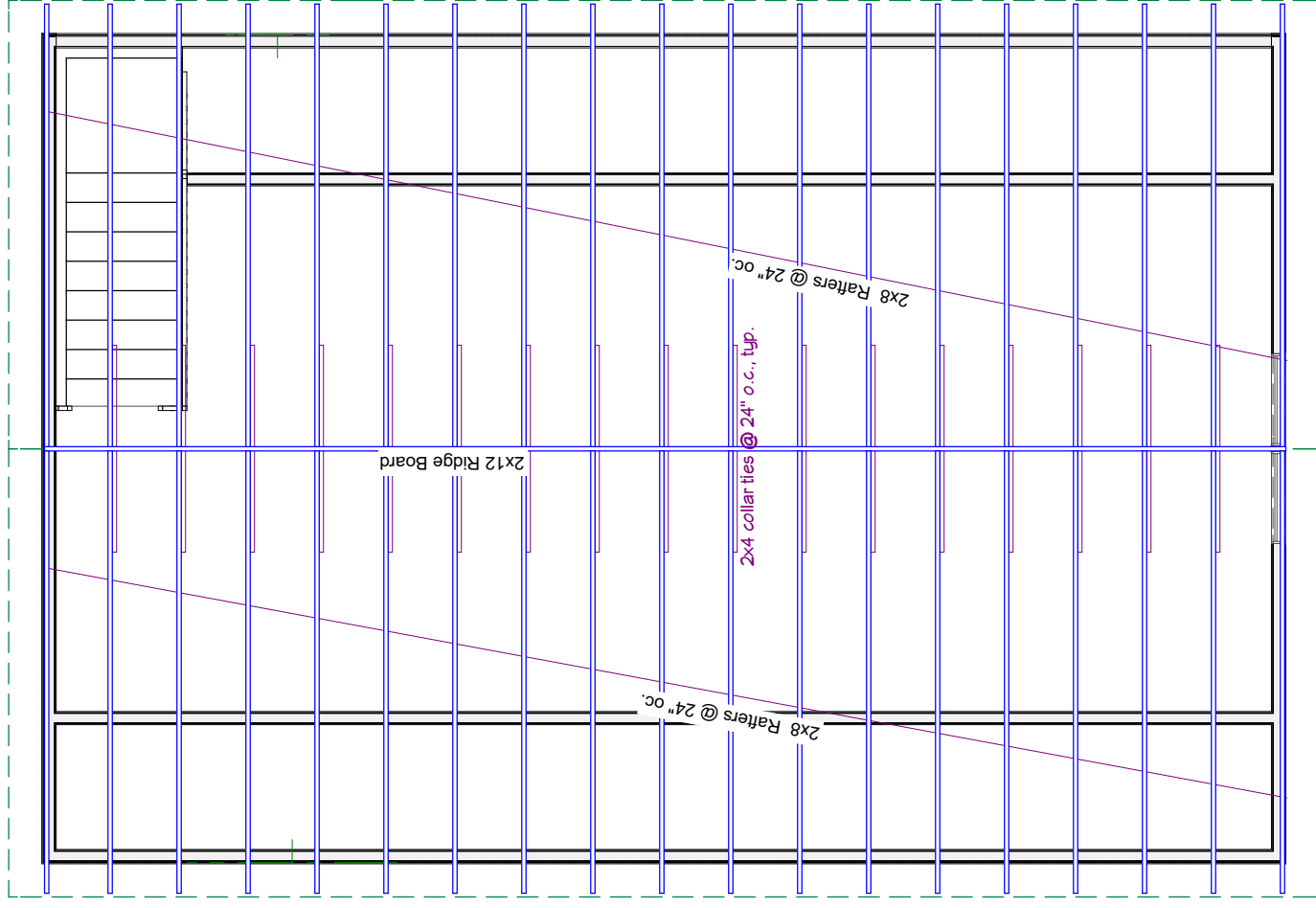
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10/9/2021



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DESIGNED BY:	DM

SHEET #
9



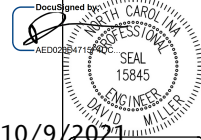
ROOF FRAMING PLAN
 SCALE: 3/16 in = 1 ft



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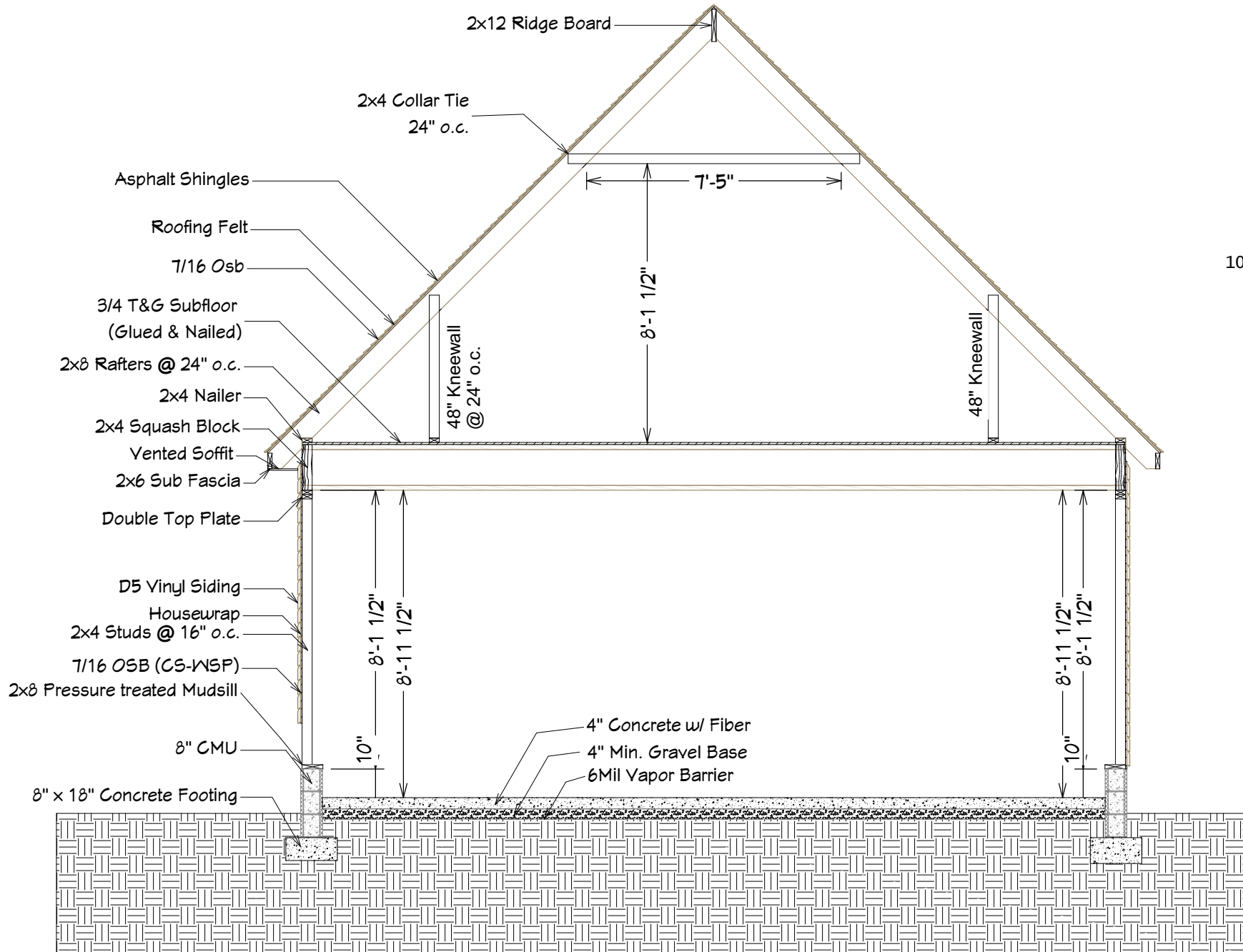
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CHECKED BY: DAJ
DESIGNED BY: DM

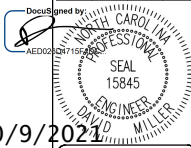
SHEET #
10



FRONT SECTION
SCALE: 1/4 in = 1 ft

SHEET #
11

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Method PFG (Portal Frame at Garage Door Openings)

GENERAL STRUCTURAL NOTES:

- CONSTRUCTION SHALL CONFORM TO THE 2018 NC Residential Code.
- CONTRACTOR SHALL VERIFY ALL DIMENSIONS. CONTRACTOR SHALL COMPLY WITH THE CONTENTS OF THE DRAWINGS FOR THIS SPECIFIC PROJECT. ENGINEER IS NOT RESPONSIBLE FOR ANY DEVIATIONS FROM THIS PLAN.
- CONTRACTOR IS RESPONSIBLE FOR PROVIDING TEMPORARY BRACING REQUIRED TO RESIST ALL FORCES ENCOUNTERED DURING ERECTION.
- THE FOLLOWING DESIGN LOADS ARE USED:

DESIGN LOADS	Live Load (PSF)	Dead Load (PSF)	Deflection (LL)
Usage			
Attics Without Storage	10	----	L/240
Attics With Limited Storage	20	10	L/360
Attics With Fixed Stairs	40	10	L/360
Balconies & Decks	40	10	L/360
Fire Escapes	40	10	L/360
Guardrails and Handrails	200	----	----
Guardrail In-fill components	50	----	----
Passenger vehicle garages	50	10	L/360
Rooms other than sleeping	40	10	L/360
Sleeping rooms	30	10	L/360
Stairs	40	----	----
Snow	20	----	----

- PROPERTIES USED IN THE DESIGN ARE AS FOLLOWS:
MICROLLAM (LVL): F = 2600 PSI, E = 285 PSI, E = 190,000,000 PSI
PARALLAM (PSL): F = 2900 PSI, F = 290 PSI, E = 125,000,000 PSI
- ALL WOOD MEMBERS SHALL BE #2 SPF UNLESS NOTED ON PLANS. ALL STUD COLUMNS AND JOISTS SHALL BE #2 SPF UNLESS NOTED OTHERWISE.
- ALL BEAMS SHALL BE SUPPORTED WITH A (2) 2X4 #2 SPF STUD COLUMN AT EACH END UNLESS NOTED OTHERWISE.
- ALL PARALLEL NON-LOAD BEARING WALLS SHALL BE SUPPORTED WITH A DOUBLE JOIST UNLESS NOTED OTHERWISE.
- COMPRESSIVE STRENGTH OF CONCRETE SHALL BE A MINIMUM OF 3000 PSI / 28-DAYS.
- SOIL BEARING CAPACITY TO BE A MINIMUM OF 2000 PSF
- ALL REINFORCING STEEL SHALL BE GRADE 60 BARS CONFORMING TO ASTM A615 AND SHALL HAVE A MINIMUM COVER OF 3".
- FOOTINGS AND PIERS SHALL BE CENTERED AROUND THEIR RESPECTIVE ELEMENTS. PROVIDE A MINIMUM OF 3" FOOTING PROJECTION FROM FACE OF MASONRY.
- MAXIMUM DEPTH OF UNBALANCED FILL AGAINST MASONRY WALLS TO BE AS SPECIFIED IN THE 2018 NC Residential Code TABLE R404.1.1(1).
- FOUNDATION ANCHORAGE SHALL BE CONSTRUCTED PER 2018 NC Residential Code SECTION 403.1.6. 1/2" DIA. BOLTS SPACED AT 6'-0" CENTERS WITH A 7" MINIMUM EMBEDMENT INTO MASONRY OR CONCRETE. ANCHOR BOLTS SHALL BE 12" FROM THE END OF EACH PLATE SECTION.
- COMPONENT & CLADDING DESIGNED FOR THE FOLLOWING LOADS: (IN PSF)

MEAN ROOF HT	UP TO 30'	30'-35'	35'-40'	40'-45'
ZONE 1	16.5, -18.0	17.3, -18.9	18.0, -19.6	18.5, -20.2
ZONE 2	16.5, -21.0	17.3, -22.1	18.0, -22.9	18.5, -23.5
ZONE 3	16.5, -21.0	17.3, -22.1	18.0, -22.9	18.5, -23.5
ZONE 4	18.0, -19.5	18.9, -20.5	19.6, -21.3	20.2, -21.8
ZONE 5	18.0, -24.1	18.9, -25.3	19.6, -26.3	20.2, -27.0

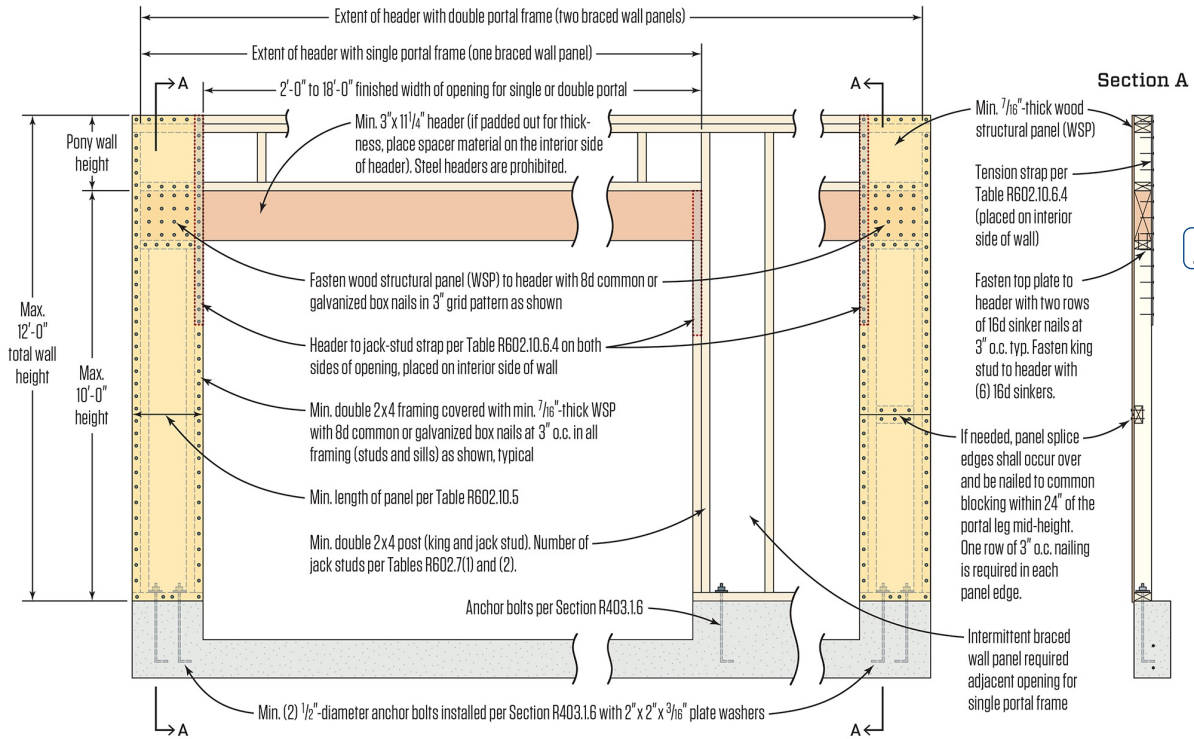
BASIC DESIGN WIND VELOCITY = 110 MPH, EXPOSURE B

- CONTRACTOR TO PROVIDE LOOKOUTS WHEN CEILING JOISTS SPAN PERPENDICULAR TO RAFTERS.
- FLITCH BEAMS AND 3- OR 4-PLY LVLS SHALL BE BOLTED TOGETHER W/ 1/2" THRU BOLTS SPACED @ 24" O.C. (MAX) STAGGERED. MINIMUM EDGE DISTANCE SHALL BE 2" AND (2) BOLTS SHALL BE LOCATED MINIMUM 6" FROM EACH END OF BEAM.
- ALL NON-LOAD BEARING INTERIOR DOOR HEADERS SHALL BE (2) FLAX 2" DROPPED, U.N.O.
- ALL LOAD BEARING HEADERS SHALL BE (2) 2X10 SPF UNO.

TS - TIMBER STRAND
SC - STUD COLUMN
EE - EACH END
TJ - TRIPLE JOIST
CL - CENTER LINE

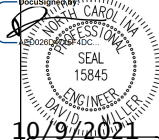
DJ - DOUBLE JOIST
DR - DOUBLE RAFTER
TR - TRIPLE RAFTER
OC - ON CENTER
PL - POINT LOAD

Front Elevation



SHEET #
12

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