



Town of Erwin

Zoning Application & Permit

Planning & Inspections Department

| |
|----------|
| Permit # |
| 26-191 |

Rev Sep2014

Each application should be submitted with an attached plot/site plan with the proposed use/structure showing lot shape, existing and proposed buildings, parking and loading areas, access drives and front, rear, and side yard dimensions.

| | | | |
|-------------------|--------------------------|------------------|--|
| Name of Applicant | ECLS Global % Jack Hobbs | Property Owner | Jackson Family Enterprises %Michael R. Jackson, Sr |
| Home Address | 19 N. McKinley Street | Home Address | 111 Denim Dr. |
| City, State, Zip | Coats, NC 27521 | City, State, Zip | Erwin, NC 28339 |
| Telephone | 843.949.4854 | Telephone | 910.890.4296 |
| Email | jackh@eclsglobalinc.com | Email | michaeljackson.sr@gmail.com |

| | | | |
|--|--|---|-------------|
| Address of Proposed Property | 503 E. Jackson Boulevard, Erwin | | |
| Parcel Identification Number(s) (PIN) | 1507-43-3194.000 | Estimated Project Cost | \$1,200,000 |
| What is the applicant requesting to build / what is the proposed use of the subject property? Be specific. | 664 SF Scooters Coffee Shop | | |
| Description of any proposed improvements to the building or property | Parking lot & landscaping | | |
| What was the Previous Use of the subject property? | N/A (Vacant) | | |
| Does the Property Access DOT road? | Yes (See access permit) | | |
| Number of dwelling/structures on the property already | 1 | Property/Parcel size | 21,590 SF |
| Floodplain SFHA Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> | Watershed Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> | Wetlands Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> | |
| MUST circle one that applies to property | Existing/Proposed Septic System Or Existing/Proposed County/City Sewer | | |

Owner/Applicant Must Read and Sign

The undersigned property owner, or duly authorized agent/representative thereof certifies that this application and the forgoing answers, statements, and other information herewith submitted are in all respects true and correct to the best of their knowledge and belief. The undersigning party understands that any incorrect information submitted may result in the revocation of this application. Upon issuance of this permit, the undersigning party agrees to conform to all applicable town ordinances, zoning regulations, and the laws of the State of North Carolina regulating such work and to the specifications of plans herein submitted. The undersigning party authorizes the Town of Erwin to review this request and conduct a site inspection to ensure compliance to this application as approved.

| | | |
|----------------------|--------------------------------------|---------|
| Michael Jackson, Sr. | | 3/16/26 |
| Print Name | Signature of Owner or Representative | Date |

For Office Use

| | | | |
|--------------------|-------|---|---|
| Zoning District | B2 | Existing Nonconforming Uses or Features | NA |
| Front Yard Setback | | Other Permits Required | <input type="checkbox"/> Conditional Use <input checked="" type="checkbox"/> Building <input checked="" type="checkbox"/> Fire Marshal <input type="checkbox"/> Other |
| Side Yard Setback | 30 ft | Requires Town Zoning Inspection(s) | <input checked="" type="checkbox"/> Foundation <input checked="" type="checkbox"/> Prior to C. of O. |
| Rear Yard Setback | 0 ft | Zoning Permit Status | <input checked="" type="checkbox"/> Approved <input type="checkbox"/> Denied |
| | 20 ft | Fee Paid: \$100 | Date Paid: Staff Initials: |

| | |
|----------|-------------------------------|
| Comments | Trade permit from Harnett Co. |
|----------|-------------------------------|

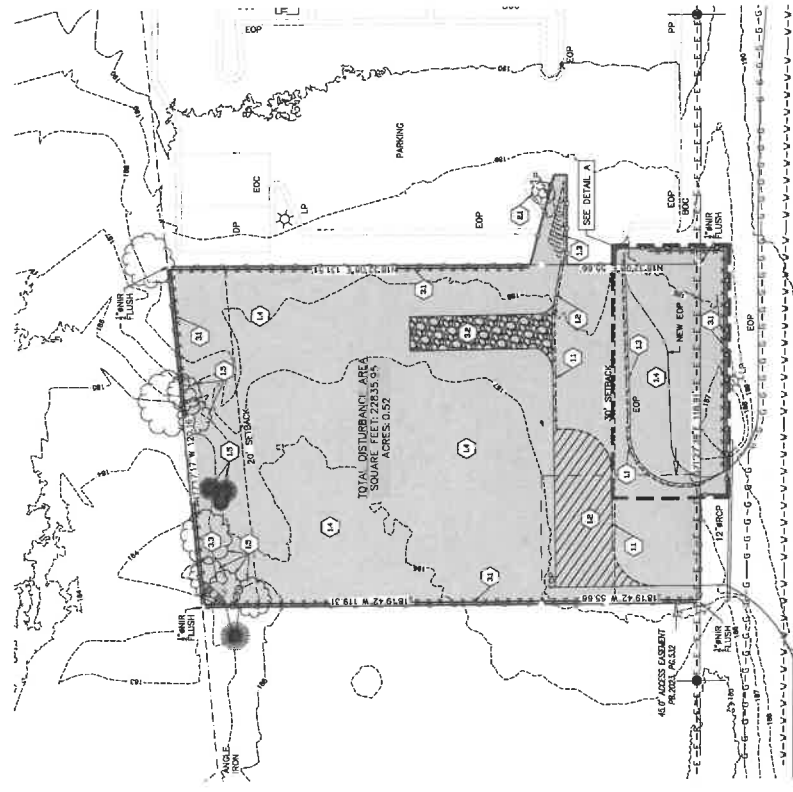
| | |
|-----------------------------------|------------------------------|
| Signature of Town Representative: | Date Approved/Denied: 4/9/26 |
|-----------------------------------|------------------------------|

Has received approval from the Fire Marshal, GIS and NCDOT, Harnett

Structure may not obtain C/O until water line & hydrant have been placed and accepted by all agencies (Harnett & NCDOT)



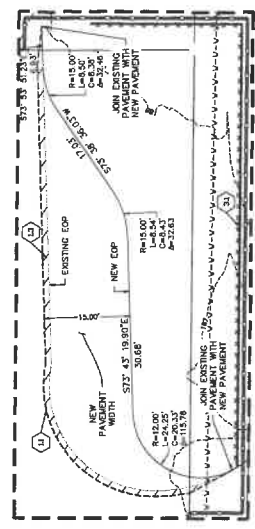
- LEGEND:**
- OF CURB
 - EDGE OF PAVEMENT
 - EXISTING/PROPOSED CONCRETE PIPE
 - ADJOURNING LINE
 - SANITARY SEWER LINE
 - WATER LINE
 - UNIT DISBURSANCE
 - WATER LINE
 - POWER LINE
 - POWER LINE
 - WATERSHED (VALLEY)
 - CHARGES METER
 - EXISTING/PROPOSED
 - EXISTING ROAD (ER)
 - MAG NAIL SET (MNS)
 - EXISTING MAG NAIL (EM)
 - ABOVE/BELOW GROUND (CP)
 - ABOVE/BELOW GROUND
 - TREE
 - PINE



US HWY 421 E JACKSON BLVD
 150' PUBLIC R/W PB.2023, PG.532

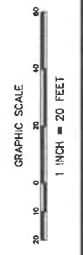
- KEYNOTES:**
- 1.1. SAWCUT PAVEMENT.
 - 1.2. REMOVE BITUMINOUS PAVEMENT.
 - 1.3. REMOVE CURB AND GUTTER.
 - 1.5. REMOVE TREE.
 - 2.1. PROTECT EXISTING UTILITIES AND STRUCTURES.
 - 3.1. EROSION CONTROL ITEMS. E1/4.
 - 3.2. TEMPORARY GRAVEL CONSTRUCTION ENTRANCE EXIT - SEE DETAIL E2/4.
 - 3.3. SEDIMENT TRAP - SEE DETAIL E3/4.

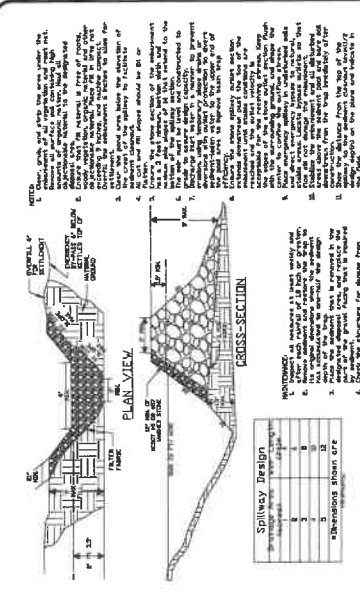
- NOTES:**
1. CLEAR AND GRUB WITHIN CONSTRUCTION LIMITS AS REQUIRED FOR CONSTRUCTION.
 2. CONSTRUCTION SITE SHALL HAVE STABILIZED EXIT AT ALL TIMES TO PREVENT EROSION. THE OWNER OR PROJECTS CONTRACTOR IS RESPONSIBLE TO PROTECT DOWNSTREAM WATERS FROM CONSTRUCTION RUNOFF. AND SILT FENCES ARE SHOWN APPLICABLE. OFFSET FROM PROPERTY LINE FOR CLARITY, WHERE APPLICABLE.
 3. CONSTRUCTION OFFSETS AS NECESSARY PER GEOTECH REPORT AND FIELD ENGINEER, IF APPLICABLE.



- CONSTRUCTION SEQUENCE**
1. INSTALL TEMPORARY CONSTRUCTION ENTRANCE.
 2. INSTALL SILT FENCE.
 3. GRADE SITE.
 4. MAINTAIN ALL TEMPORARY MEASURES UNTIL PERMANENT EROSION CONTROL MEASURES ARE ESTABLISHED. NO MEASURES ARE TO BE REMOVED UNTIL DENIAL APPROVAL.

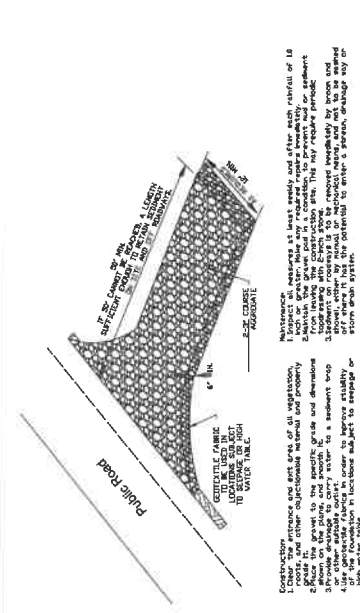
SCALE: 1" = 10'





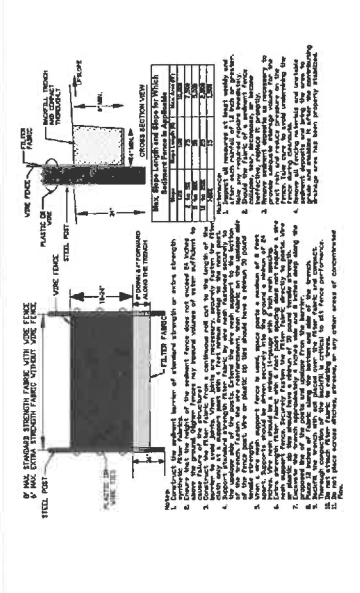
NOTES:

1. SPILLWAY SHALL BE CONSTRUCTED AS SHOWN.
2. SPILLWAY SHALL BE CONSTRUCTED WITH A 1:1 SLOPE ON THE RIGHT SIDE AND A 2:1 SLOPE ON THE LEFT SIDE.
3. SPILLWAY SHALL BE CONSTRUCTED WITH A 6" GRAVEL FILTER FABRIC ON THE TOP SURFACE.
4. SPILLWAY SHALL BE CONSTRUCTED WITH A 6" GRAVEL FILTER FABRIC ON THE BOTTOM SURFACE.
5. SPILLWAY SHALL BE CONSTRUCTED WITH A 6" GRAVEL FILTER FABRIC ON THE INSIDE SURFACE.
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12. SPILLWAY SHALL BE CONSTRUCTED WITH A 6" GRAVEL FILTER FABRIC ON THE OUTSIDE SURFACE.



NOTES:

1. GRAVEL CONSTRUCTION ENTRANCE EXIT SHALL BE CONSTRUCTED AS SHOWN.
2. GRAVEL CONSTRUCTION ENTRANCE EXIT SHALL BE CONSTRUCTED WITH A 1:1 SLOPE ON THE RIGHT SIDE AND A 2:1 SLOPE ON THE LEFT SIDE.
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NOTES:

1. SILT FENCE SHALL BE CONSTRUCTED AS SHOWN.
2. SILT FENCE SHALL BE CONSTRUCTED WITH A 1:1 SLOPE ON THE RIGHT SIDE AND A 2:1 SLOPE ON THE LEFT SIDE.
3. SILT FENCE SHALL BE CONSTRUCTED WITH A 6" GRAVEL FILTER FABRIC ON THE TOP SURFACE.
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11. TEMPORARY SEDIMENT TRAP
 NTS

12. TEMPORARY GRAVEL CONSTRUCTION ENTRANCE EXIT
 NTS

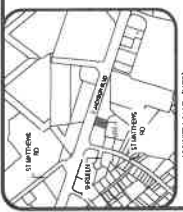
13. SILT FENCE
 NTS



REVISIONS:

SCOOTER'S COFFEE
 MICHAEL R. JACKSON, P.E.
 803 E. JACKSON BLVD.
 RRMH, NC

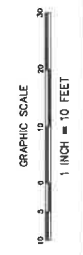
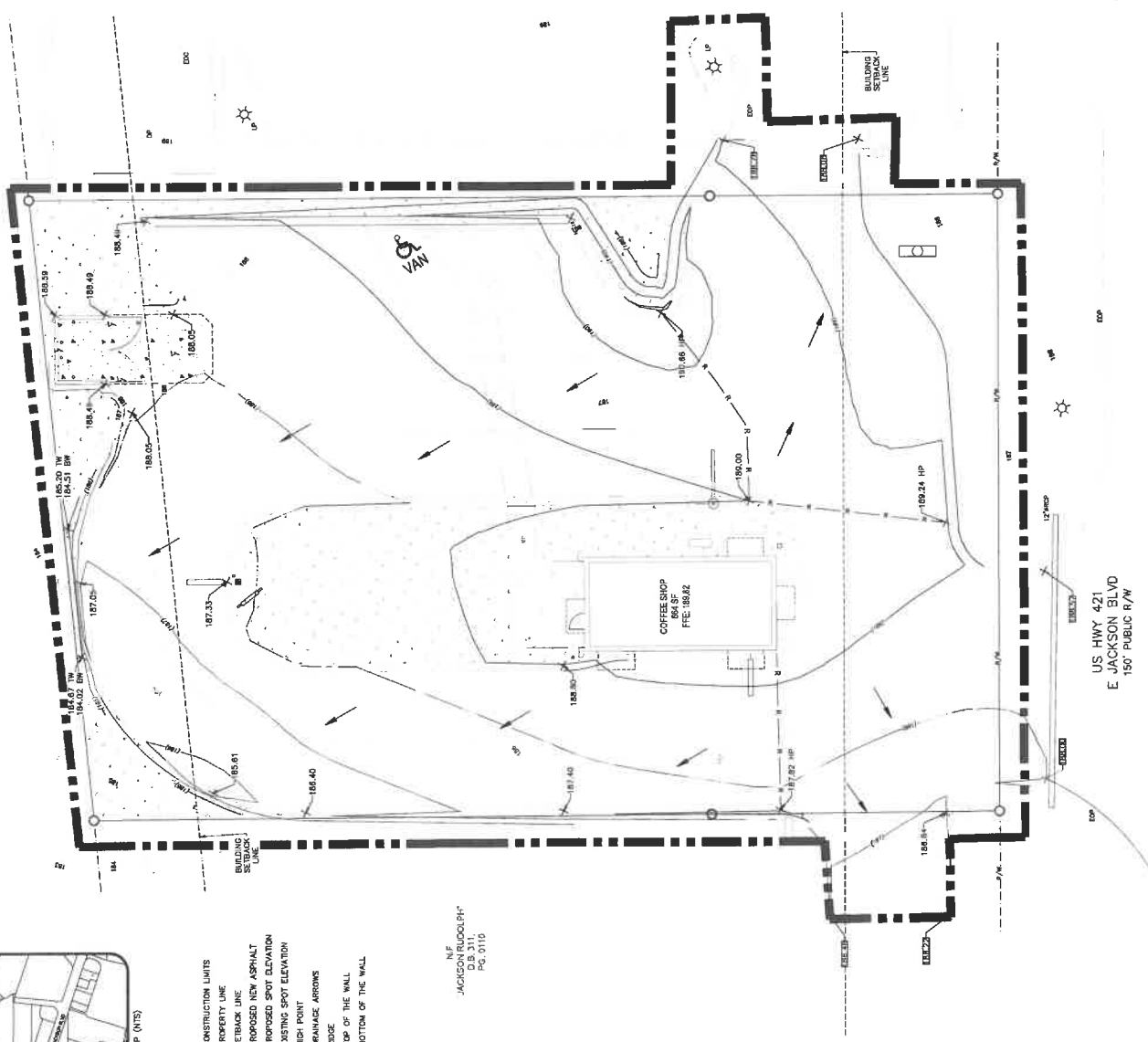
| | |
|-------------|------------|
| PROJECT NO. | 27417 |
| DATE | 08/27/2008 |
| SCALE | 1" = 10' |
| SHEET NO. | 6 OF 11 |



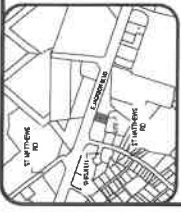
VICINITY MAP (N/S)

- LEGEND:**
- CONSTRUCTION LIMITS
 - PROPERTY LINE
 - SETBACK LINE
 - PROPOSED NEW ASPHALT
 - +502.79 PROPOSED SPOT ELEVATION
 - 102.22 EXISTING SPOT ELEVATION
 - HP HIGH POINT
 - DRAINAGE ARROWS
 - R --- RISE
 - TW --- TOP OF THE WALL
 - BW --- BOTTOM OF THE WALL

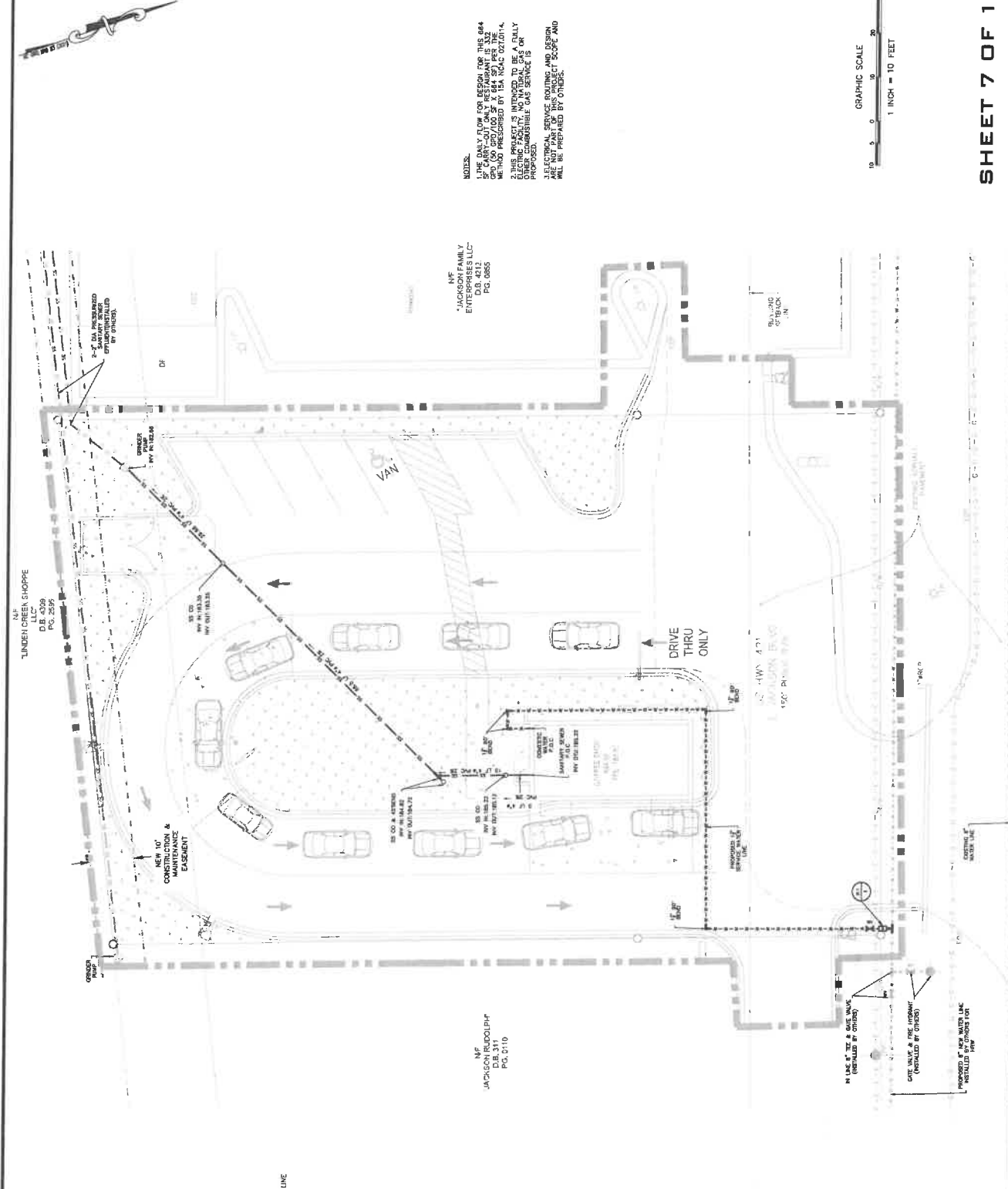
N.F.
 JACKSON FLOODPH
 PG. 0110



US HWY 421
 E JACKSON BLVD
 150' PUBLIC R/W



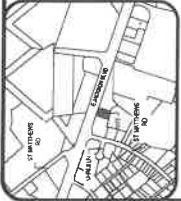
- LEGEND:**
- CONSTRUCTION LIMITS
 - PROPERTY LINE
 - SETBACK LINE
 - ADJACENT LINE
 - EASEMENT LINE
 - PROPOSED WATER LINE
 - PROPOSED SANITARY SEWER LINE
 - PROPOSED NEW ASPHALT
 - PROPOSED CONCRETE
 - PROPOSED PLANTING
 - CLEANDIRT
 - CO



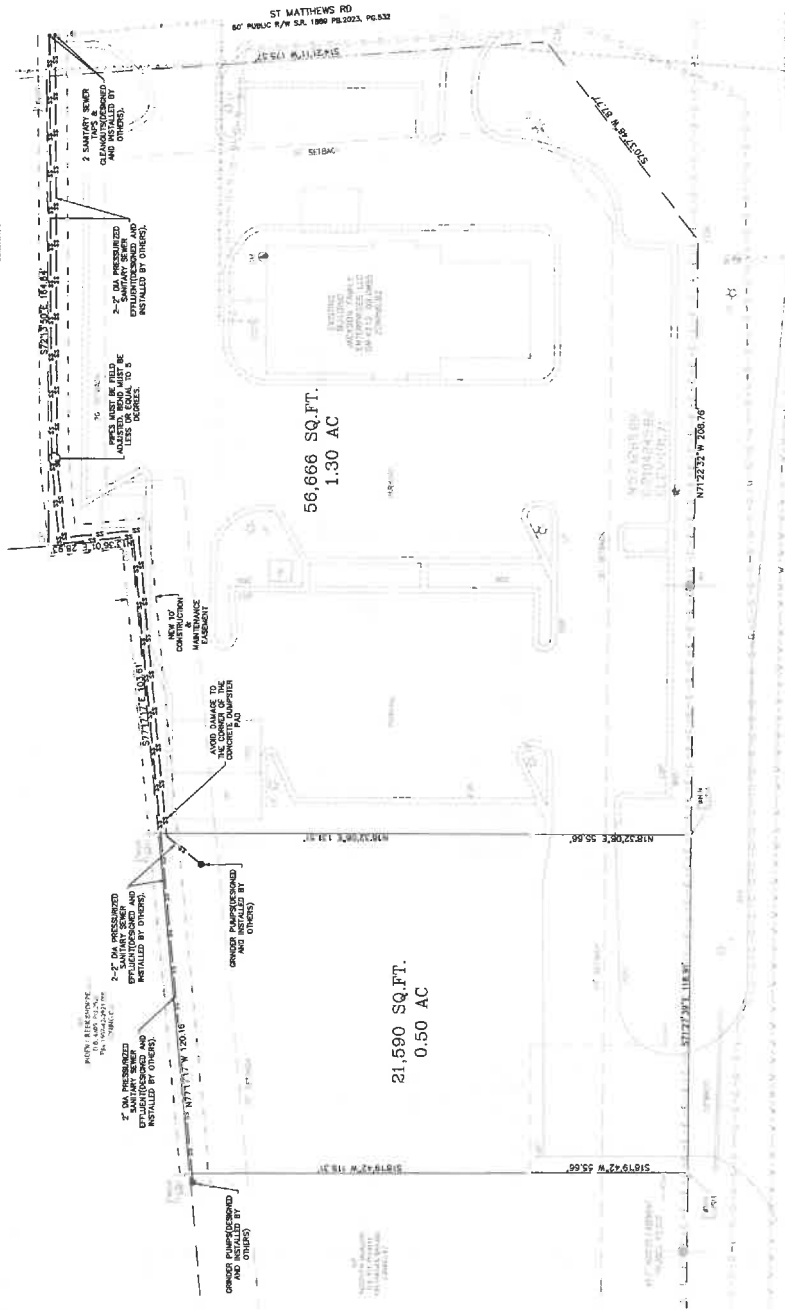
NOTES:

1. THE ONLY ROW FOR DESIGN FOR THIS 664 SF CARRY-OUT ONLY RESTAURANT IS 332 METHOD PRESCRIBED BY 23A R.O.C. 02/01/14.
2. THIS PROJECT IS INTENDED TO BE A FULLY OPERATIONAL AND VETERAN-OWNED BUSINESS.
3. ALL ELECTRICAL SERVICE, INCLUDING AND DESIGN WILL BE PREPARED BY OTHERS.

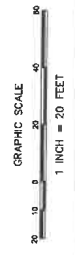




VICINITY MAP (N15)



NOTE:
 AN EASEMENT PLAT AND DESIGN WILL BE PREPARED PER NEW SPECIFICATIONS FOLLOWING NOTICE OF THE PROCEEDINGS OF THE GRINDER PUMP AND EFFLUENT PUMP LOCATION.

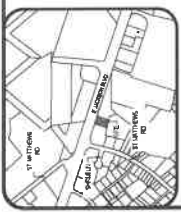
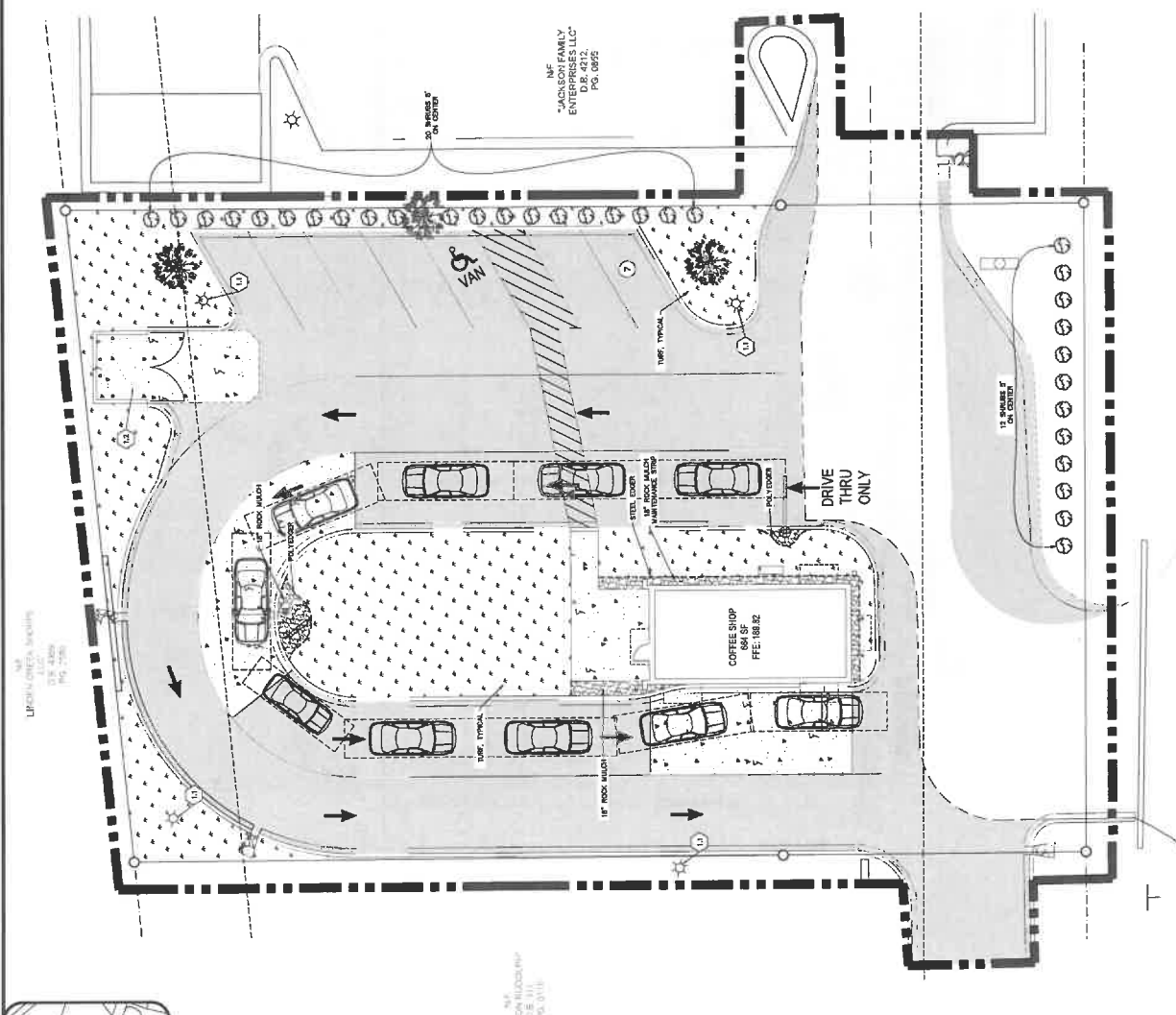




- LEGEND**
- CONSTRUCTION LIMITS
 - PROPERTY LINE
 - SETBACK LINE
 - ▨ PROPOSED NEW ASPHALT
 - ▩ PROPOSED CONCRETE
 - ▧ PROPOSED TURF
 - 🌳 PROPOSED SHADE TREE
 - 🌿 PROPOSED SHRUBS
 - ⊙ PROPOSED PARKING COUNT
 - ⊞ ROCK MULCH
 - EDGING

- NOTES:**
1. AS PER SEC. 30-214 (10) OF THE TOWN CODE 1.0P ORDINANCES:
 - A. SHADE TREES SHALL BE ALLOWED ON OR WITHIN THE SETBACK LINE.
 - B. ORNAMENTAL TREES SHALL BE SHAPE PLANTING.
 - C. TREES SHALL BE 4" DIA. OR EQUAL AND 3' SAUCER AT PLANTING OR EQUAL.
 - D. OR EDGING.

- NOTES:**
1. THE TRASH AREA SHALL BE SCREENED SO AS TO BE UNOBSERVABLE FROM THE ADJACENT STREETS AND PROPERTIES.
 2. SHADE TREES PROPOSED: 3 TOTAL IN LOCATIONS SHOWN.
 3. SHRUBS WILL BE PLANTED AT A MINIMUM OF 1' ON EACH SIDE OF THE PROPOSED SETBACK LINE IN LOCATIONS SHOWN.



US HWY 421
 E JACKSON BLVD
 150' PUBLIC R/W

HARNETT REGIONAL WATER

Erwin US 421

Water Line Extension 2026

ERWIN, NORTH CAROLINA

GOVERNING AGENCIES

ENGINEERING

HARNETT REGIONAL WATER
700 MCKINNEY PARKWAY
LILLINGTON, NORTH CAROLINA 27546
PHONE: 919-841-7575
WWW.HARNETTREGIONALWATER.ORG
CONTACT: KENNY REBEL, PE, UTILITY ENGINEER

NCDOT

DIVISION 6, DISTRICT 2
POB 1189
600 SOUTHERN AVENUE
FAYETTEVILLE, NORTH CAROLINA 28402 (MAIL)
PHONE: 315-584-9001
REGUL@LRRHINE@NCDOT.GOV
CONTACT: MICHEL BIRNES, PE

WATER SYSTEM

PERMITTING

NCDEQ-PWS
100 WEST STREET
RALEIGH, NORTH CAROLINA 27699-1634
PHONE: 919-737-9181
EMAIL: SHAWN.GUYER@DEQ.NC.GOV
CONTACT: SHAWN GUYER, PE

EROSION CONTROL

NCDEQ - DEMLR
FAYETTEVILLE REGIONAL OFFICE
100 WEST STREET
FAYETTEVILLE, NC 28401-6095
PHONE: 910-433-3300
CONTACT: TIM LABOUNTY, PE

TOWN OF ERWIN

100 WEST E STREET
ERWIN, NORTH CAROLINA 28539
PHONE: 919-850-4200
EMAIL: TOWNMGR@ERWIN.NC.GOV
CONTACT: SNOW BOWREN, TOWN MANAGER

DRAWING INDEX

- C-01 COVER SHEET
- G-02 CONSTRUCTION NOTES
- CI-01 OVERALL UTILITY PLAN
- D-01 STANDARD DETAILS

PROJECT TEAM

OWNER

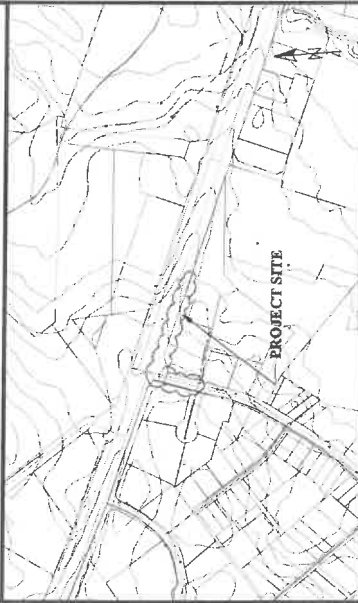
HARNETT REGIONAL WATER
700 MCKINNEY PARKWAY
PO BOX 1119
LILLINGTON, NORTH CAROLINA 27546
PHONE: 919-841-7575
CONTACT: TOMMY BURNS, UTILITY DIRECTOR

ENGINEER

MEYERS ENGINEERING, PLLC
2900 COUNTRY CLUB DRIVE
FLOUVA VARRA, NORTH CAROLINA 27546
PHONE: 919-852-4214
EMAIL: JAY.T.MEYERS@ENGINEERING.NET
CONTACT: JAY T. MEYERS, PE

VICINITY MAP

Scale: NTS



PROFESSIONAL SEAL



MEYERS
ENGINEERING, PLLC
150 LAWRENCE PARK
2900 COUNTRY CLUB DRIVE
FLOUVA VARRA, NORTH CAROLINA 27546
JAY@MEYERSENGINEERING.NET

REVISION

HARNETT REGIONAL WATER
700 MCKINNEY PARKWAY
LILLINGTON, NORTH CAROLINA 27546
919-841-7575
CONTACT: TOMMY BURNS, DIRECTOR

HARNETT REGIONAL WATER
700 MCKINNEY PARKWAY
LILLINGTON, NORTH CAROLINA 27546
919-841-7575
CONTACT: TOMMY BURNS, DIRECTOR

CONSTRUCTION DRAWINGS

ERWIN US 421 WATER LINE EXTENSION 2026

COVER SHEET

| REV | ISSUED FOR | DATE | BY |
|-----|------------|------|----|
| | | | |

CONFORMED DRAWINGS

G-01

GENERAL CONSTRUCTION NOTES

- 1. ALL CONSTRUCTION SHALL BE ACCORDING TO THE HARNETT REGIONAL WATER...
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS...
3. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES...
4. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES...
5. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES...

EROSION & SEDIMENTATION CONTROL NOTES

- 1. TOTAL DISTURBED AREA IS APPROXIMATELY 4.5 ACRES...
2. THE CONTRACTOR SHALL MAINTAIN EROSION CONTROL MEASURES...
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Table with columns: ITEM, DESCRIPTION, QUANTITY, UNIT, and NOTES. Includes items for concrete, steel, and other materials.

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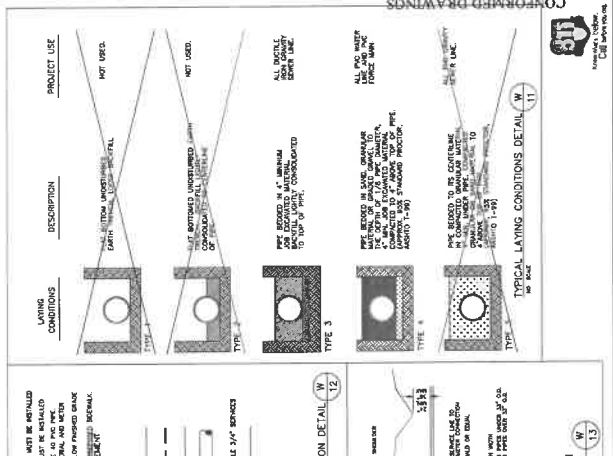
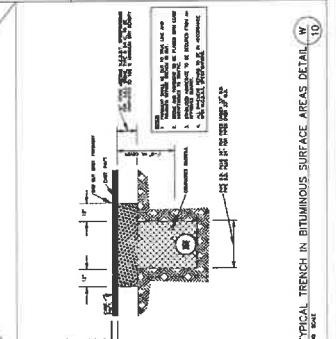
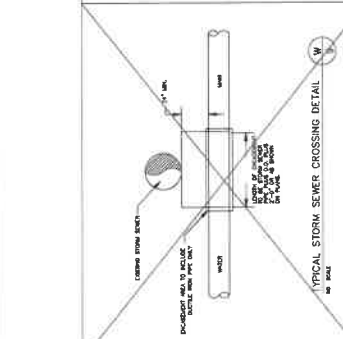
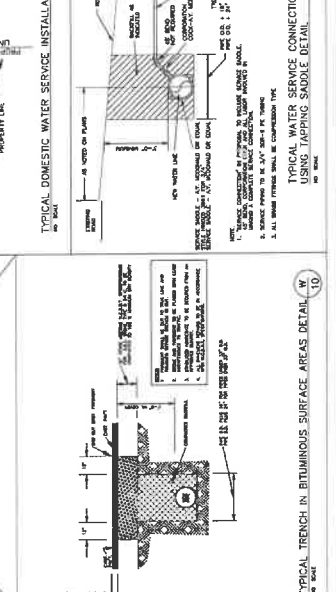
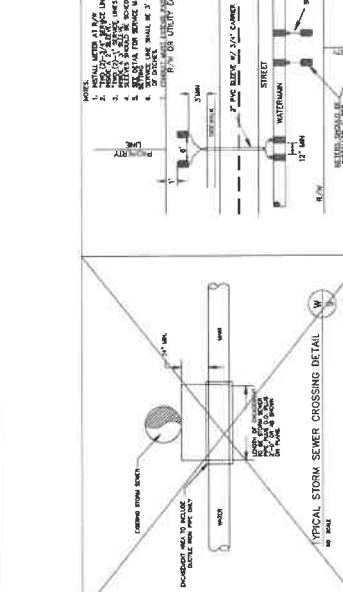
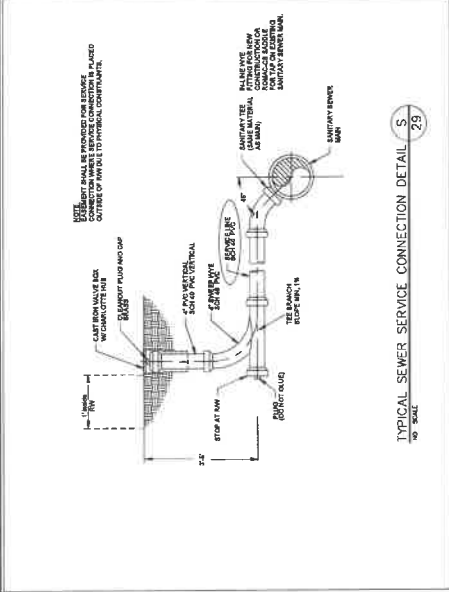
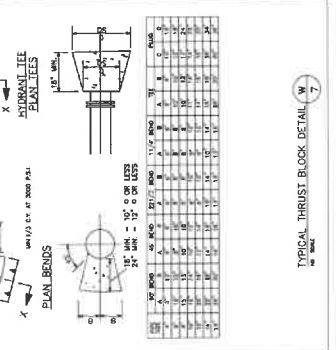
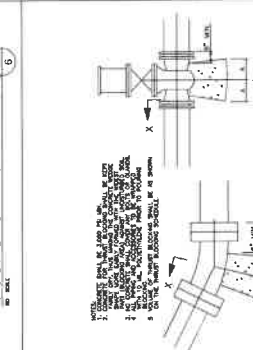
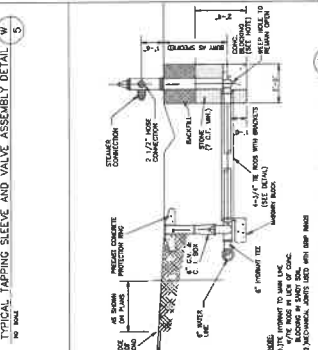
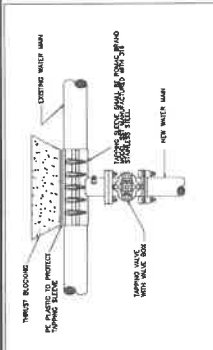
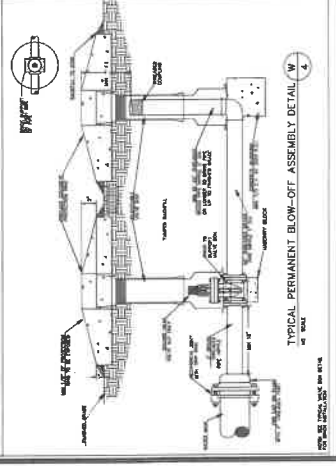
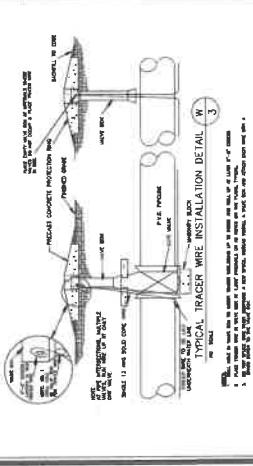
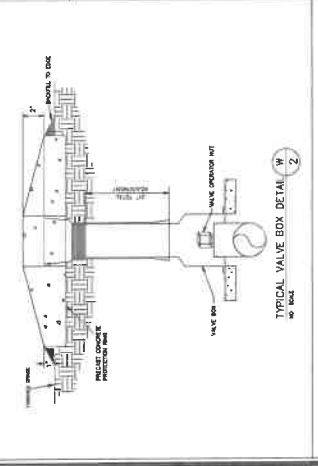
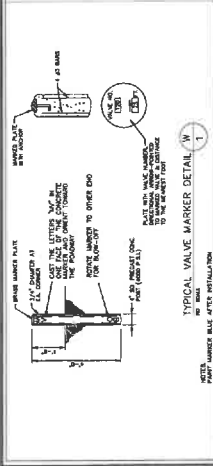
Table with columns: ITEM, DESCRIPTION, QUANTITY, UNIT, and NOTES. Includes items for concrete, steel, and other materials.

Table with columns: ITEM, DESCRIPTION, QUANTITY, UNIT, and NOTES. Includes items for concrete, steel, and other materials.

CONSTRUCTION DRAWINGS
HARNETT REGIONAL WATER
700 MCKINNEY PARKWAY
WELLSBORO, NORTH CAROLINA 27586
CONTACT: TOMMY BURNS, DIRECTOR

MEYERS ENGINEERING, PLLC
INCLOSURE # 204
500 CARRIAGE PARK COURT
WELLSBORO, NORTH CAROLINA 27586
JAY@MEYERSENGINEERING.COM

CONFORMED DRAWINGS
CONSTRUCTION DRAWINGS
ERWIN US 421 WATER LINE EXTENSION 2026
STANDARD NOTES
G-02



MEYERS ENGINEERING, PLLC
REGISTERED PROFESSIONAL ENGINEER
REGISTERED PROFESSIONAL SURVEYOR
REGISTERED PROFESSIONAL LAND SURVEYOR
REGISTERED PROFESSIONAL CIVIL ENGINEER

HARNETT REGIONAL WATER
700 MCKINNEY PARKWAY
WILKINSON, NORTH CAROLINA 27546
910.343.5255
CONTACT: TOMMY BURNS, DIRECTOR

CONSTRUCTION DRAWINGS
ERWIN US 421 WATER LINE EXTENSION 2026
STANDARD DETAILS

REV: _____ ISSUED FOR: _____ DATE: _____ BY: _____



February 10, 2026

Michelle Myatt
Harnett County
420 McKinney Parkway
Lillington, NC 27546

Via email to mmyatt@harnett.org

Re: MB-482/25-205 Scooter's #2946-503 E Jackson Boulevard, Erwin (Harnett Co, NC)
Site Plan Submittal

Dear Ms. Myatt:

The site plan for this project is herewith submitted. It is my understanding that since the zoning is administered by the Town of Erwin that a DRB application form will not be required. Please let me know if there is anything else we need to do to enter or continue the formal review and approval process for this site plan.

As for the wetlands question:

Based upon my visual inspection of the site on January 22, 2026 and review of available mapping and aerial imagery, I observed no physical indicators typically associated with jurisdictional wetlands, including but not limited to hydrophytic vegetation, hydric soils, or evidence of sustained surface water. No pond, wetland feature, or drainage pattern indicative of wetland conditions was observed at the time of inspection.

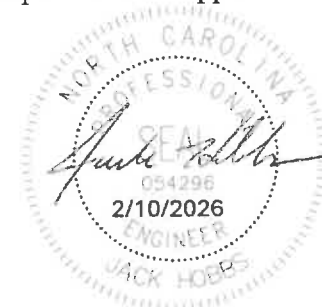
It is my understanding, based on discussions with the property owner and review of historical information, that a former man-made pond may have existed on the site many years ago and was subsequently filled during utility construction activities. Current site conditions do not reflect the presence of wetlands.

Please note that I am not a certified wetlands specialist, and this statement is based solely on a professional engineering site inspection and general field observations. This letter does not constitute a formal jurisdictional wetlands delineation under applicable state or federal regulations.

Based on the observed conditions, I found no evidence of wetlands requiring evaluation, delineation, or protection within the project limits. This letter is provided in support of the DRB application and to document my site inspection findings.

Sincerely

Jack Hobbs, PE
Vice President



Cc: Dylan Eure, Town of Erwin

Florida
312 Avenue K
Winter Haven, FL 33880
863-344-5386

North Carolina
19 N. McKinley Street
Coats, NC 27521
910-897-3257

South Carolina
350 Hilton Rd.; Suite 300
Myrtle Beach, SC 29572
843-949-4890



STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION

JOSH STEIN
GOVERNOR

DANIEL H. JOHNSON
SECRETARY

March 2, 2026

Mr. Michael R. Jackson
Jackson Family Enterprises, LLC
111 Denim Drive
Erwin, NC 28339

SUBJECT: APPLICATION - DRIVEWAY ENTRANCE ONTO US Highway 421 (East Jackson Boulevard) PERMIT NUMBER D062-043-26-00009 (Scooter's Coffee) TOWN OF ERWIN - HARNETT COUNTY

Please find attached for your file a copy of the Driveway Application, which has been properly executed. This Driveway Permit is approved subject to the provisions stated in the Permit Application Agreement and the following special provisions:

- **Driveway construction must be completed within one year after the approval date. This Driveway Permit becomes null and void after that time and the Applicant will be required to reapply for access. Consideration may be given for time extensions on a case by case basis.**
- An executed copy of the approved Driveway Permit, provisions and plans shall be present at the construction site at all times. If safety or traffic conditions warrant such an action, NCDOT reserves the right to further limit, restrict or suspend operations within the right of way.
- **Upon completion of the project, an executed North Carolina Department of Transportation Subdivision Road, Driveway Access and Encroachment Installation Certification Memo shall be returned to the District Engineer's office.**
- **Any change or alteration of the existing or proposed property use shall require a new Driveway Permit. Failure to secure required Permits prior to construction or change in property usage may result in the removal of the driveway at the expense of the Permittee**
- **This approval and associated plans and supporting documents shall not be interpreted to allow any design change or change in the intent of the design by the Owner, Design Engineer, or any of their representatives. Any revisions or changes to these approved plans or intent for construction must be obtained in writing from the Division Engineer's office or their representative prior to construction or during construction, if an issue arises during construction to warrant changes. Changes noted in red on the plans and herein shall be incorporated into and made a part of the approved Permit.**

Mailing Address:
NC DEPARTMENT OF TRANSPORTATION
DIVISION SIX / DISTRICT TWO
POST OFFICE BOX 1150
FAYETTEVILLE, NC 28302

Telephone: (910) 364-0601
Fax: (910) 437-2529
Customer Service: 1-877-368-4968

Location:
600 SOUTHERN AVENUE
FAYETTEVILLE, NC 28306

Website: www.ncdot.gov

- **All out parcels or excluded areas shall be served internally, with no additional access onto abutting roadways permitted. This condition should be conveyed in any buy, sell, lease, rent, or subdivision agreement. A new Driveway Permit review and approval will be required for the development of the adjacent property (PIN 1507-43-7066.000) to the east and the driveway connection (cross access) to the “Scooter’s Coffee” site as approved by the permit. The Developer of the adjacent property may be required to make additional internal site improvements and/or external roadway improvements as a result of this additional review.**
- A qualified NCDOT inspector shall be on the site at all times during construction. The permittee (not the contractor) shall make arrangements to have a qualified inspector, under the supervision of a Professional Engineer registered in North Carolina, on site at all times during construction. This work includes, but is not limited to, culvert and drainage structure installation, roadway widening, grading work, pavement structure installation (subgrade, base, and pavement), and traffic marking and marker installation. Please note that the Permittee is also responsible for contacting Mr. Troy L. Baker, Senior Assistant District Engineer at (910) 364-0601 at least 24 hours prior to beginning construction and prior to "critical events," such as backfilling pipe, installing curb and gutter, and placing asphalt.
- **Approval of the Driveway Permit does not constitute review or approval of subdivision streets for NCDOT maintenance.** If addition of subdivision streets to the State Maintenance System is desired, plans for review and a Petition for Addition shall be submitted to the District Engineer’s office. For further information, contact the District office at (910) 364-0601.
- All traffic control, asphalt mixes, structures, construction, workmanship and construction methods, and materials shall be in compliance with the most-recent versions of the following resources: *ASTM Standards, Manual on Uniform Traffic Control Devices, NCDOT Utilities Accommodations Manual, NCDOT Standard Specifications for Roads and Structures, NCDOT Roadway Standard Drawings, NCDOT Asphalt Quality Management System manual, and the approved plans.*
- NCDOT does not guarantee the right of way on this road, nor will it be responsible for any claim for damages brought by any property owner by reason of the installation. It is the responsibility of the permittee to verify the right of way. All right of way and easements necessary for construction and maintenance shall be dedicated to NCDOT and proof of dedication shall be furnished to the District Engineer prior to beginning work.
- No commercial advertising shall be allowed within NCDOT Right of Way.
- Two-way traffic shall be maintained at all times.
- The traveling public shall be warned of construction with complete and proper signing and traffic control devices in accordance with the current *Manual on Uniform Traffic Devices* (MUTCD): No work shall be performed in the right of way unless this requirement is satisfied. NCDOT reserves the right to require a written traffic control plan for encroachment operations. All contractor personnel will be required to wear a class II ANSI approved safety vest while working within DOT rights of way.
- Ingress and egress shall be maintained to all businesses and dwellings affected by the project. Special attention shall be paid to police, EMS and fire stations, fire hydrants, secondary schools, and hospitals.

- Traffic shall be maintained at all times. All lanes of traffic are to be open during the hours of 7:00 A.M. to 9:00 A.M. and from 4:00 P.M. to 6:00 P.M. Monday through Friday, during any time of inclement weather, **or as directed by the District Engineer**. No lane of traffic shall be closed on holidays, special events, or as directed by the engineer. Any violation of these hours will result in ceasing any further construction by the Encroaching Party or their contractor.
- Work requiring lane or shoulder closures shall not be performed on both sides of the road simultaneously within the same area.
- Any work requiring equipment or personnel within 5 feet of the edge of any travel lane of an undivided facility and within 10 feet of the edge of any travel lane of a divided facility shall require a lane closure with appropriate tapers per current *NCDOT Roadway Standard Drawings* or *MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES*.
- NCDOT reserves the right to further limit, restrict, or suspend operations within the right of way if, in the opinion of NCDOT, safety or traffic conditions warrant such action.
- The Division Traffic Engineer shall be notified at (910) 364-0606 prior to beginning work on the right of way if there are existing NCDOT signs, traffic signals, or signal equipment in or near the proposed work zone. Costs to relocate, replace, or repair NCDOT signs, signals, or associated equipment shall be the responsibility of the Permittee. Furthermore, any revisions to existing traffic signals or the installation of any new traffic signals shall require a Signal Agreement between the Permittee and the NCDOT. Additional information concerning this subject can be obtained from the Division Traffic Engineer.
- Excavation within 1000 feet of a signalized intersection will require notification by the encroaching party to the Division Traffic Engineer at telephone number (910) 364-0606 no less than one week prior to beginning work. All traffic signal or detection cables must be located prior to excavation. Cost to replace or repair NCDOT signs, signals, pavement markings or associated equipment and facilities shall be the responsibility of the encroaching party
- When surface area in excess of one acre will be disturbed, the Permittee shall submit a Sediment and Erosion Control Plan which has been approved by the appropriate regulatory agency or authority prior to beginning any work on the right of way. Failure to provide this information shall be grounds for suspension of operations. Proper temporary and permanent measures shall be used to control erosion and sedimentation in accordance with the approved sediment and erosion control plan.
- The Verification of Compliance with Environmental Regulations (VCER-1) form is required for all driveway permits when land disturbance within NCDOT right of way exceeds 1 acre. The VCER-1 form must be PE sealed by a NC registered professional engineer who has verified that all appropriate environmental permits (if applicable) have been obtained and all applicable environmental regulations have been followed.
- All erosion control devices and measures shall be constructed, installed, maintained, and removed by the Encroacher in accordance with all applicable Federal, State, and Local laws, regulations, ordinances, and policies. Permanent vegetation shall be established on all disturbed areas in accordance with the recommendations of the Division Roadside Environmental Engineer. All areas disturbed (shoulders, ditches, removed accesses, etc.) shall be graded and seeded in accordance with the latest *NCDOT Standards Specifications for Roads and Structures* and within 15 calendar days with an approved NCDOT seed mixture (all lawn type areas shall be maintained and reseeded as such). Seeding rates per acre shall be applied according to the Division Roadside Environmental Engineer. Any plant or vegetation in the NCDOT planted sites that is destroyed or damaged as a result of this encroachment shall be replaced with plants of like kind or similar shape.

- No trees within NCDOT shall be cut without authorization from the Division Roadside Environmental Engineer. An inventory of trees measuring greater than 4 caliper inches (measured 6" above the ground) is required when trees within C/A right of way will be impacted by the encroachment installation. Mitigation is required and will be determined by the Division Roadside Environmental Engineer's Office.
- It shall be the responsibility of the Permittee to determine the location of other utilities within the work zone. The Permittee shall be responsible for notifying other utility owners and providing protection and safeguards to prevent damage or interruption to existing facilities and to maintain accessibility to existing utilities.
- The contractor shall not begin the construction until after the traffic control and erosion control devices have been installed to the satisfaction of the Division Engineer or their agent.
- Curb cuts and ramps for handicapped persons shall be constructed in accordance with the current NCDOT "Standard for Wheelchair Ramp Curb Cuts" and the Americans with Disabilities Act (ADA) Accessibility Guidelines for Buildings and Facilities.
- Access granted under this permit shall remain closed (i.e. barricaded) to all traffic until all requirements relating to traffic control and signalization have been satisfied.
- Any proposed landscaping or landscape plantings shall be approved by the Division Engineer under an NCDOT Planting Permit. For further information, please contact the Division Roadside Environmental Engineer, at (910) 364-0603.
- The permittee and/or their contractor shall comply with all OSHA requirements. If OSHA visits the work area associated with this encroachment, the District Office shall be notified by the permittee immediately if any violations are cited.
- All disturbed areas are to be fully restored to current NCDOT minimum roadway standards or as directed by the Division Engineer or their representative. Disturbed areas within NCDOT Right-of-Way include, but not limited to, any excavation areas, pavement removal, drainage or other features.
- Pre-cast concrete manholes, catch basins, or other drainage structures shall be pre-approved by NCDOT for use within highway rights of way.
- Manhole rings and covers, valve covers, and storm drainage grates and frames shall be traffic bearing types approved by NCDOT for use within highway Rights of Way.
- The permittee shall notify the District Engineer or their representative immediately in the event any drainage structure is blocked, disturbed or damaged. All drainage structures disturbed, damaged or blocked shall be restored to its original condition as directed by the District Engineer or their representative.
- Unless specified otherwise, during non-working hours, equipment shall be located away from the job site or parked as close to the right of way line as possible and be properly barricaded in order not to have any equipment obstruction within the Clear Recovery Area. Also, during non-working hours, no parking or material storage shall be allowed along the shoulders of any state-maintained roadway.
- No access to the job site, parking or material storage shall be allowed along or from the **Control of Access Roadway**.
- Guardrail removed or damaged during construction shall be replaced or repaired to its original condition, meeting current NCDOT standards or as directed by the Division Engineer or their representative.
- The resetting of the Control of Access fence shall be in accordance with the applicable NCDOT standard and as directed by the Division Engineer or their representative.

- Right of Way monuments disturbed during construction shall be referenced by a registered Land Surveyor and reset after construction.
- All driveways disturbed during construction shall be returned to a state comparable with the condition of the driveways prior to construction.
- If the approved method of construction is unsuccessful and other means are required, prior approval must be obtained through the District Engineer before construction may continue.
- Pipe, if required, shall be manufactured from reinforced concrete or steel, shall be a minimum diameter of 15", a minimum of 20' in length, provide the minimum amount of required cover and shall be inspected for proper installation prior to backfilling. Contact the Harnett County Maintenance Engineer, at (910) 985-8120 for inspection. Please provide at least 24 hours advance notice for inspection.
- Any drainage pipe and drainage structures shall be constructed in accordance with Division 3 of the latest NCDOT *Standard Specifications for Roads and Structures* and Amendments or Supplementals thereto and Standard Drawing numbers 300.01 of the latest NCDOT *Roadway Standards Drawings* or as directed by the engineer. Any drainage structure must be inspected by and shall bear the stamp of approval by the NCDOT Materials and Tests Unit. All concrete testing results shall be provided to the District Engineer's office at time of project completion.
- Any Flared End Section shall be constructed in accordance with Division 3 of the latest NCDOT *Standard Specifications for Roads and Structures* and Amendments or Supplementals thereto and shall bear the stamp of approval by the NCDOT Materials and Tests Unit and Standard Drawing numbers 310.02 and/or 310.03 of the NCDOT *Roadway Standards Drawings* or as directed by the engineer. All concrete testing results shall be provided to the District Engineer's office at time of project completion.
- Trenching, bore pits and/or other excavations shall not be left overnight. The contractor shall comply with all OSHA requirements and provide a competent person on site to supervise excavation at all times.
- Excavated areas adjacent to pavement having more than a 2" drop shall be safed up at a 6:1 or flatter slope and designated by appropriate delineation during periods of inactivity, including, but not limited to, night and weekend hours. Excavated material shall not be placed on the roadway at any time.
- It is the responsibility of the permittee or their contractor to prevent any mud/dirt from tracking onto the roadway. Any dirt which may collect on the roadway pavement from equipment and/or truck traffic on site shall be immediately removed to avoid any unsafe traffic conditions.
- All material to a depth of 8 inches below the finished surface of the subgrade shall be compacted to a density equal to at least 100% of that obtained by compacting a sample of the material in accordance with AASHTO T99 as modified by the Department. The subgrade shall be compacted at a moisture content which is approximately that required to produce the maximum density indicated by the above test method. The contractor shall dry or add moisture to the subgrade when required to provide a uniformly compacted and acceptable subgrade. The option to backfill any trenches with dirt or either #57 stone or #78 stone with consolidation with a plate tamp and without a conventional density test may be pursued with the written consent of the District Engineer. If this option is exercised, then roadway ABC stone and asphalt repair as required will also be specified by the District Engineer.
- The proposed driveway shall be paved fifty (50) feet down the centerline of the driveway alignment as measured from the edge of existing pavement as shown on the attached plans.

- **The commercial driveways onto US Highway 421 (East Jackson Boulevard) shall not be modified as shown on the attached plans. No additional access to US Highway 421 (East Jackson Boulevard) from the restaurant site shall be permitted. Any proposed modification of the existing driveways shall be reviewed by the District Engineer. Unauthorized access to US Highway 421 (East Jackson Boulevard) may result in the removal of the driveway or street connections at the property owner's expense.**
- **The shared driveway to the existing restaurant site shall be located a minimum of 160' as measured from the travel lane of US Highway 421 (East Jackson Boulevard) along the restaurant access roadway as shown on the attached plans.**
- **The proposed entrances onto the restaurant site access roadways shall be constructed in accordance with the attached plans.**
- The entrance grade shall be constructed in accordance with Figure 6 (attached).
- Any pavement damaged because of settlement of the pavement or damaged by equipment used to perform Driveway Permit work, shall be re-surfaced to the satisfaction of the District Engineer. This may include the removal of pavement and a mechanical overlay. All pavement work and pavement markings (temporary and final) are the responsibility of the permittee.
- All 30" curb and gutter within NCDOT rights of way shall be constructed with Class B concrete in accordance with Section 846 of the latest NCDOT Standard Specifications for Roads and Structures and Roadway Standard Drawings (Std. Dwg. No. 846.01) and Amendment or Supplemental thereto or as directed by the engineer. All concrete testing results shall be provided to the District Engineer's office at time of project completion.
- **Sidewalks and/or bus stops are not approved as a part of this Driveway Permit. A separate three party (RW 16.6) encroachment agreement review and approval shall be required before any sidewalk and/or bus stops are constructed within the right of way of US Highway 421 (East Jackson Boulevard).**
- The permittee shall notify the District Engineer's office within 2 business days after construction is complete. The District Engineer may perform a construction inspection. Any deficiencies may be noted and reported to the permittee to make immediate repairs or resolve any issues to restore the right-of-way to a similar condition prior to construction, including pavement, signage, traffic signals, pavement markings, drainage, structures/pipes, or other highway design features.
- At the discretion of the District Engineer, a final inspection report may be provided to the encroaching party upon satisfactory completion of the work.

Pavement Markings, Markers and Signing

- The Permittee shall be responsible for the removal, relocation, and / or installation of all pavement markings and NCDOT signs within the limits affected by the development. Pavement marking and signing plans shall be submitted to the District Engineer for approval prior to the installation of any signs and/or pavement markings.
- The Permittee has been approved to use NCDOT approved ~~waterborne paint for all pavement markings (edge lines, lane lines and centerlines) on US Highway 421 (East Jackson Boulevard)~~ and thermoplastic pavement markings for all pavement markings (edge lines, lane lines and centerlines) and non-snowplowable markers on US Highway 421 (East Jackson Boulevard). (The use of waterborne paint or thermoplastic pavement markings is to be determined according to the location of the site and AADT of the roadway).

- Pavement marking and marker changes shall be accomplished by completely covering the lines by means of asphalt overlaying or by an approved method of grinding. **In no case shall the application of liquid asphalt "tack" be accepted as an approved method for covering pavement markings.** Any pavement markings that are damaged or obliterated shall be restored by the encroaching party at no expense to NCDOT.
- All work performed under this section shall meet standards outlined in the current *Manual on Uniform Traffic Control Devices* (MUTCD) published by the Federal Highway Administration, NCDOT Supplement to the MUTCD, and the latest NCDOT *Roadway Standard Drawings*.
- The pavement marking contractor is required to have at least one member of every pavement marking crew that is working on the project, preferably the Crew Supervisor, be certified through the NCDOT Pavement Marking Technician Certification Process. For more information please contact the **Work Zone Traffic Control Unit** at (919) 773-2800 or <http://www.ncdot.org/doh/preconstruct/wztc/>.
- Prior to installing pavement markings, please contact the Division Traffic Engineer at (910) 364-0606 with the NCDOT Division Six Traffic Services Unit to review the proposed pavement-marking layout. This notification should take place a minimum of 48 hours in advance of the pavement marking installation.
- Failure to contact the Traffic Services Unit to review the pavement-marking layout prior to installation may result in the removal and reinstallation of the markings at the expense of the Permittee.

If further information or assistance is needed in reference to this project, please feel free to call Mr. Lee R. Hines, Jr. (Richie), PE, District Engineer at (910) 364-0601.

Sincerely,
Signed by:

Lee R. Hines, Jr., (Richie)
Lee R. Hines, Jr., (Richie), PE
District Engineer

Initial
TLB

LRH:tlb:eab

cc: <https://connect.ncdot.gov/site/files/drivewaypermit/Cumberland/Forms/AllItems.aspx>

| APPLICATION IDENTIFICATION | | N.C. DEPARTMENT OF TRANSPORTATION STREET AND DRIVEWAY ACCESS PERMIT APPLICATION |
|--|---------------------|---|
| Driveway Permit No. | Date of Application | |
| County: <u>Harnett</u> | | |
| Development Name: <u>503 East Jackson Blvd - Scooters Coffee</u> | | |

| LOCATION OF PROPERTY | |
|--|---|
| Route/Road: <u>East Jackson Blvd (US 421) Erwin</u> | |
| Exact Distance <u>375</u> | <input type="checkbox"/> Miles <input type="checkbox"/> N <input type="checkbox"/> S <input type="checkbox"/> E <input type="checkbox"/> W <input checked="" type="checkbox"/> Feet <input type="checkbox"/> <input type="checkbox"/> <input checked="" type="checkbox"/> <input type="checkbox"/> |
| From the Intersection of Route No. <u>St Matthew Rd</u> and Route No. <u>East Jackson Blvd</u> Toward <u>Dunn</u> | |
| Property Will Be Used For: <input type="checkbox"/> Residential /Subdivision <input checked="" type="checkbox"/> Commercial <input type="checkbox"/> Educational Facilities <input type="checkbox"/> TND <input type="checkbox"/> Emergency Services <input type="checkbox"/> Other Property: <input checked="" type="checkbox"/> is <input type="checkbox"/> is not within <u>Erwin</u> City Zoning Area. | |

AGREEMENT

- I, the undersigned property owner, request access and permission to construct driveway(s) or street(s) on public right-of-way at the above location.
- I agree to construct and maintain driveway(s) or street entrance(s) in absolute conformance with the current "Policy on Street and Driveway Access to North Carolina Highways" as adopted by the North Carolina Department of Transportation.
- I agree that no signs or objects will be placed on or over the public right-of-way other than those approved by NCDOT.
- I agree that the driveway(s) or street(s) will be constructed as shown on the attached plans.
- I agree that that driveway(s) or street(s) as used in this agreement include any approach tapers, storage lanes or speed change lanes as deemed necessary.
- I agree that if any future improvements to the roadway become necessary, the portion of driveway(s) or street(s) located on public right-of-way will be considered the property of the North Carolina Department of Transportation, and I will not be entitled to reimbursement or have any claim for present expenditures for driveway or street construction.
- I agree that this permit becomes void if construction of driveway(s) or street(s) is not completed within the time specified by the "Policy on Street and Driveway Access to North Carolina Highways".
- I agree to pay a \$50 construction inspection fee. Make checks payable to NCDOT. This fee will be reimbursed if application is denied.
- I agree to construct and maintain the driveway(s) or street(s) in a safe manner so as not to interfere with or endanger the public travel.
- I agree to provide during and following construction proper signs, signal lights, flaggers and other warning devices for the protection of traffic in conformance with the current "Manual on Uniform Traffic Control Devices for Streets and Highways" and Amendments or Supplements thereto. Information as to the above rules and regulations may be obtained from the District Engineer.
- I agree to indemnify and save harmless the North Carolina Department of Transportation from all damages and claims for damage that may arise by reason of this construction.
- I agree that the North Carolina Department of Transportation will assume no responsibility for any damages that may be caused to such facilities, within the highway right-of-way limits, in carrying out its construction.
- I agree to provide a Performance and Indemnity Bond in the amount specified by the Division of Highways for any construction proposed on the State Highway system.
- The granting of this permit is subject to the regulatory powers of the NC Department of Transportation as provided by law and as set forth in the N.C. Policy on Driveways and shall not be construed as a contract access point.
- I agree that the entire cost of constructing and maintaining an approved private street or driveway access connection and conditions of this permit will be borne by the property owner, the applicant, and their grantees, successors, and assignees.
- **I AGREE TO NOTIFY THE DISTRICT ENGINEER WHEN THE PROPOSED WORK BEGINS AND WHEN IT IS COMPLETED.**

TRAFFIC CONTROL AND WORK ZONE SAFETY

The Contractor shall maintain traffic during construction and provide, install, and maintain all traffic control devices in accordance with these project guidelines, the Project Special Provisions, North Carolina Department of Transportation Standard Specifications for Roads and Structures, and the current edition of the Manual of Uniform Traffic Control Devices (MUTCD).

The Contractor shall utilize complete and proper traffic controls and traffic control devices during all operations. All traffic control and traffic control devices required for any operation shall be functional and in place prior to the commencement of that operation. Signs for temporary operations shall be removed during periods of inactivity. The Contractor is required to leave the project in a manner that will be safe to the traveling public and which will not impede motorists.

Traffic movements through lane closures on roads with two way traffic shall be controlled by flaggers stationed at each end of the work zone. In situations where sight distance is limited, the Contractor shall provide additional means of controlling traffic, including, but not limited to, two-way radios, pilot vehicles, or additional flaggers. Flaggers shall be competent personnel, adequately trained in flagging procedures, and furnished with proper safety devices and equipment, including, but not limited to, safety vests and stop/slow paddles.

All personnel when working in traffic areas or areas in close proximity to traffic shall wear an approved safety vest, or shirt or jacket which meets the color requirements of the Manual of Uniform Traffic Control Devices (MUTCD).

The Contractor shall comply with all applicable Federal, State, and local laws, ordinances, and regulations governing safety, health, and sanitation, and shall provide all safeguards, safety devices, and protective equipment, and shall take any other needed actions, on his own responsibility that are reasonably necessary to protect the life and health of employees on the job and the safety of the public, and to protect property in connection with the performance of the work covered by the contract.

Failure to comply with any of the requirements for safety and traffic control of this contract shall result in suspension of work as provided in subarticle 108-7(2) of the Standard Specifications.

SPECIFICATIONS FOR EROSION CONTROL

The Contractor shall seed all disturbed areas as directed by the Engineer, in accordance with Section 1660 of the Standard Specifications. Seeding and mulching shall immediately follow shoulder construction operations and in no case shall shoulder construction operations exceed seeding and mulching operations by more than two weeks without written permission of the Engineer. Failure to meet this requirement shall be cause to cease all operations until it can be met.

Seeding and Mulching: (East)

The kinds of seed and fertilizer, and the rates of application of seed, fertilizer, and limestone, shall be as stated below. During periods of overlapping dates, the kind of seed to be used shall be determined by the Engineer. All rates are in pounds per acre.

The kinds of seed and fertilizer, and the rates of application of seed, fertilizer, and limestone, shall be as stated below. During periods of overlapping dates, the kind of seed to be used shall be determined. All rates are in pounds per acre.

All Roadway Areas

March 1 - August 31

| | |
|-------|-----------------------|
| 50# | Tall Fescue |
| 10# | Centipede |
| 25# | Bermudagrass (hulled) |
| 500# | Fertilizer |
| 4000# | Limestone |

September 1 - February 28

| | |
|-------|-------------------------|
| 50# | Tall Fescue |
| 10# | Centipede |
| 35# | Bermudagrass (unhulled) |
| 500# | Fertilizer |
| 4000# | Limestone |

Waste and Borrow Locations

March 1 - August 31

| | |
|-------|-----------------------|
| 75# | Tall Fescue |
| 25# | Bermudagrass (hulled) |
| 500# | Fertilizer |
| 4000# | Limestone |

September 1 - February 28

| | |
|-------|-------------------------|
| 75# | Tall Fescue |
| 35# | Bermudagrass (unhulled) |
| 500# | Fertilizer |
| 4000# | Limestone |

Note: 50# of Bahiagrass may be substituted for either Centipede or Bermudagrass only upon Engineer's request.

Approved Tall Fescue Cultivars

| | | | |
|----------------------------|------------------------|--------------|--------------------|
| 2 nd Millennium | Duster | Magellan | Rendition |
| Avenger | Endeavor | Masterpiece | Scorpion |
| Barlexas | Escalade | Matador | Shelby |
| Barlexas II | Falcon II, III, IV & V | Matador GT | Signia |
| Barrera | Fidelity | Millennium | Silverstar |
| Barrington | Finesse II | Montauk | Southern Choice II |
| Biltmore | Firebird | Mustang 3 | Stetson |
| Bingo | Focus | Olympic Gold | Tarheel |
| Bravo | Grande II | Padre | Titan Ltd |
| Cayenne | Greenkeeper | Paraiso | Titanium |
| Chapel Hill | Greystone | Picasso | Tomahawk |
| Chesapeake | Inferno | Piedmont | Tacer |
| Constitution | Justice | Pure Gold | Trooper |
| Chipper | Jaguar 3 | Prospect | Turbo |
| Coronado | Kalahari | Quest | Ultimate |
| Coyote | Kentucky 31 | Rebel Exeda | Watchdog |
| Davinci | Kitty Hawk | Rebel Sentry | Wolfpack |
| Dynasty | Kitty Hawk 2000 | Regiment II | |
| Dominion | Lexington | Rembrandt | |

On cut and fill slopes 2:1 or steeper Centipede shall be applied at the rate of 5 pounds per acre and add 20# of Sericea Lespedeza from January 1 - December 31.

Fertilizer shall be 10-20-20 analysis. A different analysis of fertilizer may be used provided the 1-2-2 ratio is maintained and the rate of application adjusted to provide the same amount of plant food as a 10-20-20 analysis and as directed.

All areas seeded and mulched shall be tacked with asphalt. Crimping of straw in lieu of asphalt tack shall not be allowed on this project.

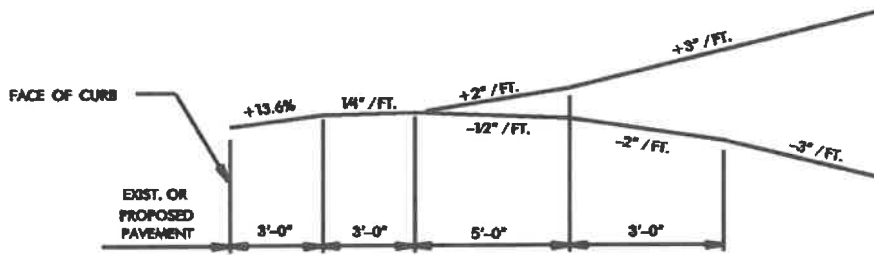
CRIMPING STRAW MULCH

Crimping shall be required on this project adjacent to any section of roadway where traffic is to be maintained or allowed during construction. In areas within six feet of the edge of pavement, straw is to be applied and then crimped. After the crimping operation is complete, an additional application of straw shall be applied and immediately tacked with a sufficient amount of undiluted emulsified asphalt.

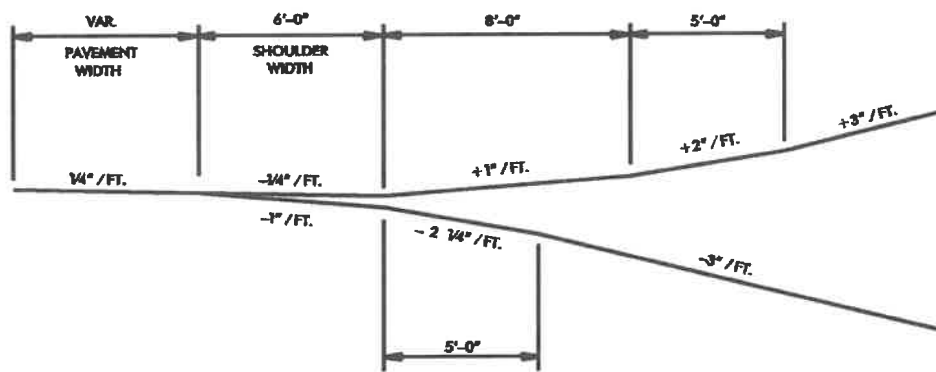
Straw mulch shall be of sufficient length and quality to withstand the crimping operation.

Crimping equipment including power source shall be subject to the approval of the Engineer providing that maximum spacing of crimper blades shall not exceed 8".

Within seven (7) calendar days to fourteen (14) calendar days of completion of any phase of grading, all disturbed areas shall be planted or otherwise provided with temporary or permanent ground cover, devices, or structures sufficient to restrain erosion. The Erosion and Sediment Control plan will identify the areas that require seven (7) and/or fourteen (14) calendar day ground stabilization. The Contractor is herein advised to follow all current regulations set forth by the North Carolina Department of Environment and Natural Resources (NCDENR) Division of Water Quality (DWQ) as defined in the *General Stormwater Permit for Construction Activities NCG-010000*.



A. CURB & GUTTER SECTION



B. SHOULDER SECTION

FIGURE 6

DRIVEWAY TURNOUT GRADES

**North Carolina Department of Transportation
Subdivision Road Construction, Driveway Access, and
Encroachment Installation Certification Memo**

(Date)

(District Engineer Name & Address)

RE: CONSTRUCTION CERTIFICATION
County: _____
Driveway Permit #: _____
and/or Subdivision I.D. #: _____
and/or Encroachment #: _____
Recording Information (if applicable): _____
Route(s) and/or Street(s): _____

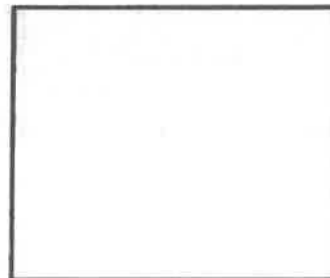
Project Name/Phase/Description: _____

I, _____, have reviewed the construction within the right-of-way and/or easements required under subject project(s) for the above development and in accordance with the design drawings approved by the North Carolina Department of Transportation (NCDOT) on the following dates:

| | | | |
|----------------------------|-------|-------|-------|
| Driveway Permit #: | _____ | Date: | _____ |
| and/or Subdivision I.D. #: | _____ | Date: | _____ |
| and/or Encroachment #: | _____ | Date: | _____ |

My inspection and attached testing report(s) and/or supporting documentation indicate the construction within the right-of-way and/or easements have been constructed in accordance with the standards established by current NCDOT Standard Specifications for Roads and Structures, and with the approved plans.

Name: _____
NC PE #: _____
Signature: _____



SEAL

Received by NCDOT: _____



STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION

JOSH STEIN
GOVERNOR

DANIEL H. JOHNSON
SECRETARY

March 25, 2026

Mr. Tommy Burns
Harnett Regional Water
700 McKinney Parkway
Lillington, NC 27546

SUBJECT: Encroachment Agreement along US Highway 421 (East Jackson Boulevard) in the town of Erwin in Harnett County (E062-043-25-01172).

Dear Sir:

Attached is an approved R/W form 16.1 and plans for the installation of 530'± of 8"Φ C-900 DR-18 water main with one (1) fire hydrant assembly, one (1) 8"Φ gate valve, and one (1) 2"Φ blow-off assembly by open shoulder cut method for tie-in and related appurtenances along US Highway 421 (East Jackson Boulevard) in the town of Erwin in Harnett County as shown on attached plans. (Erwin US Highway 421 Water Line Extension 2026).

Location:

| Route | At a point | Towards |
|--------------|---|----------------|
| US 421 | 3,136'± southeast of the intersection of US Highway 421 (East Jackson Boulevard) and SR 1703 (Masonic Road) | SR 1719 |

This encroachment is approved subject to the following:

Pre-Construction

Contact Offices & Outside Agency issues/contacts/info

1. Approval may be rescinded upon failure to follow any of the provisions in this permit and may be considered a violation of the encroachment agreement.
2. **The Encroaching party or their contractor shall provide the following notices prior to construction activity within the NCDOT Right of Way:**
 - a. **Three (3) business days advance phone call Mr. Troy L. Baker, Senior Assistant District Engineer at telephone (910) 364-0601 or email to tlbaker2@ncdot.gov to the District Engineer's office.**

Failure to provide these notifications prior to beginning construction is subject to the Division Engineer's discretion to cease construction activity for this encroachment. NCDOT reserves the right to cease any construction or maintenance work associated with this installation by the encroaching party until the construction or maintenance meets the satisfaction of the Division Engineer or their representative.

Mailing Address:
NC DEPARTMENT OF TRANSPORTATION
DIVISION SIX / DISTRICT TWO
POST OFFICE BOX 1150
FAYETTEVILLE, NC 28302

Telephone: (910) 364-0601
Fax: (910) 437-2529
Customer Service: 1-877-368-4968

Website: www.ncdot.gov

Location:
600 SOUTHERN AVENUE
FAYETTEVILLE, NC 28306

3. Prior to beginning work, it is the requirement of the Encroaching Party to contact the appropriate Utility Companies involved and make arrangements to adjust or relocate any utilities that conflict with the proposed work.
4. It shall be the responsibility of the encroaching party to determine the location of utilities within the encroachment area. NCGS § 87-115 through § 87-130 of the Underground Utility Safety and Damage Prevention Act requires underground utilities to be located by calling 811 prior to construction. The encroaching party shall be responsible for notifying other utility owners and providing protection and safeguards to prevent damage or interruption to existing facilities and maintain access to them.
5. The encroaching party shall notify the appropriate municipal office prior to beginning any work within the municipality's limits of jurisdiction.
6. **Excavation within 1000 feet of a signalized intersection will require notification by the encroaching party to the Division Traffic Engineer at telephone number (910) 364-0606 no less than one week prior to beginning work. All traffic signal or detection cables must be located prior to excavation. Cost to replace or repair NCDOT signs, signals, pavement markings or associated equipment and facilities shall be the responsibility of the encroaching party.**
7. At the option of the District Engineer, a preconstruction meeting including representatives of NCDOT, the encroaching party, contractors and municipality, if applicable, shall be required. A pre-construction conference held between a municipality (or other facility owner) and a contractor without the presence of NCDOT personnel with subsequent construction commencing may be subject to NCDOT personnel ceasing any work on NCDOT right-of-way related to this encroachment until such meeting is held. Contact the District office to schedule.
8. **A NOTIFICATION FOR UTILITY / NON-UTILITY ENCROACHMENT WITHIN NCDOT R/W form (See corresponding attachment) with the scheduled pre-construction meeting and associated construction schedule details must be completed and submitted to the District Engineer's office a minimum of one week prior to construction.**
9. **A qualified NCDOT inspector shall be on the site at all times during construction. The encroaching party (not the utility contractor) shall make arrangements to have a qualified inspector, under the supervision of a Professional Engineer registered in North Carolina, on site at all times during construction. The registered Professional Engineer shall be required to submit a signed and PE sealed certification that the utility was installed in accordance with the encroachment agreement.**
10. **An NCDOT inspector shall be on site during any boring operations beneath all state maintained roadways. Utility installation beneath state maintained roadways performed without the presence of the required NCDOT inspector will be subject to removal and the direction to cease any construction or maintenance work associated with any associated encroachment agreement(s).**

Legal & Right-of-Way Issues

11. This approval and associated plans and supporting documents shall not be interpreted to allow any design change or change in the intent of the design by the Owner, Design Engineer, or any of their representatives. Any revisions or changes to these approved plans or intent for construction must be obtained in writing from the Division Engineer's office or their representative prior to construction or during construction if an issue arises during construction to warrant changes.
12. NCDOT does not guarantee the right of way on this road, nor will it be responsible for any claim for damages brought about by any property owner by reason of this installation. It is the responsibility of the encroaching party to verify the right of way.
13. Encroaching party shall be responsible for obtaining all necessary permanent and/or temporary construction, drainage, utility and/or sight distance easements.
14. All Right of Way and easements necessary for construction and maintenance shall be dedicated to NCDOT with proof of dedication furnished to the District Engineer prior to beginning work.
15. No commercial advertising shall be allowed within NCDOT Right of Way.
16. The encroaching party shall obtain proper approval from all affected pole owners prior to attachment to any pole.
17. This agreement does not authorize installations within nor encroachment onto railroad rights of way. Permits for installations within railroad right of way must be obtained from the railroad and are the responsibility of the encroaching party.

Work Zone Traffic

18. Traffic control shall be coordinated with the District Engineer and the Division Traffic Engineer at telephone (910) 364-0606, prior to construction.
19. WORK ZONE TRAFFIC CONTROL QUALIFICATIONS AND TRAINING PROGRAM

All personnel performing any activity inside the highway right of way are required to be familiar with the NCDOT Maintenance / Utility Traffic Control Guidelines (MUTCG). No specific training course or test is required for qualification in the Maintenance /Utility Traffic Control Guidelines (MUTCG).

All flagging, spotting, or operating Automated Flagger Assist Devices (AFAD) inside the highway right of way requires qualified and trained Work Zone Flaggers. Training for this certification is provided by NCDOT approved training resources and by private entities that have been pre-approved to train themselves.

All personnel involved with the installation of Work Zone Traffic Control devices inside the highway right of way are required to be qualified and trained Work Zone Installers. Training for this certification is provided by NCDOT approved training resources and by private entities that have been pre-approved to train themselves.

All personnel in charge of overseeing work zone Temporary Traffic Control operations and installations inside the highway right of way are required to be qualified and trained Work Zone Supervisors. Training for this certification is provided by NCDOT approved training resources and by private entities that have been pre-approved to train themselves.

For questions and/or additional information regarding this training program please refer to <https://connect.ncdot.gov/projects/WZTC/Pages/Training.aspx> or call the NCDOT Work Zone Traffic Control Section (919) 814-5000.

20. The party of the second part shall employ traffic control measures that are in accordance with the prevailing federal, state, local, and NCDOT policies, standards, and procedures. These policies, standards, and procedures include, but are not limited to the following:
 - a. Manual on Uniform Traffic Control Devices (MUTCD) – North Carolina has adopted the MUTCD to provide basic principles and guidelines for traffic control device design, application, installation, and maintenance. North Carolina uses the MUTCD as a minimum requirement where higher supplemental standards specific to North Carolina are not established. Use fundamental principles and best practices of MUTCD (Part 6, Temporary Traffic Control).
 - b. NCDOT Maintenance / Utility Traffic Control Guidelines – This document enhances the fundamental principles and best practices established in MUTCD Part 6, Temporary Traffic Control, incorporating NCDOT-specific standards and details. It also covers important safety knowledge for a wide range of work zone job responsibilities.
21. If the Traffic Control Supervisor determines that portable concrete barrier (PCB) is required to shield a hazard within the clear zone, then PCB shall be designed and sealed by a licensed North Carolina Professional Engineer. PCB plans and design calculations shall be submitted to the District Engineer for review and approval prior to installation.
22. Ingress and egress shall be maintained to all businesses and dwellings affected by the project. Special attention shall be paid to police, EMS and fire stations, fire hydrants, secondary schools, and hospitals.
23. Traffic shall be maintained at all times. All lanes of traffic are to be open during the hours of 7:00 A.M. to 9:00 A.M. and from 4:00 P.M. to 6:00 P.M. Monday through Friday, during any time of inclement weather, **or as directed by the District Engineer**. If the location of work calls for a rolling roadblock on Interstates and divided highway US routes, the restriction is to work only on Sunday from 1:00 A.M. to 10:00 A.M., **or as Directed by the District Engineer**. Any violation of these hours will result in ceasing any further construction by the Encroaching Party or their contractor.
24. Nighttime and weekend operations will NOT be allowed unless written approval is received from the District Engineer. If nighttime or weekend work is allowed or required, all signs must be retro-reflective, and a work zone lighting plan must be submitted for approval prior to construction.
25. Two-way traffic shall be maintained at all times unless designated by the District Engineer. Traffic shall not be rerouted or detoured without the prior written approval from the District Engineer. No utility work will be allowed on state holidays from 7:00 PM the night before through 9:00 AM the day prior to, following or during local events without prior approval from the District Engineer. If the construction is within 1000 feet of a school location or on a designated bus route, the construction shall be coordinated with the school start and end times to avoid traffic delays.
26. Work requiring lane or shoulder closures shall not be performed on both sides of the road simultaneously within the same area.
27. Any work requiring equipment or personnel within 5 feet of the edge of any travel lane of an undivided facility and within 10 feet of the edge of any travel lane of a divided facility shall require a lane closure with appropriate tapers per current *NCDOT Roadway Standard Drawings* or *MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES*.

28. At the discretion of the District Engineer, a traffic control plan shall be developed and submitted under the seal and signature of a Licensed North Carolina Professional Engineer prior to construction. The plan shall be specific to the site and adequately detailed. Issues such as the close proximity to intersections shall be addressed.
29. Temporary and final pavement markings are the responsibility of the encroaching party. Final pavement markings and sign plans shall be submitted with the encroachment request to the Division Traffic Engineer prior to construction. Final pavement markings shall be thermoplastic unless otherwise directed by the Division Traffic Engineer or District Engineer.
30. Any pavement markings that are damaged or obliterated shall be restored by the encroaching party at no expense to NCDOT.
31. Sidewalk closures shall be installed as necessary. Pedestrian traffic shall be detoured around these closures and shall be signed appropriately and in accordance with The American with Disabilities Act Accessibility Guidelines. The encroaching party must adhere to the guidelines for accommodating pedestrians in encroachment work zones as described in the NCDOT Pedestrian Work Zone Accommodations Training found at <https://www.youtube.com/watch?v=A0uYa5IW3dg&feature=youtu.be>

Roadside Environmental

32. The encroaching party shall comply with all applicable Federal, State and local environmental regulations and shall obtain all necessary Federal, State and local environmental permits, including but not limited to, those related to sediment control, stormwater, wetland, streams, endangered species and historical sites. Additional information can be obtained by contacting the NCDOT Roadside Environmental Engineer regarding the North Carolina Natural Heritage Program or the United States Fish and Wildlife Services. Contact the Division Roadside Environmental Engineer's Office at (910) 364-0603.
33. When surface area in excess of one acre will be disturbed, the Encroacher shall submit a Sediment and Erosion Control Plan which has been approved by the appropriate regulatory agency or authority prior to beginning any work on the Right of Way. Failure to provide this information shall be grounds for suspension of operations. Proper temporary and permanent measures shall be used to control erosion and sedimentation in accordance with the approved sediment and erosion control plan.
34. The Verification of Compliance with Environmental Regulations (VCER-1) form is required for all non-utility encroachment agreements or any utility encroachments when land disturbance within NCDOT right of way exceeds 1 acre. When required, the VCER-1 form must be PE sealed by a NC registered professional engineer who has verified that all appropriate environmental permits (if applicable) have been obtained and all applicable environmental regulations have been followed.

35. All erosion control devices and measures shall be constructed, installed, maintained, and removed by the Encroacher in accordance with all applicable Federal, State, and Local laws, regulations, ordinances, and policies. Permanent vegetation shall be established on all disturbed areas in accordance with the recommendations of the Division Roadside Environmental Engineer. All areas disturbed (shoulders, ditches, removed accesses, etc.) shall be graded and seeded in accordance with the latest *NCDOT Standards Specifications for Roads and Structures* and within 15 calendar days with an approved NCDOT seed mixture (all lawn type areas shall be maintained and reseeded as such). Seeding rates per acre shall be applied according to the Division Roadside Environmental Engineer. Any plant or vegetation in the NCDOT planted sites that is destroyed or damaged as a result of this encroachment shall be replaced with plants of like kind or similar shape.
36. No trees within NCDOT right of way shall be cut without authorization from the Division Roadside Environmental Engineer. An inventory of trees measuring greater than 4 caliper inches (measured 6" above the ground) is required when trees within C/A right of way will be impacted by the encroachment installation. Mitigation is required and will be determined by the Division Roadside Environmental Engineer's Office.
37. Prior to installation, the Encroaching Party shall contact the District Engineer to discuss any environmental issues associated with the installation to address concerns related to the root system of trees impacted by boring or non-utility construction of sidewalk, roadway widening, etc.
38. The applicant is responsible for identifying project impacts to waters of the United States (wetlands, intermittent streams, perennial streams and ponds) located within the NCDOT right-of-way. The discharge of dredged or fill material into waters of the United States requires authorization from the United States Army Corps of Engineers (USACE) and certification from the North Carolina Division of Water Quality (NCDWQ). The applicant is required to obtain pertinent permits or certification from these regulatory agencies if construction of the project impacts waters of the United States within the NCDOT right-of-way. The applicant is responsible for complying with any river or stream Riparian Buffer Rule as regulated by the NCDWQ. The Rule regulates activity within a 50-foot buffer along perennial streams, intermittent streams and ponds. Additional information can be obtained by contacting the NCDWQ or the USACE.
39. The contractor shall not begin the construction until after the traffic control and erosion control devices have been installed to the satisfaction of the Division Engineer or their agent.
40. The contractor shall perform all monitoring and record keeping and any required maintenance of erosion and sediment control measures to maintain compliance with stormwater regulations.

Bonds

41. ~~A Performance and Indemnity Bond in the amount of ***** was posted with the District Engineer's Office on 02/21/2024 by the Party of the Second Part prior to beginning any work within the NCDOT Right of Way. The bond shall be held for a minimum of one year after a satisfactory final inspection of the installation by NCDOT. The bond may be held for a period longer than one year after completion if, in the opinion of NCDOT, the size or complexity of the installation warrants a longer period.~~
42. The release of the bond is subject to a final inspection by NCDOT. Contact the District office to schedule a Final Inspection and to request release of the bond.
43. When a Continuing Indemnity bond is on file with the central Raleigh office, the cashing of that bond may be used to fund any necessary repairs by NCDOT forces for unaddressed defects in workmanship by the encroaching party and/or by their contractor.

Control of Access

44. No access to the job site, parking or material storage shall be allowed along or from the **Control of Access Roadway**.
45. The installation within the Control of Access fence shall not adversely affect the design, construction, maintenance, stability, traffic safety or operation of the controlled access highway, and the utility must be serviced without access from the through-traffic roadways or ramps.
46. The resetting of the Control of Access fence shall be in accordance with the applicable NCDOT standard and as directed by the Division Engineer or their representative.

STIP (or Division Managed) Projects

47. ~~State Transportation Improvement Project (STIP) X-XXXX is scheduled for future construction. Any encroachment determined to be in conflict with the construction of this NCDOT project shall be removed and/or relocated at the encroaching party's expense.~~

Construction

General

48. An executed copy of the encroachment agreement, provisions and approved plans shall be present at the construction site at all times. If safety or traffic conditions warrant such an action, NCDOT reserves the right to further limit, restrict or suspend operations within the right of way.
49. If the approved method of construction is unsuccessful and other means are required, prior approval must be obtained through the District Engineer before construction may continue.
50. Any REVISIONS marked in RED on the attached non-PE sealed plans shall be incorporated into and made part of the approved encroachment agreement.
51. All disturbed areas are to be fully restored to current NCDOT minimum roadway standards or as directed by the Division Engineer or their representative. Disturbed areas within NCDOT Right-of-Way include, but not limited to, any excavation areas, pavement removal, drainage or other features.

52. The encroaching party shall notify the Division Engineer or their representative immediately in the event any drainage structure is blocked, disturbed or damaged. All drainage structures disturbed, damaged or blocked shall be restored to its original condition as directed by the Division Engineer or their representative.
53. At points where the utility is placed under existing storm drainage, the trench will be backfilled with excavatable flowable fill up to the outside diameter of the existing pipe.
54. Unless specified otherwise, during non-working hours, equipment shall be located away from the job site or parked as close to the right of way line as possible and be properly barricaded in order not to have any equipment obstruction within the Clear Zone. Also, during non-working hours, no parking or material storage shall be allowed along the shoulders of any state-maintained roadway.
55. The Encroaching Party and/or their Contractor shall comply with all OSHA requirements. If OSHA visits the work area associated with this encroachment, the District Office shall be notified by the encroaching party immediately if any violations are cited.
56. Any guardrail removed or damaged during construction shall be replaced or repaired to its original condition, meeting current NCDOT standards or as directed by the Division Engineer or their representative.
57. Right of Way monuments disturbed during construction shall be referenced by a registered Land Surveyor and reset after construction.
58. All Traffic signs moved during construction shall be reinstalled as soon as possible to the satisfaction of the Division Engineer or their representative.
59. Detection tape, where required by NCGS § 87-115 through § 87-130 of the Underground Utility Safety and Damage Prevention Act, shall be buried in the trench approximately 1 foot above the installed facility. Where conduit is installed in the right of way and is not of ferrous material, locating tape or detection wire shall be installed with the conduit.
60. All driveways disturbed during construction shall be returned to a state comparable with the condition of the driveways prior to construction.
61. Conformance with driveway permit review should be required in conjunction with this encroachment agreement. In the event there is a conflict between the driveway permit and the encroachment agreement, the District Engineer should resolve the conflict and notify the parties involved.

Engineering

62. All traffic control, asphalt mixes, structures, construction, workmanship and construction methods, and materials shall be in compliance with the most-recent versions of the following resources: *ASTM Standards*, *Manual on Uniform Traffic Control Devices*, *NCDOT Utilities Accommodations Manual*, *NCDOT Standard Specifications for Roads and Structures*, *NCDOT Roadway Standard Drawings*, *NCDOT Asphalt Quality Management System manual*, **and the approved plans.**
63. Regulator stations, metering stations, cathodic test stations, and anode beds are not permitted within NCDOT right of way. Header wires are permitted.

64. Non-Utility Communication and Data Transmission installations (ground mounted type or Small Cell pole-mounted type) must adhere to guidelines in the Utilities Accommodations Manual and, when located within municipal jurisdictions, are subject to review and approval by municipal ordinances and any additional municipal approval for proximity to historic districts and landmarks. All wiring and related telecommunications work shall conform to the latest regulations by the Federal Communications Commission.
65. All wiring and related electrical work shall conform to the latest edition of the National Electrical Safety Code.
66. Prior approval for any blasting must be obtained from the Division Engineer or their representative.

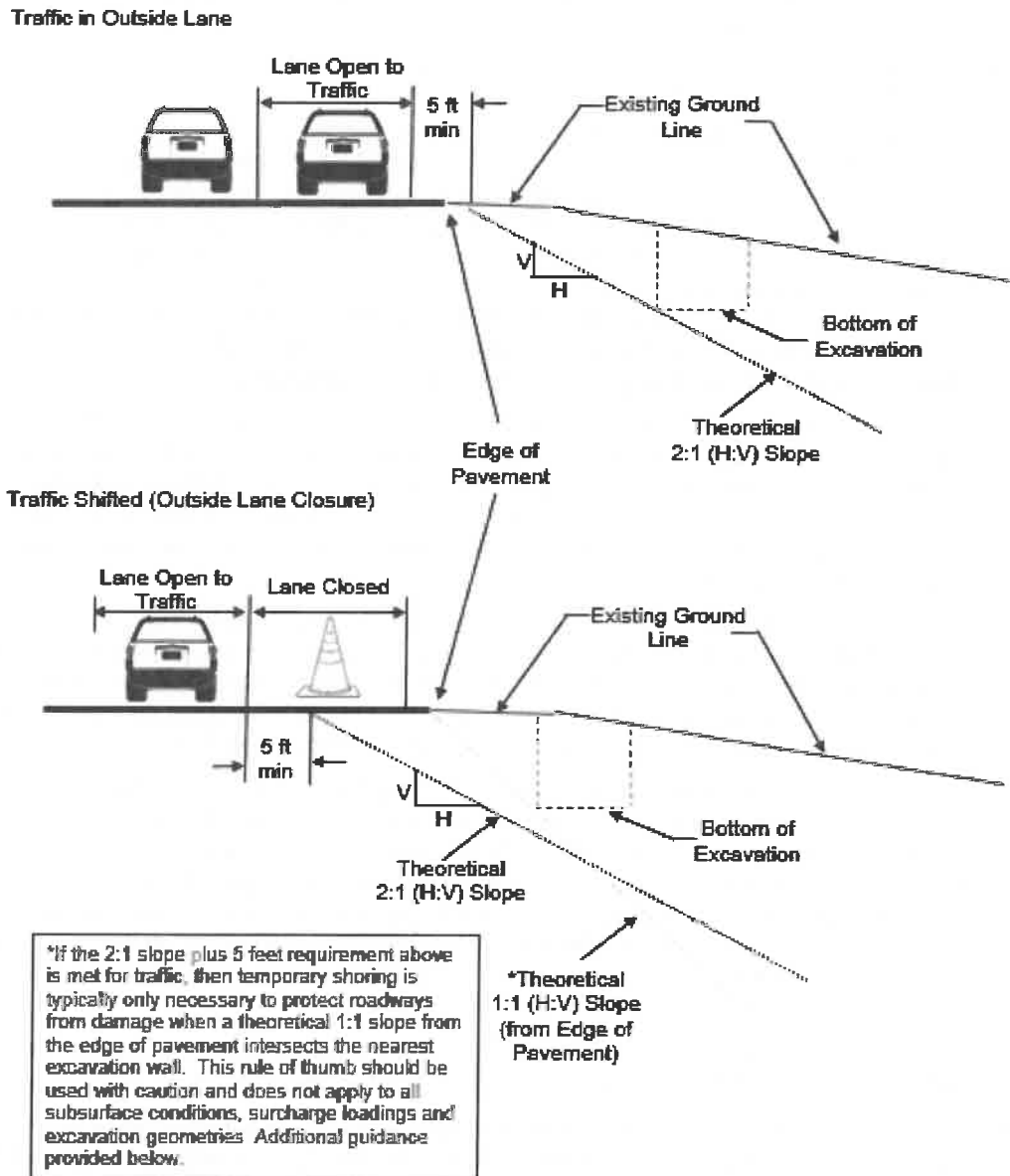
Location within R/W

67. **The utility shall be installed within five (5) feet of the right of way line.**
68. All utility access points, such as manholes, vaults, handholes, splice boxes and junction boxes shall be located as close to the right of way line as possible and shall not be placed in the ditch line, side slopes of the ditches or in the pavement. All manholes, handholes, splice boxes, junction boxes and vaults and covers shall be flush with the ground when located within the vehicle clear zone. Slack loops for telecommunications in industry standard housing units shall be buried a minimum of 18 inches when buried or meet minimum NCDOT vertical and horizontal clearances when installed aerially.
69. Any utility markers, cabinets, pedestals, meter bases and services for meter reading required shall be as close to the Right of Way line as possible. If it is not feasible to install at or near Right of Way line, then written approval shall be obtained from NCDOT prior to installation.
70. Fire Hydrants shall be of the breakaway type. Hydrants shall be placed near the right of way line. In curb and gutter sections with written approval from the District, the hydrants may be placed at 6' behind the back of the curb or minimum 2' back of sidewalk.
71. Hot box (aka ASSE 1060) or Safe-T-Cover type enclosures covering utility main pipe joints, backflow preventers, valves, vent pipes, cross connections, pumps, grinders, irrigation assemblies, transformers, generators, and other similar large appurtenances shall be located outside sight distance triangles and off of the NCDOT Right-of-Way.
72. Sprinkler heads shall be located a minimum of 10 feet from the edge of pavement, edge of shoulder, or back of curb whichever is greater and shall be directed so that water does not spray or drain on the roadway surface, sidewalk, or passing vehicles at any time. Upon completion of the installation and prior to activation of the system, the Encroacher shall contact the District Engineer to schedule a test of the system to verify the spray pattern. Sprinkler systems shall not be operated during periods of high wind or freezing weather, or to the extent that the subgrade adjacent to the pavement structure becomes saturated. NCDOT reserves the right to require immediate termination and removal of any sprinkler system which in its judgement and opinion adversely affects safety, maintenance, or operation of the roadway.

73. Luminaire and/or utility poles and guy wires shall be set as close to the Right of Way line as practical and outside the Clear Zone in accordance with the latest version of the AASHTO Roadside Design Guide (See corresponding attachment) or made breakaway in accordance with the requirements of NCHRP Report 350. Any relocation of the utility poles from the original design due to Clear Zone requirements shall require a re-submittal for the utility design.
74. Luminaire and/or utility poles shall be set a minimum of 5'-6" behind face of any guardrail or otherwise sufficiently protected. However, standard placement may be reduced to 3'-6" behind face of guardrail when posts are spaced 3'-1 ½", or where speed limit is less than 55 MPH.
75. **Any cabinets, pedestals, vents or any other aboveground utility appurtenances installed as part of the underground system extending more than four (4) inches above the ground shall be of breakaway type, located at or near the right of way line, outside the clear zone, outside sight distance triangles and shall not interfere with ADA requirements.**
76. **A minimum of two (2) feet clearance is required for utility installations beneath drainage pipes 54"Φ and below (or equivalent pipe sizes), headwalls, and a minimum of two (2) feet clearance below the flowline of streams. A minimum of five (5) feet clearance is required for utility installations beneath or near drainage pipes 54"Φ and above (or equivalent pipe sizes) as well as box culverts. If directional drilling, a minimum ten (10) foot clearance distance is required from drainage structures.**

Excavation

77. Excavation material shall not be placed on pavement.
78. It is the responsibility of the encroaching party or their contractor to prevent any mud/dirt from tracking onto the roadway. Any dirt which may collect on the roadway pavement from equipment and/or truck traffic on site shall be immediately removed to avoid any unsafe traffic conditions.
79. The utility shall be installed within 5 feet of the right of way line and outside the 5-foot minimum from travel lane plus theoretical 2:1 slope from the edge of pavement to the bottom of the nearest excavation wall for temporary shoring. Temporary shoring is required when a theoretical 2:1 slope from the bottom of excavation will intersect the existing ground line less than 5 feet from the outside edge of an open travel lane as shown in the figure below or when a theoretical 2:1 slope from the bottom of excavation will intersect any existing structure, support, utility, property, etc. to be protected.



If the 2:1 slope plus 5 feet requirement above is met for traffic, then temporary shoring is typically only necessary to protect roadways from damage when a theoretical 1:1 slope from the edge of pavement intersects the nearest excavation wall. This rule of thumb should be used with caution and does not apply to all subsurface conditions, surcharge loadings and excavation geometries. Situations where this 1:1 slope is not recommended include groundwater depth is above bottom of excavation or excavation is deeper than 10 feet or in Type B or C soils as defined by OSHA Technical Manual. Temporary shoring may be avoided by locating trenches, bore pits, and other excavations far enough away from the open travel lane, edge of pavement and any existing structure, support, utility, property, etc. to be protected.

Temporary shoring shall be designed and constructed in accordance with current NCDOT Standard Temporary Shoring provisions (refer to <https://connect.ncdot.gov/resources/Specifications/Pages/2018-Specifications-and-Special-Provisions.aspx> and see SP11 R002

- a. Temporary excavation shoring, such as sheet piling, shall be installed. The design of the shoring shall include the effects of traffic loads. The shoring system shall be designed and sealed by a licensed North Carolina Professional Engineer. Shoring plans and design calculations shall be submitted to the Division Engineer for review and approval prior to construction. (See NCDOT *Utilities Accommodations Manual* for more information on requirements for shoring plans, design calculations, and subsurface investigation report.) **Trench boxes shall not be accepted as temporary shoring and will not be approved for use in instances where shoring is required to protect the highway, drainage structure, and/or supporting pavement or structure foundation.**
 - b. All trench excavation inside the limits of the theoretical two-to-one slope plus 5 feet requirement, as defined by the policy, shall be completely backfilled and compacted at the end of each construction day. No portion of the trench shall be left open overnight. Any excavation that is not backfilled by the end of the workday must address any safety and traveling public concerns including accommodations for bicycles, pedestrians and persons with disabilities.
 - c. The trench backfill material shall meet the Statewide Borrow Criteria. The trench shall be backfilled in accordance with Section 300-7 of the latest *NCDOT Standard Specifications for Roads and Structures*, which basically requires the backfill material to be placed in layers not to exceed 6 inches loose and compacted to at least 95% of the density obtained by compacting a sample in accordance with AASHTO T99 as modified by DOT.
 - d. At the discretion of the Division Engineer, a qualified NCDOT inspector shall be on the site at all times during construction. The encroaching party shall reimburse NCDOT for the cost of providing the inspector. If NCDOT cannot supply an inspector, the encroaching party (not the utility contractor) should make arrangements to have a qualified inspector, under the supervision of a licensed North Carolina Professional Engineer, on the site at all times. The Professional Registered Engineer shall certify that the utility was installed in accordance with the encroachment agreement and that the backfill material meets the Statewide Borrow Criteria.
 - e. The length of parallel excavation shall be limited to the length necessary to install and backfill one joint of pipe at a time, not to exceed twenty-five (25) feet.
80. All material to a depth of 8 inches below the finished surface of the subgrade shall be compacted to a density equal to at least 100% of that obtained by compacting a sample of the material in accordance with AASHTO T99 as modified by the Department. The subgrade shall be compacted at a moisture content which is approximately that required to produce the maximum density indicated by the above test method. The contractor shall dry or add moisture to the subgrade when required to provide a uniformly compacted and acceptable subgrade. The option to backfill any trenches with dirt or either #57 stone or #78 stone with consolidation with a plate tamp and without a conventional density test may be pursued with the written consent of the District Engineer. If this option is exercised, then roadway ABC stone and asphalt repair as required will also be specified by the District Engineer.

Boring

81. Boring equipment will be provided of a type and size to facilitate boring in the local geologic conditions and shall be able to facilitate the encroachment work.

82. When Horizontal Directional Drilling (HDD) is used, the following stipulations apply:

- a. Use drilling fluids as appropriate for the type soils but use of water alone is prohibited. Pump drilling fluids only while drilling or reaming. Directional boring using jetting with a Bentonite (or equivalent material) slurry is recommended. Monitor flow rates to match the amount leaving the bore hole and do not increase pressure or flow to free stuck drill heads, reamers or piping. Open cutting to retrieve stuck drill heads is not allowed without prior permission from the District Engineer.
- b. The minimum depth shall adhere to the table below for transverse (under non-controlled access, partial controlled access, or limited controlled access roadway) installations and refers to maximum diameter of hole drilled and not the dimension of the carrier or encasement pipe.

| <u>Diameter of Drilled Hole (Backream)</u> | <u>Minimum Depth of Cover</u> |
|--|--|
| 2" to 6" | 5 feet |
| >6" to 15" | 12 times hole diameter (e.g. 6-inch hole means 6 feet minimum depth) |
| >15" to 36" | 15 feet or greater |

- c. Under fully controlled access roadway installations, the minimum depth for transverse crossings shall be 15 feet under any pavement (ramps or thru lanes)
- d. An overbore (backream diameter) shall not be more than 1.5 times the outside diameter of the pipe or encasement under any highway for pipes 12 inches in diameter or less. For pipes with outer diameter larger than 12 inches, the overbore may be no larger than outer diameter of pipe plus 6 inches. An overbore exceeding 1.5 times greater than the outside diameter of the pipe or encasement may be considered if the encroachment agreement includes a statement signed and sealed by a licensed North Carolina Professional Engineer indicating that an overbore in excess of 1.5 times the outside diameter of the pipe or encasement will appropriately arch and no damage will be done to the pavement or sub-grade.
- e. Directional boring is allowed beneath embankment material in naturally occurring soil.
- f. Any parallel installation utilizing the directional boring method shall be made at a minimum depth of three (3') feet (cover) below the ground surface and outside the theoretical 1:1 slope from the existing edge of pavement except where the parallel installation crosses a paved roadway.
- g. All directional bores shall maintain ten (10) feet minimum (clear) distance from the nearest part of any structure, including but not limited to bridges, footings, pipe culverts or box culverts. Directional bores are not allowed beneath bridge footings, culvert wingwall footings, slope protection or retaining walls.

- h. The tip of the drill string shall have a cutter head.
- i. Detection wire shall be installed with non-ferrous material.
- j. HDPE pipe installed by directional boring shall not be connected to existing pipe or fittings for one (1) week from the time of installation to allow tensional stresses to relax.

Aerial clearances

- 83. Vertical clearance of overhead power and communication lines shall meet the National Electrical Safety Code requirements except the minimum vertical clearance shall be 18' for crossings over NCDOT roadways (24' over Fully Controlled Access roadways) and 16' for parallel installations.
- 84. When applicable for aerial installations, in relation to the bridge, the utility line shall be located with minimum clearances as indicated in Figure 3-3 in the Utilities Accommodations Manual for NCDOT **Required Clearances for Aerial Installations by Encroachment Near Bridge Structures**.

Pavement Detail and Repair

- 85. The paving of this roadway shall be in accordance with the latest version of NCDOT Standard Specifications, Sections 610, 1012 and 1020. The Contractor shall follow all procedures of the latest Quality Management System (QMS) Asphalt Manual for asphalt pavement - Maintenance Version (see <https://connect.ncdot.gov/resources/Materials/MaterialsResources/Forms/Default.aspx>) to find the most recent version. The Contractor must adhere to all testing requirements and quality control requirements specified. The Contractor shall contact the NCDOT Division QA Supervisor prior to producing plant mix and make the Supervisor aware that the mix is being produced for a future NCDOT road. Contact the District Engineer to determine the NCDOT Division QA Supervisor. Only NCDOT approved mix designs will be acceptable. A Quality Control Plan shall be submitted (as Directed by the District Engineer) to the District Engineer's Office prior to asphalt production utilizing form QMS-MV1. Failing mixes and/or densities are subject to penalties including monetary payments or removal and replacement. To minimize traffic queuing in construction areas, the possibility of traffic detours may be considered when working on high traffic routes even if traffic control is used. The District Engineer may require traffic detours.
- 86. When paving beyond utility installation is involved, a Roadway certification report sealed by a Professional Engineer shall be submitted to the District Engineer's office indicating the following:
 - Pavement thickness by type
 - Pavement density, core and/or test locations
 - Base thickness
 - Base density
 - Subgrade density

Test frequency and method shall be in conformance with the NCDOT *Materials and Tests Manual*. Test must be performed by a Certified Technician including name and Certification number on report.

87. **“Potholing” (or “daylighting”) pavement cores to expose existing utilities shall be made with a circular minimum 6” to maximum 18” diameter “test” hole to a maximum depth of 12 inches.** Pavement core locations shall not be placed in the wheel path whenever possible. Vacuum excavation shall be utilized to expose underground utilities below pavement subgrade. Displaced dirt and rock debris must be suctioned away from the excavation area through a large hose to a vacuum truck and disposed by the encroaching party. Avoid using mechanized equipment in the proximity of all exposed underground utility lines. Pavement cores shall be repaired within the same working day. The pavement core shall be retained and evaluated for reuse to fill the core hole.

The excavation shall be backfilled and compacted with select material to the bottom of the existing pavement structure or as indicated by the District Engineer. If in good condition, the retained core shall be placed in the hole and secured with a waterproof, mechanical joint. If the pavement core is damaged and cannot be re-used, the core may be replaced with the surface mix, S 9.5 C. The asphalt patch shall match the thickness of the existing asphalt or four inches, whichever is greater and the use of NCDOT approved sealant applied to the cracks to fill the voids. All materials must be listed on the NCDOT Approved Products List (APL) found at: <https://apps.ncdot.gov/vendor/approvedproducts/>.

88. All open cuts (**if permitted**) on primary routes will require full depth patching with 5.0” of B 25.0 B (ACBC) Asphalt Concrete Base Course, 3.0” of I 19.0 B (ACIC) Asphalt Concrete Intermediate Course and 2.0” of S 9.5 B (ACSC) Asphalt Concrete Surface Course the same day as cut is made. It will also be required to mill the existing pavement surface at a depth of 2.0” and a width of 1.0’ on each side of the cut and transversely across the width of the travel lane to key in the patch with the existing pavement surface in accordance with the attached detail.
89. All open cuts (**if permitted**) on secondary routes will require full depth patching with 4.0” of B 25.0 C (ACBC) Asphalt Concrete Base Course and 3.0” of S 9.5 C (ACSC) Asphalt Concrete Surface Course the same day as cut is made. It will also be required to mill the existing pavement surface at a depth of 1.5” and a width of 1.0’ on each side of the cut and transversely across the width of the travel lane to key in the patch with the existing pavement surface in accordance with the attached detail.
90. Eight inches of ABC will be used as the base. Compaction test shall be performed at the location of every open cut that crosses NCDOT roadways. The owner will be required to have an approved laboratory furnish the District Office a copy of the test results.




91. Pavement cuts shall be repaired the same day the cuts are made unless an asphalt patch cannot be accomplished the same day due to material availability or time restrictions. When the asphalt patch is not feasible, the following apply:
 - a. The pavement cut shall be filled to the surface with ABC stone or Flowable Fill per NCDOT's Standards and Specifications.
 - b. Once the cut is filled, a minimum ¾-inch steel plate shall be placed and pinned to prevent moving. Plates shall be designed large enough to span a minimum of 1-foot on all sides on the pavement cut.
 - c. When flowable fill is used, it shall cure for 24 hours prior to any asphalt material placement. Flowable fill bleed water shall not be present during paving operations. Paving shall not cause damage (shoving, distortion, pumping, etc.) to the flowable fill.
 - d. Install and leave "BUMP" signs according to MUTCD until the steel plate has been removed. Once the flowable fill has cured, remove the steel plate, and mill/fill according to the directions of the District Engineer.
 - e. All pavement cuts must be sealed with NCDOT approved sealant to prevent future pavement separation or cracking.
92. **Any pavement damaged because of settlement of the pavement or damaged by equipment used to perform encroachment work, shall be re-surfaced to the satisfaction of the District Engineer. This may include the removal of pavement and a 50' mechanical overlay. All pavement work and pavement markings (temporary and final) are the responsibility of the Encroaching Party.**
93. All concrete installed within NCDOT rights of way shall be constructed in accordance with the latest NCDOT **Standard Specifications for Roads and Structures** and **Roadway Standard Drawings** and Amendments or Supplementals thereto. All concrete shall be an approved NCDOT Class B mix. All materials testing results shall be provided to the District Engineer upon completion of the project.
94. All concrete sidewalk installed within NCDOT rights of way shall be constructed in accordance with the latest NCDOT **Standard Specifications for Roads and Structures** and **Roadway Standard Drawings** (Std. Dwg. No. 846.01 and 848.01) and Amendments or Supplementals thereto. All concrete shall be an approved NCDOT Class B mix. All materials testing results shall be provided to the District Engineer upon completion of the project.
95. All ADA compliant curb ramps shall be constructed in accordance with the latest NCDOT **Standard Specifications for Roads and Structures** and **Roadway Standard Drawings** (Std. Dwg. No. 848.06) and Amendments or Supplementals thereto including but not limited to the Alternate Curb Ramp Designs (Curb Ramp Details - Parallel Ramps). All concrete shall be an approved NCDOT Class B mix. All materials testing results shall be provided to the District Engineer upon completion of the project.
96. All 30" curb and gutter within NCDOT rights of way shall be constructed with Class B concrete in accordance with Section 846 of the latest NCDOT **Standard Specifications for Roads and Structures** and **Roadway Standard Drawings** (Std. Dwg. No. 846.01) and Amendment or Supplemental thereto or as directed by the engineer. All concrete testing results shall be provided to the District Engineer's office at time of project completion.

Post Construction

Close out/ Inspection

97. The Encroaching party shall notify the District Engineer's office within 2 business days after construction is complete. The District Engineer may perform a construction inspection. Any deficiencies may be noted and reported to the encroaching party to make immediate repairs or resolve any issues to restore the right-of-way to a similar condition prior to construction, including pavement, signage, traffic signals, pavement markings, drainage, structures/pipes, or other highway design features.
98. At the discretion of the District Engineer, a final inspection report may be provided to the encroaching party upon satisfactory completion of the work.
99. A written acknowledgement of the completed work by the District Engineer's office begins the one-year warranty period associated with the performance bond.
100. **A copy of the "as-built" plan shall be submitted to the District Engineer's office (online encroachment database) in a PDF format and in a current ESRI GIS format within 4 weeks of construction along with an executed Certification Memo. The As-Built drawing(s) shall depict the horizontal and vertical locations of all utilities and associated appurtenances.**
101. A copy (in PDF format) of the completed ground water analysis shall be given to the District Engineer, including detailed drawings of the "as-built" wells showing location, depth and water level in well.

If further information or assistance is needed in reference to this project, please feel free to call Mr. Lee R. Hines, Jr. (Richie), PE, District Engineer at (910) 364-0601.

Sincerely,
Signed by:  Initial: 

Wallace "Lee" Jernigan, Jr., PE
Division Engineer

WLJ:tlb:eab

cc: <https://connect.ncdot.gov/site/Permits/Pages/All-Submissions.aspx>

DEPARTMENT OF TRANSPORTATION RIGHT OF WAY ENCROACHMENT AGREEMENT
-AND- PRIMARY AND SECONDARY HIGHWAYS

Harnett Regional Water
POB 1119, 700 McKinney Parkway
Lillington, North Carolina 27546

THIS AGREEMENT, made and entered into this the 25 day of March 2026 by and between the Department of Transportation, party of the first part; and Harnett Regional Water party of the second part,

WITNESSETH

THAT WHEREAS, the party of the second part desires to encroach on the right of way of the public road designated as Route(s) US 421 (E Jackson Blvd), located starting on the south side of US421 at the intersection of St Matthews Road (Town of Erwin Maintained roadway) and US421, then along US 421 to the east approximately 530 linear feet with the construction and/or erection of: an 8-inch diameter water main with appurtenances.

See Approval Letter

WHEREAS, it is to the material advantage of the party of the second part to effect this encroachment, and the party of the first part in the exercise of authority conferred upon it by statute, is willing to permit the encroachment within the limits of the right of way as indicated, subject to the conditions of this agreement;

NOW, THEREFORE, IT IS AGREED that the party of the first part hereby grants to the party of the second part the right and privilege to make this encroachment as shown on attached plan sheet(s), specifications and special provisions which are made a part hereof upon the following conditions, to wit:

That the installation, operation, and maintenance of the above described facility will be accomplished in accordance with the party of the first part's latest UTILITIES ACCOMMODATIONS MANUAL, and such revisions and amendments thereto as may be in effect at the date of this agreement. Information as to these policies and procedures may be obtained from the Division Engineer or State Utilities Manager of the party of the first part.

That the said party of the second part binds and obligates himself to install and maintain the encroaching facility in such a safe and proper condition that it will not interfere with or endanger travel upon said highway, nor obstruct nor interfere with the proper maintenance thereof, to reimburse the party of the first part for the cost incurred for any repairs or maintenance to its roadways and structures necessary due to the installation and existence of the facilities of the party of the second part, and if at any time the party of the first part shall require the removal of or changes in the location of the said facilities, that the said party of the second part binds himself, his successors and assigns, to promptly remove or alter the said facilities, in order to conform to the said requirement, without any cost to the party of the first part.

That the party of the second part agrees to provide during construction and any subsequent maintenance proper signs, signal lights, flagmen and other warning devices for the protection of traffic in conformance with the latest Manual on Uniform Traffic Control Devices for Streets and Highways and Amendments or Supplements thereto. Information as to the above rules and regulations may be obtained from the Division Engineer of the party of the first part.

That the party of the second part hereby agrees to indemnify and save harmless the party of the first part from all damages and claims for damage that may arise by reason of the installation and maintenance of this encroachment.

That the party of the second part agrees to restore all areas disturbed during installation and maintenance to the satisfaction of the Division Engineer of the party of the first part. The party of the second part agrees to exercise every reasonable precaution during construction and maintenance to prevent eroding of soil; silting or pollution of rivers, streams, lakes, reservoirs, other water impoundments, ground surfaces or other property; or pollution of the air. There shall be compliance with applicable rules and regulations of the North Carolina Division of Environmental Management, North Carolina Sedimentation Control Commission, and with ordinances and regulations of various counties, municipalities and other official agencies relating to pollution prevention and control. When any installation or maintenance operation disturbs the ground surface and existing ground cover, the party of the second part agrees to remove and replace the sod or otherwise reestablish the grass cover to meet the satisfaction of the Division Engineer of the party of the first part.

That the party of the second part agrees to assume the actual cost of any inspection of the work considered to be necessary by the Division Engineer of the party of the first part.

That the party of the second part agrees to have available at the construction site, at all times during construction, a copy of this agreement showing evidence of approval by the party of the first part. The party of the first part reserves the right to stop all work unless evidence of approval can be shown.

Provided the work contained in this agreement is being performed on a completed highway open to traffic; the party of the second part agrees to give written notice to the Division Engineer of the party of the first part when all work contained herein has been completed. Unless specifically requested by the party of the first part, written notice of completion of work on highway projects under construction will not be required.

That in the case of noncompliance with the terms of this agreement by the party of the second part, the party of the first part reserves the right to stop all work until the facility has been brought into compliance or removed from the right of way at no cost to the party of the first part.

That it is agreed by both parties that this agreement shall become void if actual construction of the work contemplated herein is not begun within one (1) year from the date of authorization by the party of the first part unless written waiver is secured by the party of the second part from the party of the first part.

During the performance of this contract, the second party, for itself, its assignees and successors in interest (hereinafter referred to as the "contractor"), agrees as follows:

- a. Compliance with Regulations: The contractor shall comply with the Regulations relative to nondiscrimination in Federally-assisted programs of the U. S. Department of Transportation, Title 49, Code of Federal Regulations, Part 21, as they may be amended from time to time, (hereinafter referred to as the Regulations), which are herein incorporated by reference and made a part of this contract.
- b. Nondiscrimination: The contractor, with regard to the work performed by it during the contract, shall not discriminate on the grounds of race, color, or national origin in the selection and retention of subcontractors, including procurements of materials

and leases of equipment. The contractor shall not participate either directly or indirectly in the discrimination prohibited by Section 21.5 of the Regulations, including employment practices when the contract covers a program set forth in Appendix B of the Regulations.

- c. **Solicitations for Subcontracts, including Procurements of Materials and Equipment:** In all solicitations either by competitive bidding or negotiation made by the contractor for work to be performed under a subcontract, including procurements of materials or leases of equipment, each potential subcontractor or supplier shall be notified by the contractor of the contractor's obligations under this contract and the Regulations relative to nondiscrimination on the grounds of race, color, or national origin.
- d. **Information and Reports:** The contractor shall provide all information and reports required by the Regulations, or directives issued pursuant thereto, and shall permit access to its books, records, accounts, other sources of information, and its facilities as may be determined by the Department of Transportation or the Federal Highway Administration to be pertinent to ascertain compliance with such Regulations or directives. Where any information required of a contractor is in the exclusive possession of another who fails or refuses to furnish this information, the contractor shall so certify to the Department of Transportation, or the Federal Highway Administration as appropriate, and shall set forth what efforts it has made to obtain the information.
- e. **Sanctions for Noncompliance:** In the event of the contractor's noncompliance with the nondiscrimination provisions of this contract, the Department of Transportation shall impose such contract sanctions as it or the Federal Highway Administration may determine to be appropriate, including, but not limited to,
 - (1) withholding of payments to the contractor under the contract until the contractor complies, and/or
 - (2) cancellation, termination or suspension of the contract, in whole or in part.
- f. **Incorporation of Provisions:** The contractor shall include the provisions of paragraphs "a" through "f" in every subcontract, including procurements of materials and leases of equipment, unless exempt by the Regulations, or directives issued pursuant thereto. The contractor shall take such action with respect to any subcontract or procurement as the Department of Transportation or the Federal Highway Administration may direct as a means of enforcing such provisions including sanctions for noncompliance: Provided, however, that, in the event a contractor becomes involved in, or is threatened with, litigation with a subcontractor or supplier as a result of such direction, the contractor may request the Department of Transportation to enter into such litigation to protect the interests of the State, and, in addition, the contractor may request the United States to enter into such litigation to protect the interests of the United States.

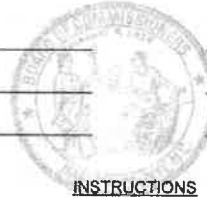
R/W (161) : Party of the Second Part certifies that this agreement is true and accurate copy of the form R/W (161) incorporating all revisions to date.

IN WITNESS WHEREOF, each of the parties to this agreement has caused the same to be executed the day and year first above written.

DEPARTMENT OF TRANSPORTATION
BY: Wallace "W" Jernigan, Jr., PE ^{Initial} TBS
DIVISION ENGINEER

ATTEST OR WITNESS:

Melissa Capps
Melissa Capps, County Clerk
Harnett County
455 McKinney Parkway
Lillington, NC 27546



Tommy Burns
Tommy Burns, Utility Director
Harnett Regional Water
Second Party 700 McKinney Parkway
Lillington, NC 27546

When the applicant is a corporation or a municipality, this agreement must have the corporate seal and be attested by the corporation secretary or by the empowered city official, unless a waiver of corporate seal and attestation by the secretary or by the empowered City official is on file in the Raleigh office of the State Utilities Manager. In the space provided in this agreement for execution, the name of the corporation or municipality shall be typed above the name, and title of all persons signing the agreement should be typed directly below their signature.

When the applicant is not a corporation, then his signature must be witnessed by one person. The address should be included in this agreement and the names of all persons signing the agreement should be typed directly below their signature.

This agreement must be accompanied, in the form of an attachment, by plans or drawings showing the following applicable information:

1. All roadways and ramps.
2. Right of way lines and where applicable, the control of access lines.
3. Location of the existing and/or proposed encroachment.
4. Length, size and type of encroachment.
5. Method of installation.
6. Dimensions showing the distance from the encroachment to edge of pavement, shoulders, etc.
7. Location by highway survey station number. If station number cannot be obtained, location should be shown by distance from some identifiable point, such as a bridge, road, intersection, etc. (To assist in preparation of the encroachment plan, the Department's roadway plans may be seen at the various Highway Division Offices, or at the Raleigh office.)
8. Drainage structures or bridges if affected by encroachment (show vertical and horizontal dimensions from encroachment to nearest part of structure).
9. Method of attachment to drainage structures or bridges.
10. Manhole design.
11. On underground utilities, the depth of bury under all traveled lanes, shoulders, ditches, sidewalks, etc.
12. Length, size and type of encasement where required.
13. On underground crossings, notation as to method of crossing - boring and jacking, open cut, etc.
14. Location of vents.

GENERAL REQUIREMENTS

1. Any attachment to a bridge or other drainage structure must be approved by the State Utilities Manager in Raleigh prior to submission of encroachment agreement to the Division Engineer.
2. All crossings should be as near as possible normal to the centerline of the highway.
3. Minimum vertical clearances of overhead wires and cables above all roadways must conform to clearances set out in the National Electric Safety Code.
4. Encasements shall extend from ditch line to ditch line in cut sections and 5' beyond toe of slopes in fill sections.
5. All vents should be extended to the right of way line or as otherwise required by the Department.
6. All pipe encasements as to material and strength shall meet the standards and specifications of the Department.
7. Any special provisions or specifications as to the performance of the work or the method of construction that may be required by the Department must be shown on a separate sheet attached to encroachment agreement provided that such information cannot be shown on plans or drawings.
8. The Department's Division Engineer should be given notice by the applicant prior to actual starting of installation included in this agreement.

TRAFFIC CONTROL AND WORK ZONE SAFETY

The Contractor shall maintain traffic during construction and provide, install, and maintain all traffic control devices in accordance with these project guidelines, the Project Special Provisions, the latest North Carolina Department of Transportation Standard Specifications for Roads and Structures, and the current edition of the Manual of Uniform Traffic Control Devices (MUTCD).

The Contractor shall utilize complete and proper traffic controls and traffic control devices during all operations. All traffic control and traffic control devices required for any operation shall be functional and in place prior to the commencement of that operation. Signs for temporary operations shall be removed during periods of inactivity. The Contractor is required to leave the project in a manner that will be safe to the traveling public and which will not impede motorists.

Traffic movements through lane closures on roads with two way traffic shall be controlled by flaggers stationed at each end of the work zone. In situations where sight distance is limited, the Contractor shall provide additional means of controlling traffic, including, but not limited to, two-way radios, pilot vehicles, or additional flaggers. Flaggers shall be competent personnel, adequately trained in flagging procedures, and furnished with proper safety devices and equipment, including, but not limited to, safety vests and stop/slow paddles.

All personnel when working in traffic areas or areas in close proximity to traffic shall wear an approved safety vest, or shirt or jacket which meets the color requirements of the Manual of Uniform Traffic Control Devices (MUTCD).

The Contractor shall comply with all applicable Federal, State, and local laws, ordinances, and regulations governing safety, health, and sanitation, and shall provide all safeguards, safety devices, and protective equipment, and shall take any other needed actions, on his own responsibility that are reasonably necessary to protect the life and health of employees on the job and the safety of the public, and to protect property in connection with the performance of the work covered by the contract.

Failure to comply with any of the requirements for safety and traffic control of this contract shall result in suspension of work as provided in subarticle 108-7(2) of the Standard Specifications.

SPECIFICATIONS FOR EROSION CONTROL

The Contractor shall seed all disturbed areas as directed by the Engineer, in accordance with Section 1660 of the Standard Specifications. Seeding and mulching shall immediately follow shoulder construction operations and in no case shall shoulder construction operations exceed seeding and mulching operations by more than two weeks without written permission of the Engineer. Failure to meet this requirement shall be cause to cease all operations until it can be met.

Seeding and Mulching: (East)

The kinds of seed and fertilizer, and the rates of application of seed, fertilizer, and limestone, shall be as stated below. During periods of overlapping dates, the kind of seed to be used shall be determined by the Engineer. All rates are in pounds per acre.

The kinds of seed and fertilizer, and the rates of application of seed, fertilizer, and limestone, shall be as stated below. During periods of overlapping dates, the kind of seed to be used shall be determined. All rates are in pounds per acre.

All Roadway Areas

March 1 - August 31

| | |
|-------|-----------------------|
| 50# | Tall Fescue |
| 10# | Centipede |
| 25# | Bermudagrass (hulled) |
| 500# | Fertilizer |
| 4000# | Limestone |

September 1 - February 28

| | |
|-------|-------------------------|
| 50# | Tall Fescue |
| 10# | Centipede |
| 35# | Bermudagrass (unhulled) |
| 500# | Fertilizer |
| 4000# | Limestone |

Waste and Borrow Locations

March 1 - August 31

| | |
|-------|-----------------------|
| 75# | Tall Fescue |
| 25# | Bermudagrass (hulled) |
| 500# | Fertilizer |
| 4000# | Limestone |

September 1 - February 28

| | |
|-------|-------------------------|
| 75# | Tall Fescue |
| 35# | Bermudagrass (unhulled) |
| 500# | Fertilizer |
| 4000# | Limestone |

Note: 50# of Bahiagrass may be substituted for either Centipede or Bermudagrass only upon Engineer's request.

Approved Tall Fescue Cultivars

| | | | |
|----------------------------|------------------------|--------------|--------------------|
| 2 nd Millennium | Duster | Magellan | Rendition |
| Avenger | Endeavor | Masterpiece | Scorpion |
| Barlexas | Escalade | Matador | Shelby |
| Barlexas II | Falcon II, III, IV & V | Matador GT | Signia |
| Barrera | Fidelity | Millennium | Silverstar |
| Barrington | Finesse II | Montauk | Southern Choice II |
| Biltmore | Firebird | Mustang 3 | Stetson |
| Bingo | Focus | Olympic Gold | Tarheel |
| Bravo | Grande II | Padre | Titan Ltd |
| Cayenne | Greenkeeper | Paraiso | Titanium |
| Chapel Hill | Greystone | Picasso | Tomahawk |
| Chesapeake | Inferno | Piedmont | Tacer |
| Constitution | Justice | Pure Gold | Trooper |
| Chipper | Jaguar 3 | Prospect | Turbo |
| Coronado | Kalahari | Quest | Ultimate |
| Coyote | Kentucky 31 | Rebel Exeda | Watchdog |
| Davinci | Kitty Hawk | Rebel Sentry | Wolfpack |
| Dynasty | Kitty Hawk 2000 | Regiment II | |
| Dominion | Lexington | Rembrandt | |

On cut and fill slopes 2:1 or steeper Centipede shall be applied at the rate of 5 pounds per acre and add 20# of Sericea Lespedeza from January 1 - December 31.

Fertilizer shall be 10-20-20 analysis. A different analysis of fertilizer may be used provided the 1-2-2 ratio is maintained and the rate of application adjusted to provide the same amount of plant food as a 10-20-20 analysis and as directed.

All areas seeded and mulched shall be tacked with asphalt. Crimping of straw in lieu of asphalt tack shall not be allowed on this project.

CRIMPING STRAW MULCH

Crimping shall be required on this project adjacent to any section of roadway where traffic is to be maintained or allowed during construction. In areas within six feet of the edge of pavement, straw is to be applied and then crimped. After the crimping operation is complete, an additional application of straw shall be applied and immediately tacked with a sufficient amount of undiluted emulsified asphalt.

Straw mulch shall be of sufficient length and quality to withstand the crimping operation.

Crimping equipment including power source shall be subject to the approval of the Engineer providing that maximum spacing of crimper blades shall not exceed 8".

Within seven (7) calendar days to fourteen (14) calendar days of completion of any phase of grading, all disturbed areas shall be planted or otherwise provided with temporary or permanent ground cover, devices, or structures sufficient to restrain erosion. The Erosion and Sediment Control plan will identify the areas that require seven (7) and/or fourteen (14) calendar day ground stabilization. The Contractor is herein advised to follow all current regulations set forth by the North Carolina Department of Environment and Natural Resources (NCDENR) Division of Water Quality (DWQ) as defined in the *General Stormwater Permit for Construction Activities NCG-010000*.

U.S. Customary Units

| Design Speed (mph) | Design ADT | Foreslopes | | | Bankslopes | | |
|--------------------|------------------------|--------------------|--------------------|-------|------------|----------------|------------------|
| | | 1V:6H or flatter | 1V:5H to 1V:4H | 1V:3H | 1V:3H | 1V:5H to 1V:4H | 1V:6H or flatter |
| ≤40 | UNDER 750 ^a | 7-10 | 7-10 | * | 7-10 | 7-10 | 7-10 |
| | 750-1500 | 10-12 | 12-14 | * | 12-14 | 12-14 | 12-14 |
| | 1500-6000 | 12-14 | 14-16 | * | 14-16 | 14-16 | 14-16 |
| | OVER 6000 | 14-16 | 16-18 | * | 16-18 | 16-18 | 16-18 |
| 45-50 | UNDER 750 ^a | 10-12 | 12-14 | * | 8-10 | 8-10 | 10-12 |
| | 750-1500 | 14-16 | 16-20 | * | 10-12 | 12-14 | 14-16 |
| | 1500-6000 | 16-18 | 20-26 | * | 12-14 | 14-16 | 16-18 |
| | OVER 6000 | 20-22 | 24-28 | * | 14-16 | 18-20 | 20-22 |
| 55 | UNDER 750 ^a | 12-14 | 14-18 | * | 8-10 | 10-12 | 10-12 |
| | 750-1500 | 16-18 | 20-24 | * | 10-12 | 14-16 | 16-18 |
| | 1500-6000 | 20-22 | 24-30 | * | 14-16 | 16-18 | 20-22 |
| | OVER 6000 | 22-24 | 26-32 ^b | * | 16-18 | 20-22 | 22-24 |
| 60 | UNDER 750 ^a | 16-18 | 20-24 | * | 10-12 | 12-14 | 14-16 |
| | 750-1500 | 20-24 | 26-32 ^b | * | 12-14 | 16-18 | 20-22 |
| | 1500-6000 | 26-30 | 32-40 ^b | * | 14-18 | 18-22 | 24-26 |
| | OVER 6000 | 30-32 ^b | 36-44 ^b | * | 20-22 | 24-26 | 26-28 |
| 65-70 ^d | UNDER 750 ^a | 18-20 | 20-26 | * | 10-12 | 14-16 | 14-16 |
| | 750-1500 | 24-26 | 28-36 ^b | * | 12-16 | 18-20 | 20-22 |
| | 1500-6000 | 28-32 ^b | 34-42 ^b | * | 16-20 | 22-24 | 26-28 |
| | OVER 6000 | 30-34 ^b | 38-46 ^b | * | 22-24 | 26-30 | 28-30 |

Notes:

- a) When a site-specific investigation indicates a high probability of continuing crashes or when such occurrences are indicated by crash history, the designer may provide clear-zone distances greater than the clear zone shown in Table 3-1. Clear zones may be limited to 30 ft for practicality and to provide a consistent roadway template if previous experience with similar projects or designs indicates satisfactory performance.
- b) Because recovery is less likely on the unshielded, traversable 1V:3H fill slopes, fixed objects should not be present in the vicinity of the toe of these slopes. Recovery of high-speed vehicles that encroach beyond the edge of the shoulder may be expected to occur beyond the toe of slope. Determination of the width of the recovery area at the toe of slope should consider right-of-way availability, environmental concerns, economic factors, safety needs, and crash histories. Also, the distance between the edge of the through traveled lane and the beginning of the 1V:3H slope should influence the recovery area provided at the toe of slope. While the application may be limited by several factors, the foreslope parameters that may enter into determining a maximum desirable recovery area are illustrated in Figure 3-2. A 10-ft recovery area at the toe of slope should be provided for all traversable, non-recoverable fill slopes.
- c) For roadways with low volumes it may not be practical to apply even the minimum values found in Table 3-1. Refer to Chapter 12 for additional considerations for low volume roadways and Chapter 10 for additional guidance for urban applications.
- d) When design speeds are greater than the values provided, the designer may provide clear-zone distances greater than those shown in Table 3-1.

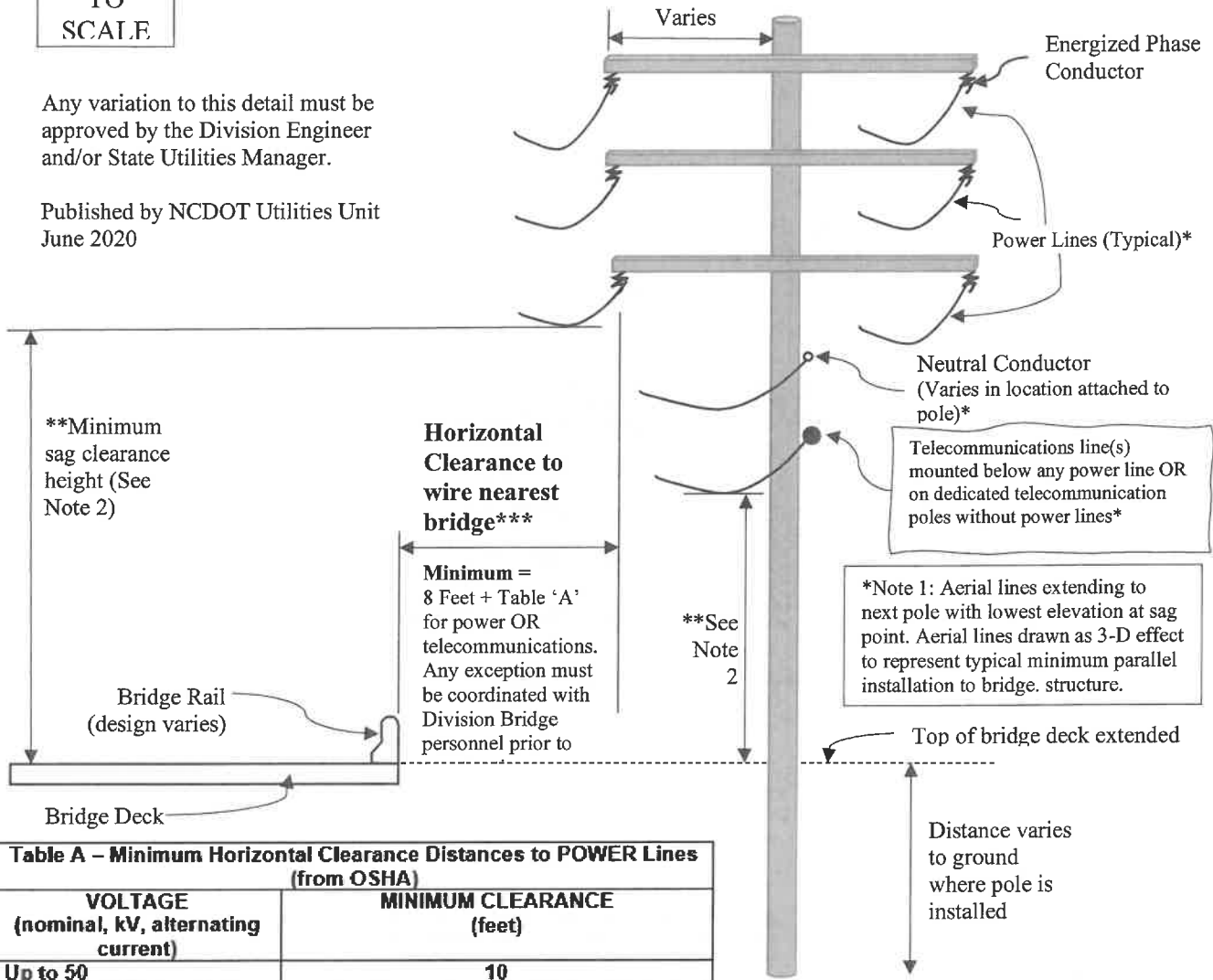
NCDOT Required Clearances for Aerial Installations Near Bridge Structures

NOT TO SCALE

Any variation to this detail must be approved by the Division Engineer and/or State Utilities Manager.

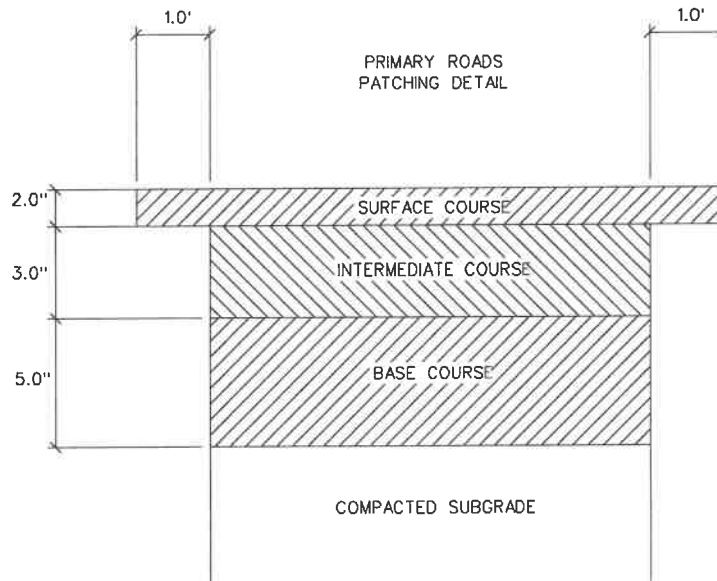
Published by NCDOT Utilities Unit
June 2020

Utility Pole (Typical-Power and/or Telecommunications)



| VOLTAGE (nominal, kV, alternating current) | MINIMUM CLEARANCE (feet) |
|--|---|
| Up to 50 | 10 |
| Over 50 to 200 | 15 |
| Over 200 to 350 | 20 |
| Over 350 to 500 | 25 |
| Over 500 to 750 | 35 |
| Over 750 to 1000 | 45 |
| Over 1000 | As established by the utility owner/operator or registered professional engineer who is a qualified person with respect to electrical power transmission and distribution |

***Note 3: HORIZONTAL CLEARANCE EXCEPTION. If vertical sag clearance height for power above bridge deck is ≥ 45 feet AND voltage is ≤ 350 kV, then Minimum Horizontal Clearance may be reduced to 3 feet. Any telecommunications attachment to power pole allowed in this exception must have a minimum 25 feet sag clearance height above bridge deck.



PAVEMENT SCHEDULE

MILLING REQUIRED ONE FOOT AT DEPTH OF 2.0"
ON EACH SIDE OF PAVEMENT CUT AND TRANSVERSELY
ACROSS THE THE WIDTH OF THE TRAVEL LANE.

MATCH EXISTING PAVEMENT DEPTHS OR WHICHEVER IS GREATER.

2.0" S 9.5 C

3.0" I 19.0 C

5.0" B 25.0 C

DRAWING IS NOT TO SCALE

**North Carolina Department of Transportation
Subdivision Road Construction, Driveway Access, and
Encroachment Installation Certification Memo**

(Date)

(District Engineer Name & Address)

RE: CONSTRUCTION CERTIFICATION

County: _____

Driveway Permit #: _____

and/or Subdivision I.D. #: _____

and/or Encroachment #: _____

Recording Information (if applicable): _____

Route(s) and/or Street(s): _____

Project Name/Phase/Description: _____

I, _____, have reviewed the construction within the right-of-way and/or easements required under subject project(s) for the above development and in accordance with the design drawings approved by the North Carolina Department of Transportation (NCDOT) on the following dates:

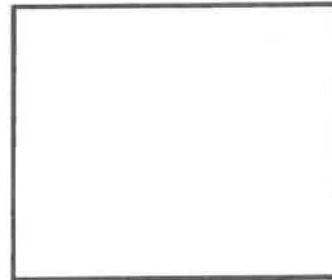
| | | | |
|----------------------------|-------|-------|-------|
| Driveway Permit #: | _____ | Date: | _____ |
| and/or Subdivision I.D. #: | _____ | Date: | _____ |
| and/or Encroachment #: | _____ | Date: | _____ |

My inspection and attached testing report(s) and/or supporting documentation indicate the construction within the right-of-way and/or easements have been constructed in accordance with the standards established by current NCDOT Standard Specifications for Roads and Structures, and with the approved plans.

Name: _____

NC PE #: _____

Signature: _____



SEAL

Received by NCDOT: _____

ATTACHMENT FORM

NOTIFICATION FOR UTILITY / NON-UTILITY ENCROACHMENT WITHIN NCDOT R/W

Instructions for use:

This form must be completed in its entirety and submitted directly to the designated personnel in the District Engineer's office via email, fax or hand delivery a minimum of one week prior to construction for the encroachment. If the designated NCDOT personnel names are unknown by the person completing this form, please contact the District Engineer's office to determine that contact info.

Date: _____ Submitted by Name: _____

To: District Personnel Name: _____
District Personnel Email: _____
District Fax No.: _____

This notification is to inform you that we (encroaching party or their contractor) will begin construction work on the following project in a minimum of one week.

Encroachment number
(assigned by NCDOT) for the project: _____

Construction start date: _____

Approximate ending date: _____

Contact NCDOT inspector a minimum of 72 hrs. in advance to set-up Preconstruction meeting in the District Engineer's office or other location as directed by the District Engineer

Preconstruction meeting date & time: _____

Preconstruction meeting address: _____

Type of project: _____
[Examples: power, telecommunication, water, sewer, gas, petroleum, other (describe)]

Contact Info for this project:

Contractor Company Name: _____

Contractor Contact Name: _____

Contractor Phone Number: _____

Contractor Email: _____

NCDOT Utility Inspector Name: _____

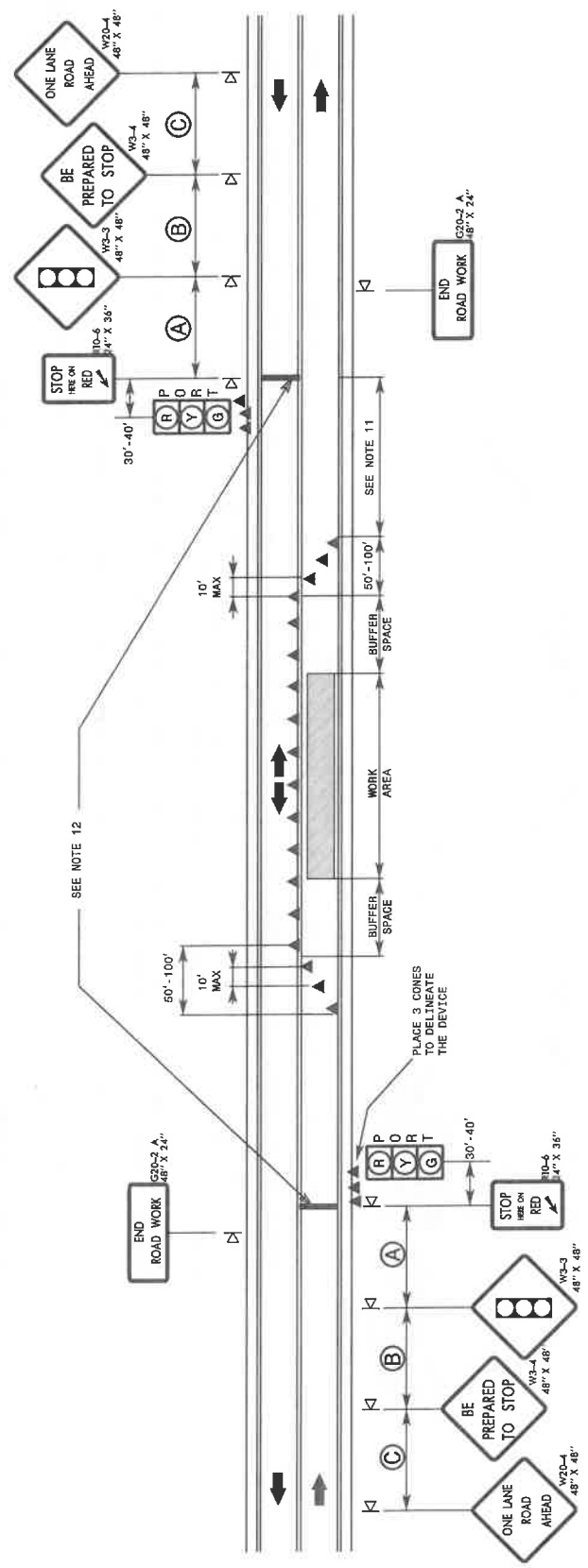
NCDOT Utility Inspector Phone: _____

NCDOT Utility Inspector Email: _____

NCDOT Utility Project Manager Name: _____

NCDOT Utility Project Manager Phone: _____

NCDOT Utility Project Manager Email: _____



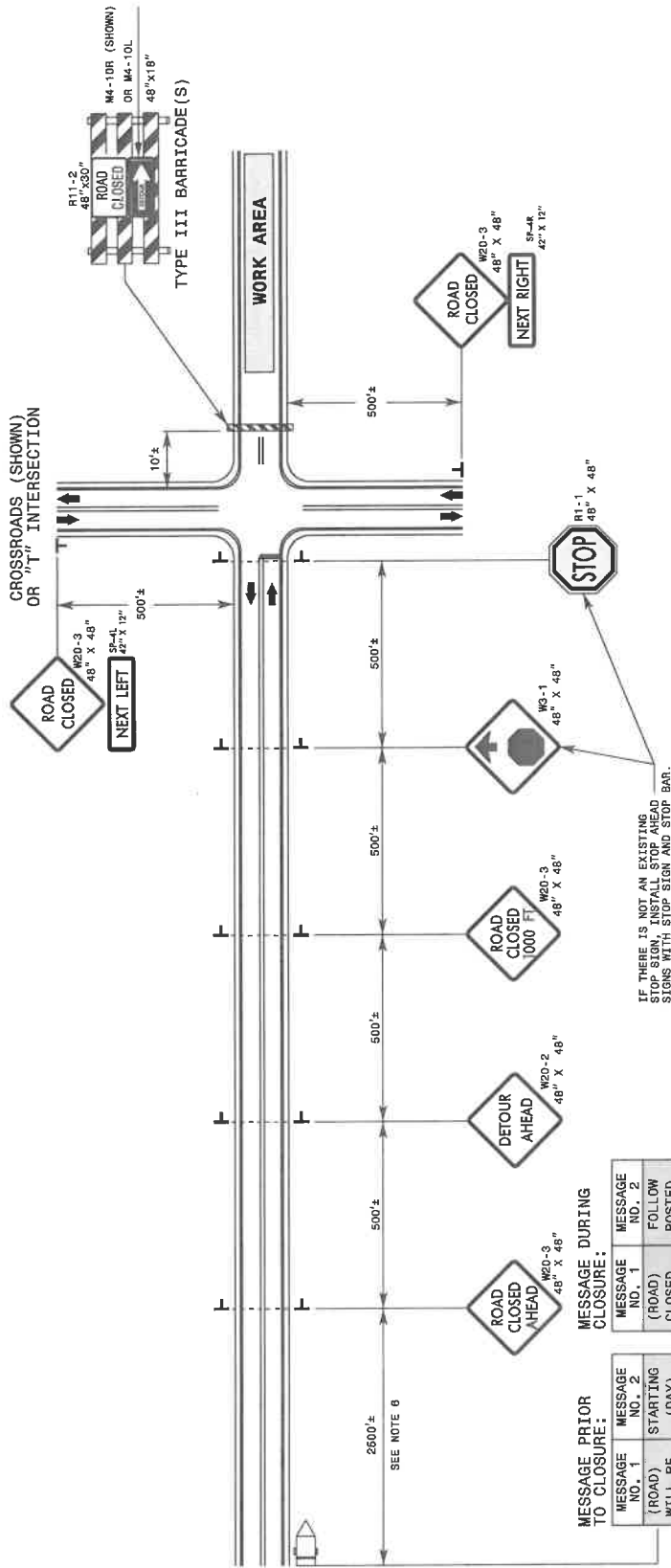
GENERAL NOTES

- 1- REFER TO RSD. 1101.11, SHEETS 1 & 4, FOR "L" DISTANCE AND SIGN SPACING.
- 2- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC.
- 3- REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 4- FOR POSTED SPEED LIMITS BELOW 45 MPH, CHANNELIZING DEVICE SPACING SHALL BE 40 FEET IN THE TANGENTS. FOR POSTED SPEED LIMITS GREATER THAN OR EQUAL TO 45 MPH, CHANNELIZING DEVICE SPACING SHALL BE 80 FEET IN THE TANGENTS.
- 5- EXTEND LANE CLOSURES AT THE BUFFER SPACE SUCH THAT STOPPING SIGHT DISTANCE IS PROVIDED TO THE SIGNAL (REFER TO RSD 1101.11, SHEET 2).
- 6- DRUMS OR SKINNY DRUMS MAY BE USED IN LIEU OF CONES. REFER TO RSD 1180.01 FOR SKINNY DRUM REQUIREMENTS.
- 7- REFER TO THE CURRENT MUTCD FOR REQUIREMENTS AND PROCEDURES.
- 8- DO NOT EXCEED A 1 MILE LANE CLOSURE LENGTH UNLESS OTHERWISE SHOWN IN THE TMP OR AS DIRECTED BY THE ENGINEER.
- 9- DO NOT PLACE TEMPORARY SIGNALS IN LOCATIONS SUCH THAT VEHICLE QUEUES WILL REACH WITHIN 100' OF EITHER SIDE OF ACTIVE RAILROAD TRACKS OR ANOTHER SIGNALIZED INTERSECTION.
- 10- INTERSECTIONS AND DRIVEWAYS LOCATED WITHIN THE ONE-LANE OPERATION WILL REQUIRE ADDITIONAL SIGNALIZATION.
- 11- ALLOW ADEQUATE ROOM FOR ROAD USERS TO RETURN TO THEIR NORMAL DRIVING PATH.
- 12- STOP BARS REQUIRED FOR OPERATIONS OCCUPYING A SINGLE LOCATION FOR MORE THAN ONE DAYLIGHT PERIOD.
- 13- THIS DETAIL IS APPLICABLE FOR OPERATIONS IN PLACE FOR 72 HOURS OR LESS. FOR LONGER DURATION OPERATIONS, SIGNING AND PAVEMENT MARKINGS MAY NEED TO BE ALTERED.

LEGEND

- ⊏ PORTABLE SIGN
- ← DIRECTION OF TRAFFIC FLOW
- ⊠ PORTABLE SIGNAL
- ▲ CONE

ROADWAY STANDARD DRAWING FOR
 TEMPORARY ROAD CLOSURES
 CLOSURE AT DETOUR POINT



MESSAGE PRIOR TO CLOSURE:

| | |
|------------------------|------------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| (ROAD) WILL BE CLOSED | STARTING (DAY) |
| | (TIME) |
| CHANGABLE MESSAGE SIGN | CHANGABLE MESSAGE SIGN |

MESSAGE DURING CLOSURE:

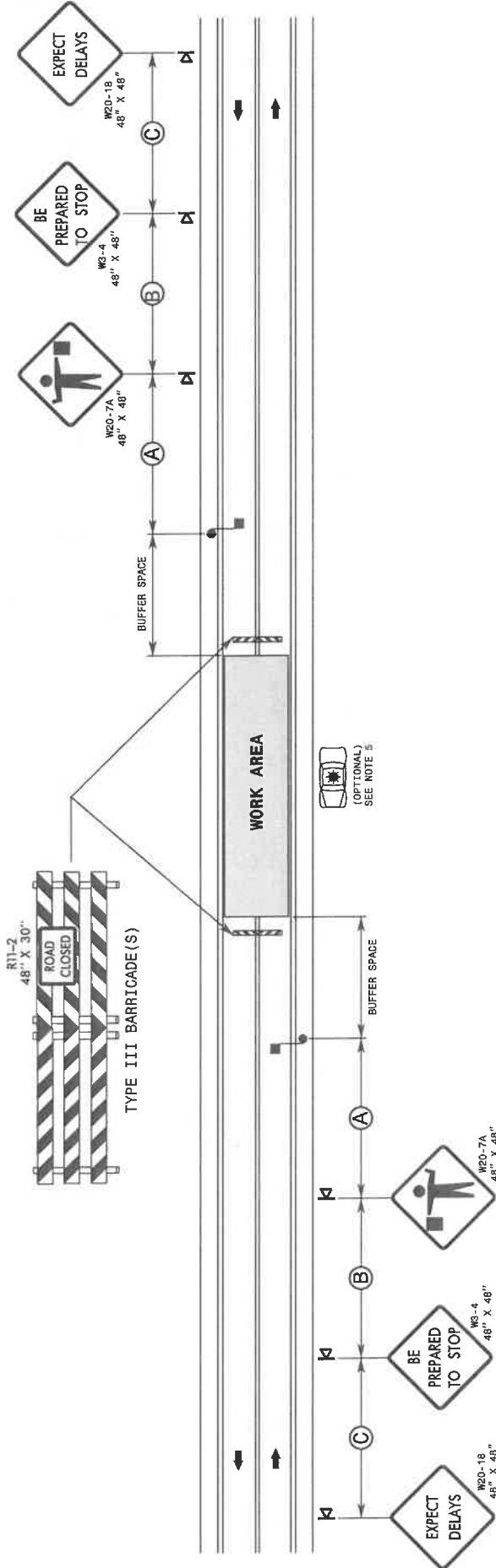
| | |
|------------------------|------------------------|
| MESSAGE NO. 1 | MESSAGE NO. 2 |
| (ROAD) CLOSED | FOLLOW |
| XX MI | POSTED |
| | DETOUR |
| CHANGABLE MESSAGE SIGN | CHANGABLE MESSAGE SIGN |

GENERAL NOTES

- 1- USE THIS RSD. FOR CROSS-ROADS AS SHOWN OR FOR "T" INTERSECTIONS ON 2-LANE, 2-WAY AND MULTILANE UNDIVIDED AND DIVIDED ROADWAYS.
- 2- INSTALLATION OF DETOUR ROUTING PANELS, TEMPORARY ROUTE MARKERS, DESTINATION SIGNS, AND ANY NECESSARY MODIFICATIONS TO EXISTING OR PROPOSED REGULATORY OR WARNING SIGNS WILL BE MADE BY OTHERS (STATE OR CITY FORCES) UNLESS OTHERWISE DESIGNATED IN THE PLANS. A MINIMUM 30 CALENDAR DAY NOTICE IS REQUIRED TO BE PROVIDED TO STATE FORCES BEFORE A ROADWAY IS CLOSED TO TRAFFIC SUCH THAT THE NECESSARY PROVISIONS CAN BE MADE TO INSTALL DETOUR ROUTE SIGNS, AND TO INFORM LOCAL EMERGENCY AND LAW ENFORCEMENT PERSONNEL, SCHOOLS, OR ANY OTHER PARTIES AFFECTED BY THE ROAD CLOSURE.
- 3- INSTALL SIGNS BEFORE BARRICADES WHEN CLOSING THE ROADWAY TO TRAFFIC. REMOVE BARRICADES BEFORE SIGNS WHEN OPENING THE ROADWAY TO TRAFFIC. INSTALL/REMOVE SIGNS AND BARRICADES WITHIN THE SAME CALENDAR DAY.
- 4- POSITION BARRICADES SUCH THAT THE STRIPES ARE SLOPED DOWNWARD IN THE DIRECTION TOWARD WHICH TRAFFIC MUST TURN IN DETOURING.
- 5- USE PORTABLE SIGNS IF ROAD CLOSURE IS TO BE IMPLEMENTED FOR LESS THAN THREE DAYS, OR FOR EMERGENCIES. IN THIS CASE, NO STOP BAR IS NECESSARY.
- 6- USE CMS ON ROADWAYS EXCEEDING 5000 ADT. CMS SHOULD BE POSITIONED NO MORE THAN 7 DAYS BEFORE CLOSURE AND SHOULD REMAIN FOR AT LEAST 2 WEEKS AFTER CLOSURE BEGINS, OR AS DIRECTED BY THE ENGINEER. DATES SHOULD BE AVOIDED ON THE CMS, USE DAY OF WEEK INSTEAD.

LEGEND

- STATIONARY SIGN
- ⇐ DIRECTION OF TRAFFIC FLOW
- ▤ TYPE III BARRICADE
- ◻ CHANGEABLE MESSAGE SIGN (CMS)



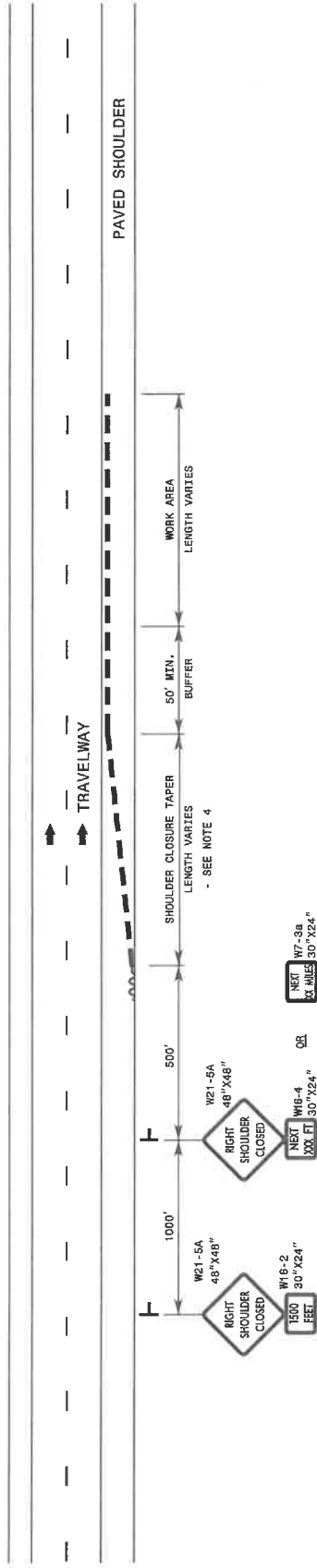
GENERAL NOTES

- 1- USE THIS DETAIL FOR OPERATIONS SUCH AS ERECTION AND REMOVAL OF OVERHEAD SIGNS, BRIDGE GIRDERS, OR OTHER TYPES OF CONSTRUCTION OVER LOW VOLUME, SECONDARY ROADS WHERE THE ENTIRE TRAVELWAY MUST BE CLOSED TO TRAFFIC.
- 2- DO NOT EXCEED 30 MINUTES AT A TIME FOR ROAD CLOSURE.
- 3- IMPLEMENT THIS TYPE OF ROAD CLOSURE DURING PERIODS OF LOW TRAFFIC VOLUMES. DEplete THE EXISTING TRAFFIC QUEUE BEFORE REclosing THE ROADWAY TO TRAFFIC IF MULTIPLE CLOSURES ARE REQUIRED.
- 4- REFER TO RSD. 1101.11, SHEETS 2 & 4, FOR SIGN SPACING AND BUFFER SPACE.
- 5- LAW ENFORCEMENT MAY BE USED TO SUPPLEMENT FLAGGER, AS DIRECTED BY THE ENGINEER.
- 6- USE FLAGGERS TO CONTROL TRAFFIC AT INTERSECTIONS AFFECTED BY THE ROAD CLOSURE. SUPPLEMENT FLAGGERS LOCATED AT INTERSECTIONS WITH FLAGGER AHEAD SIGNS (W20-7A) PLACED APPROXIMATELY 250 FT. IN ADVANCE OF THE FLAGGER. FOR SIGNALIZED INTERSECTIONS PLACE SIGNALS IN THE FLASH MODE AND USE LAW ENFORCEMENT.
- 7- REFER TO THE CURRENT MUTOD FOR FLAGGER CONTROL, REQUIREMENTS, AND PROCEDURES.
- 8- DO NOT EXCEED A 1 MILE ROAD CLOSURE LENGTH UNLESS OTHERWISE SHOWN IN THE TMP OR AS DIRECTED BY THE ENGINEER.
- 9- IF VEHICLE QUEUES WILL REACH WITHIN 100' OF EITHER SIDE OF ACTIVE RAILROAD TRACKS, PROVIDE A UNIFORMED LAW ENFORCEMENT OFFICER OR FLAGGER TO PREVENT VEHICLES FROM STOPPING WITHIN THE GRADE CROSSING. PROVIDE OFFICER OR FLAGGER EVEN IF AUTOMATIC WARNING MEASURES ALREADY EXIST.

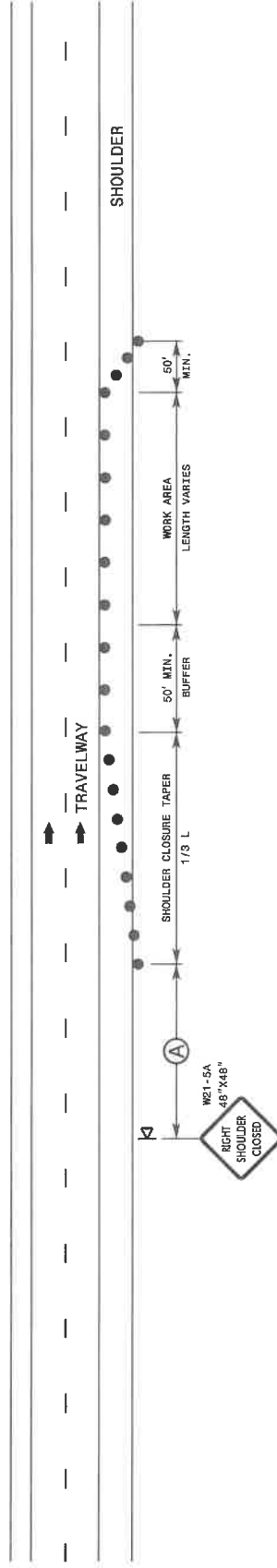
LEGEND

| | |
|--|---------------------------|
| | TYPE III BARRICADE |
| | PORTABLE SIGN |
| | FLAGGER |
| | DIRECTION OF TRAFFIC FLOW |
| | LAW ENFORCEMENT |

SHOULDER CLOSURE ON DIVIDED FACILITIES USING BARRIER



SHOULDER CLOSURE ON DIVIDED FACILITIES USING CHANNELIZING DEVICES

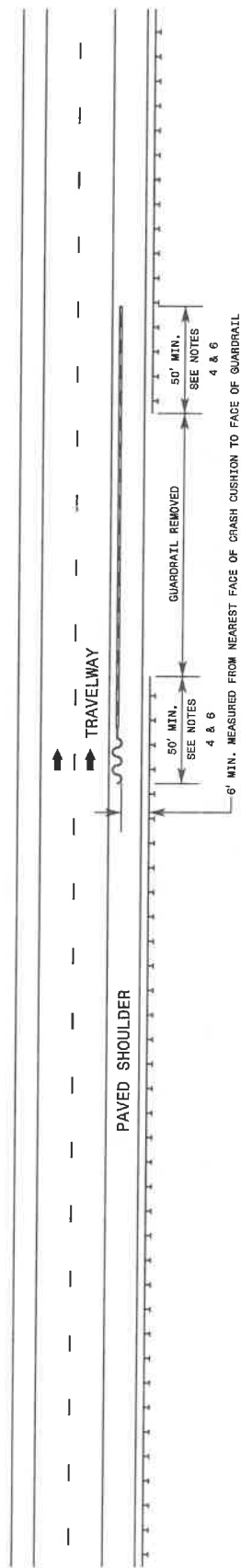


GENERAL NOTES

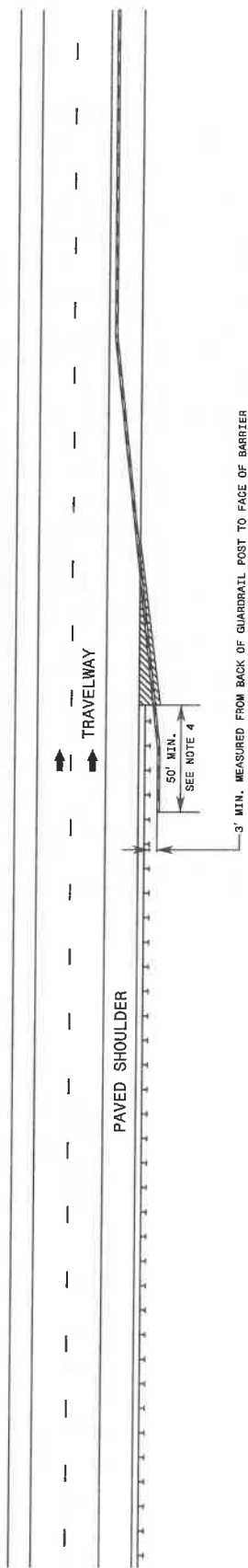
- 1- PLACE SHOULDER CLOSURE SIGNS ON THE SAME SIDE AS THE SHOULDER THAT IS CLOSED.
- 2- PLACE DRUMS IN THE SHOULDER TAPER AT THE MAXIMUM SPACING EQUAL IN FEET TO THE POSTED SPEED LIMIT. THE MAXIMUM SPACING OF DRUMS ALONG THE WORK AREA IS EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 3- USE STATIONARY SIGNS FOR OPERATIONS IN EFFECT LONGER THAN 72 HOURS.
- 4- REFER TO RSD. 1101.11, SHEETS 1, 3 & 4, FOR "L" DISTANCE, BARRIER FLARE RATES, AND SIGN SPACING.

| LEGEND | |
|--------|---------------------------|
| ● | DRUM |
| ⊥ | STATIONARY SIGN |
| ⊥ | PORTABLE SIGN |
| - - - | PORTABLE CONCRETE BARRIER |
| → | DIRECTION OF TRAFFIC FLOW |
| ~ | TEMPORARY CRASH CUSHION |

SHOULDER CLOSURE ON DIVIDED FACILITIES
WITH GUARDRAIL/PCB OVERLAP



SHOULDER CLOSURE ON DIVIDED FACILITIES
PCB BEHIND GUARDRAIL



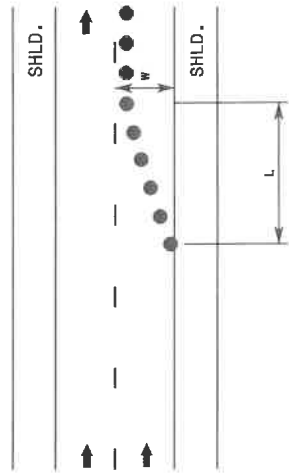
GENERAL NOTES

- 1- PLACE SHOULDER CLOSURE SIGNS ON THE SAME SIDE AS THE SHOULDER THAT IS CLOSED.
- 2- REFER TO RSD. 1101.11, SHEETS 1, 3 & 4, FOR "L" DISTANCE, BARRIER FLARE RATES, AND SIGN SPACING.
- 3- THESE STANDARDS SHALL ONLY BE USED WHEN A B-77 TRANSITION IS NOT FEASIBLE, OR AS DETERMINED BY THE ENGINEER.
- 4- 50' MIN OVERLAP REQUIRED WHEN NO APPROVED GUARDRAIL ANCHOR IS PRESENT. IF GUARDRAIL END IS ANCHORED, THEN OVERLAP CAN BE REDUCED TO 10 FEET.
5. DETAILS CAN BE REVERSED FOR LEFT SHOULDER.
6. FOR NON-REDIRECTIVE (GATING) CRASH CUSHION SYSTEMS, THE 50' OF REQUIRED OVERLAP SHALL NOT INCLUDE ANY PORTION OF THE CRASH CUSHION.

LEGEND

- ▨ TEMPORARY PAVEMENT
- — — — — EXISTING GUARDRAIL
- — — — — PORTABLE CONCRETE BARRIER
- ➔ DIRECTION OF TRAFFIC FLOW
- ~ ~ ~ TEMPORARY CRASH CUSHION

EXAMPLE OF "L" & "W" DESIGNATIONS



TAPER LENGTH CRITERIA FOR CHANNELIZING DEVICES IN WORK ZONES

| TYPES OF TAPERS | TAPER LENGTH |
|----------------------------|------------------------|
| UPSTREAM TAPER | |
| MERGING TAPER..... | L MINIMUM |
| SHIFTING TAPER..... | 1/2 L MINIMUM + |
| SHOULDER TAPER..... | 1/2 L MINIMUM |
| TWO-WAY TRAFFIC TAPER..... | .50 - 100 FEET MAXIMUM |
| DOWNSTREAM TAPER..... | 100 FEET MAXIMUM |

DO NOT INTRODUCE A MERGING OR SHIFTING TAPER WITHIN A CURVE OF THE ROADWAY

*USE FULL L FOR CONTROLLED-ACCESS FACILITIES UNLESS RESTRICTED BY FIELD CONDITIONS AS DETERMINED BY THE TRAFFIC CONTROL SUPERVISOR OR THE ENGINEER.

GENERAL NOTES

1- TABLE FOR "L" DISTANCE IS BASED ON CHANNELIZATION TAPER FORMULA FROM THE MUTCD.
WHERE:

| SPEED LIMIT | FORMULA |
|-------------------|-------------------------------------|
| 40 MPH OR LESS | $L_{MIN} = W \times \frac{S^2}{60}$ |
| 45 MPH OR GREATER | $L_{MIN} = W \times S$ |

L = MINIMUM TAPER LENGTH IN FEET (LONGITUDINAL DISTANCE)
W = WIDTH OF OFFSET IN FEET (LATERAL DISTANCE)
S = EXISTING POSTED SPEED LIMIT PRIOR TO THE IMPLEMENTATION OF A WORK ZONE SPEED LIMIT REDUCTION

2- "L" DISTANCE IS FOR APPLICATION WITH CHANNELIZING DEVICE AND PAVEMENT MARKING TAPERS AND TRANSITIONS. CHANNELIZING DEVICES INCLUDE DRUMS, CONES, TUBULAR MARKERS, BARRICADES, RAISED ASPHALT ISLANDS, AND VERTICAL PANELS.

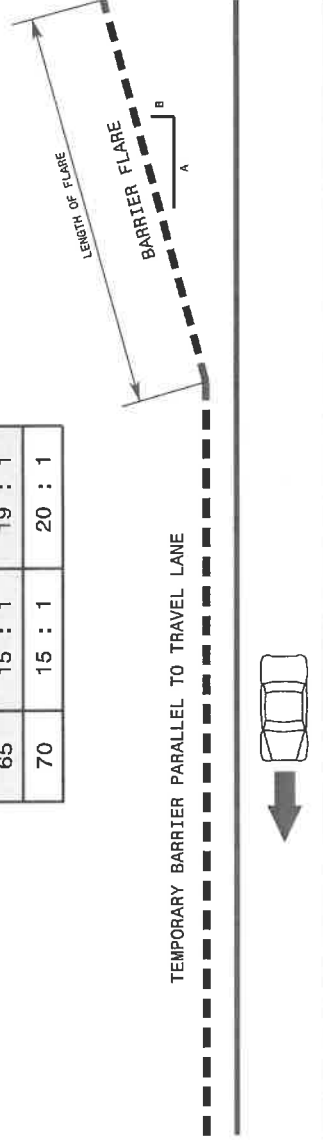
| QUICK REFERENCE - "L" DISTANCE TABLE | | | | | | | | | | | | |
|--|--------------------------|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|
| MINIMUM LONGITUDINAL DISTANCE "L" (FEET) | | | | | | | | | | | | |
| POSTED SPEED $\frac{1}{2} S^2$ (MPH) | LATERAL WIDTH "W" (FEET) | | | | | | | | | | | |
| | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 |
| 20 | 10 | 15 | 20 | 30 | 35 | 40 | 50 | 55 | 60 | 70 | 75 | 80 |
| 25 | 15 | 25 | 35 | 45 | 55 | 65 | 75 | 85 | 95 | 105 | 115 | 125 |
| 30 | 15 | 30 | 45 | 60 | 75 | 90 | 105 | 120 | 135 | 150 | 165 | 180 |
| 35 | 25 | 45 | 65 | 85 | 105 | 125 | 145 | 165 | 185 | 205 | 225 | 245 |
| 40 | 30 | 55 | 80 | 110 | 135 | 160 | 190 | 215 | 240 | 270 | 295 | 320 |
| 45 | 45 | 90 | 135 | 180 | 225 | 270 | 315 | 360 | 405 | 450 | 495 | 540 |
| 50 | 50 | 100 | 150 | 200 | 250 | 300 | 350 | 400 | 450 | 500 | 550 | 600 |
| 55 | 55 | 110 | 165 | 220 | 275 | 330 | 385 | 440 | 495 | 550 | 605 | 660 |
| 60 | 60 | 120 | 180 | 240 | 300 | 360 | 420 | 480 | 540 | 600 | 660 | 720 |
| 65 | 65 | 130 | 195 | 260 | 325 | 390 | 455 | 520 | 585 | 650 | 715 | 780 |
| 70 | 70 | 140 | 210 | 280 | 350 | 420 | 490 | 560 | 630 | 700 | 770 | 840 |

| DESIGN SPEED (MPH) | MINIMUM SIGHT DISTANCE (FEET) | | MINIMUM LONGITUDINAL BUFFER SPACE (FEET) |
|--------------------|-------------------------------|-----------|--|
| | STOPPING | TRAVELING | |
| 30 | 200 | 85 | 85 |
| 35 | 250 | 120 | 120 |
| 40 | 305 | 155 | 155 |
| 45 | 360 | 195 | 195 |
| 50 | 425 | 240 | 240 |
| 55 | 495 | 290 | 290 |
| 60 | 570 | 345 | 345 |
| 65 | 645 | 405 | 405 |
| 70 | 730 | 470 | 470 |
| 75 | 820 | 540 | 540 |
| 80 | 910 | 615 | 615 |

GENERAL NOTES

- 1- TABLES ARE BASED ON THE AASHTO GREEN BOOK "A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS" AND THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES". MINIMUM SIGHT DISTANCE VALUES ARE FOR PASSENGER CAR VEHICLES ON WET AND LEVEL ROADWAYS. CONSULT THE AASHTO GREEN BOOK TO MAKE FINAL DETERMINATION OF STOPPING SIGHT DISTANCE REQUIREMENTS.
- 2- BUFFER SPACE TABLE IS BASED ON THE BRAKING DISTANCE PORTION OF STOPPING SIGHT DISTANCE FOR WET AND LEVEL PAVEMENTS.
- 3- USE OF STOPPING SIGHT DISTANCE IN TRAFFIC CONTROL PLAN APPLICATIONS INCLUDES PROVIDING SIGHT DISTANCE FOR TRAFFIC APPROACHING A LANE CLOSURE. PROVIDE 2-LANE, 2-WAY ROADWAYS STOPPING SIGHT DISTANCE TO THE FLAGGER. FOR LANE CLOSURES ON MULTILANE ROADWAYS PROVIDE STOPPING SIGHT DISTANCE TO THE BEGINNING OF THE LANE CLOSURE MERGE TAPER, OR FLASHING ARROW BOARD. EXTEND LANE CLOSURES AT THE BUFFER SPACE SUCH THAT STOPPING SIGHT DISTANCE IS PROVIDED.

| TEMPORARY BARRIER FLARE RATES | | |
|-------------------------------|------------------|----------------|
| POSTED SPEED LIMIT (MPH) | UNANCHORED (A:B) | ANCHORED (A:B) |
| ≤ 30 | 7 : 1 | 8 : 1 |
| 35 | 8 : 1 | 9 : 1 |
| 40 | 8 : 1 | 10 : 1 |
| 45 | 10 : 1 | 12 : 1 |
| 50 | 11 : 1 | 14 : 1 |
| 55 | 12 : 1 | 16 : 1 |
| 60 | 14 : 1 | 18 : 1 |
| 65 | 15 : 1 | 19 : 1 |
| 70 | 15 : 1 | 20 : 1 |

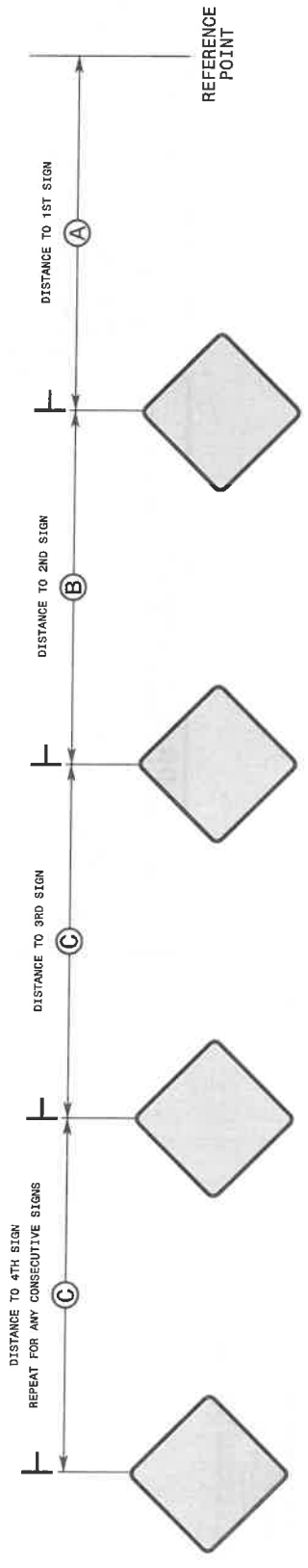


GENERAL NOTES

- 1- REFER TO THE LATEST EDITION OF THE ROADSIDE DESIGN GUIDE.
- 2- A BARRIER IS CONSIDERED FLARED WHEN IT IS NOT PARALLEL TO THE EDGE OF THE TRAVELWAY.

| ADVANCE WARNING SIGN SPACING CHART | | | |
|------------------------------------|--|------|------|
| POSTED SPEED LIMIT (MPH) | RECOMMENDED DISTANCE BETWEEN SIGNS (FEET)± | | |
| | (A) | (B) | (C) |
| ≤ 35 | 200 | 200 | 200 |
| 40-50 | 350 | 350 | 350 |
| 55 | 500 | 500 | 500 |
| CONTROLLED ACCESS ROADS (≥ 55) | 1000 | 1500 | 2700 |

STATIONARY OR PORTABLE SIGNS



GENERAL NOTES

- 1- REFER TO THE LATEST EDITION OF THE MUTCD.
- 2- USE THIS STANDARD DRAWING IN CONJUNCTION WITH OTHER TRAFFIC CONTROL ROADWAY STANDARD DRAWINGS WHERE SIGN SPACING DISTANCES A, B, C, ARE SPECIFIED.
- 3- APPLY THE ADVANCE WARNING SIGN SPACING CHART WHERE A SERIES OF 2 OR MORE SIGNS ARE USED. ALL SIGN SPACING DIMENSIONS ARE APPROXIMATE. FIELD ADJUST AS VARIOUS CONDITIONS OCCUR, SUCH AS LIMITED SIGHT DISTANCE, OBSTRUCTION INTERFERENCE, ETC.

HARNETT REGIONAL WATER

Erwin US 421

Water Line Extension 2026

ERWIN, NORTH CAROLINA

GOVERNING AGENCIES

ENGINEERING

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CONTACT: KEVIN HEARSH, PE, UTILITY ENGINEER

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EMAIL: LDRH@NCMDOT.GOV
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PHONE: 918-720-9102
EMAIL: SRAWN.GUYER@DEQ.NC.GOV
CONTACT: SHAWN GUYER, PE

EROSION CONTROL

NC DEQ - DEMLR
FAYETTEVILLE REGIONAL OFFICE
200 WEST F STREET
ERWIN, NORTH CAROLINA 27994-1654
PHONE: 918-433-3380
CONTACT: TIM LABOUNTY, PE

TOWN OF ERWIN

100 WEST F STREET
ERWIN, NORTH CAROLINA 28339
PHONE: 918-691-4400
EMAIL: GREG@ERWIN.NC.GOV
CONTACT: SNOW BOWEN, TOWN MANAGER

DRAWING INDEX

G-01 COVER SHEET
G-02 CONSTRUCTION NOTES
CI-01 OVERALL UTILITY PLAN
D-01 STANDARD DETAILS

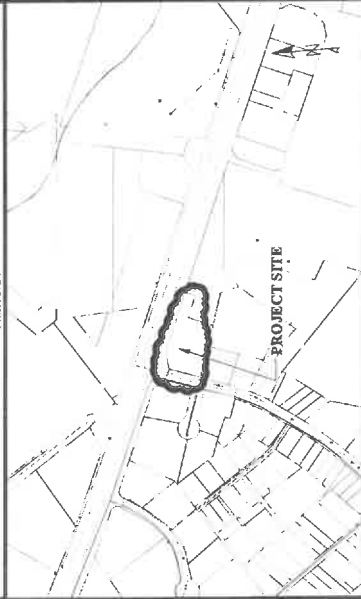
PROJECT TEAM

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PHONE: 918-893-3575
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PHONE: 918-893-3575
EMAIL: JAY@MEYERSENGINEERING.NET
CONTACT: JAY T. MEYERS, PE

VICINITY MAP

Scale: NTS



CONFORMED DRAWINGS



UNREGISTERED CONSULTANT

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ENGINEERING, PLLC

INCLUDES PLAN
FOR CARRIAGE PARK COURT
STRAZSALL
JAY@MEYERSENGINEERING.NET

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CONSTRUCTION DRAWINGS

ERWIN US 421 WATER LINE EXTENSION 2026

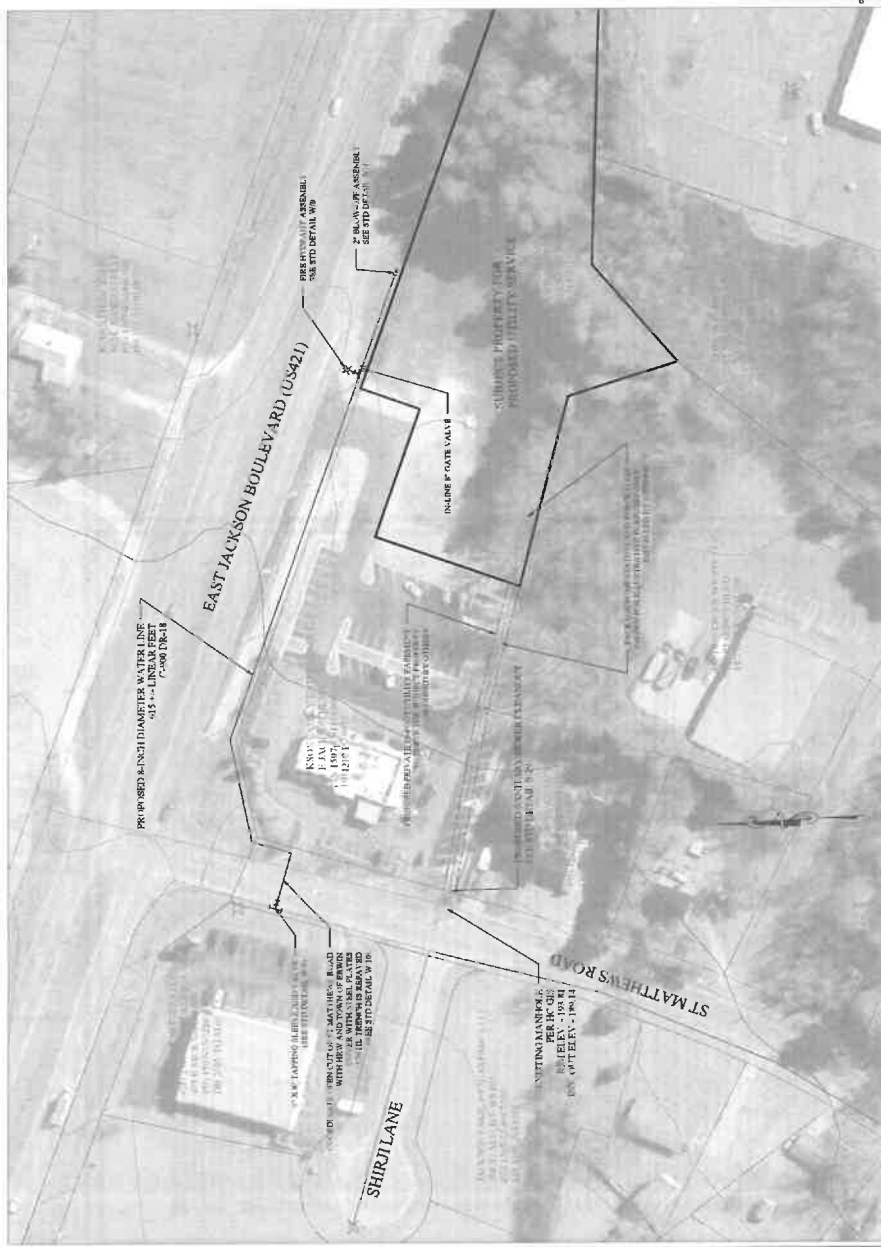
COVER SHEET

G-01

REV: _____ DATE: _____ BY: _____

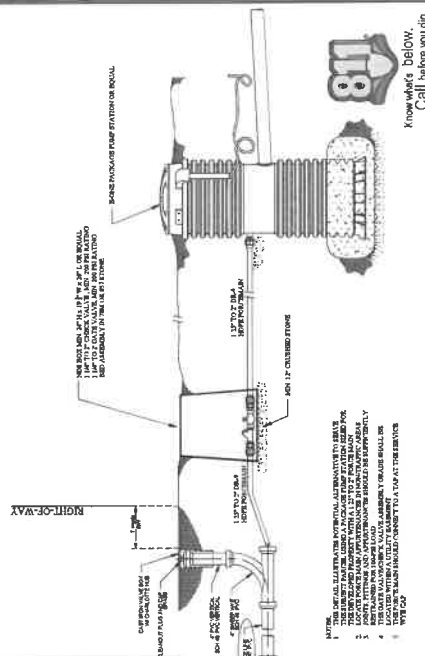
GENERAL NOTES

1. THIS PROJECT IS BASED ON HARNETT COUNTY, NC PROJECT # FOR THE EXTENSION OF THE MAIN WATER LINE AND FOR INSTALLING A SANITARY SEWER SERVICE TO PROVIDE UTILITIES FOR THE SUBJECT PROPERTY.
2. THE SUBJECT PROPERTY DEVELOPER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE HARNETT COUNTY PLANNING DEPARTMENT.
3. THE SUBJECT PROPERTY DEVELOPER SHALL INSTALL THE PUMP STATION IN LOCATION BASED ON THE SITE DESIGN AS WELL AS THE REQUIREMENTS OF THE HARNETT COUNTY PLANNING DEPARTMENT.
4. THE SUBJECT PROPERTY DEVELOPER SHALL PROVIDE ALL NECESSARY UTILITY LOCATIONS TO THE CONTRACTOR.
5. CONTRACTOR SHALL LOCATE ALL EXISTING UTILITIES AND PROVIDE TO THE ENGINEER AS PART OF THE PRELIMINARY DESIGN.
6. CONTRACTOR SHALL PROVIDE ALL NECESSARY UTILITY LOCATIONS TO THE ENGINEER AS PART OF THE PRELIMINARY DESIGN.
7. CONTRACTOR SHALL PROVIDE ALL NECESSARY UTILITY LOCATIONS TO THE ENGINEER AS PART OF THE PRELIMINARY DESIGN.
8. REFER TO STANDARD NOTES ON SHEET C-01 FOR ADDITIONAL REQUIREMENTS.



LEGEND

| | |
|------------------------------------|--------------------|
| PROPOSED 8" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 15" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 24" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 36" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 48" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 60" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 72" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 84" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 96" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 108" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 120" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 144" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 180" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 216" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 240" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 288" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 360" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 432" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 504" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 576" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 648" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 720" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 792" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 864" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 936" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 1008" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 1080" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 1296" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 1440" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 1512" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 1584" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 1656" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 1728" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 1800" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 1872" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 1944" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 2016" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 2088" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 2160" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 2232" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 2304" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 2376" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 2448" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 2520" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 2592" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 5112" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 5256" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 5328" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 5400" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 5472" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 5688" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 5760" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 5832" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 6120" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 6192" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 6264" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 6336" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 6480" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 7992" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 8712" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 8784" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 8856" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 8928" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 9000" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 9072" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 9144" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 9288" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 9432" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 9504" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 9792" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
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| PROPOSED 9936" DIAMETER WATERLINE | 15' MIN. CLEARANCE |
| PROPOSED 10008" DIAMETER WATERLINE | 15' MIN. CLEARANCE |



Know what's below.
Call before you dig.

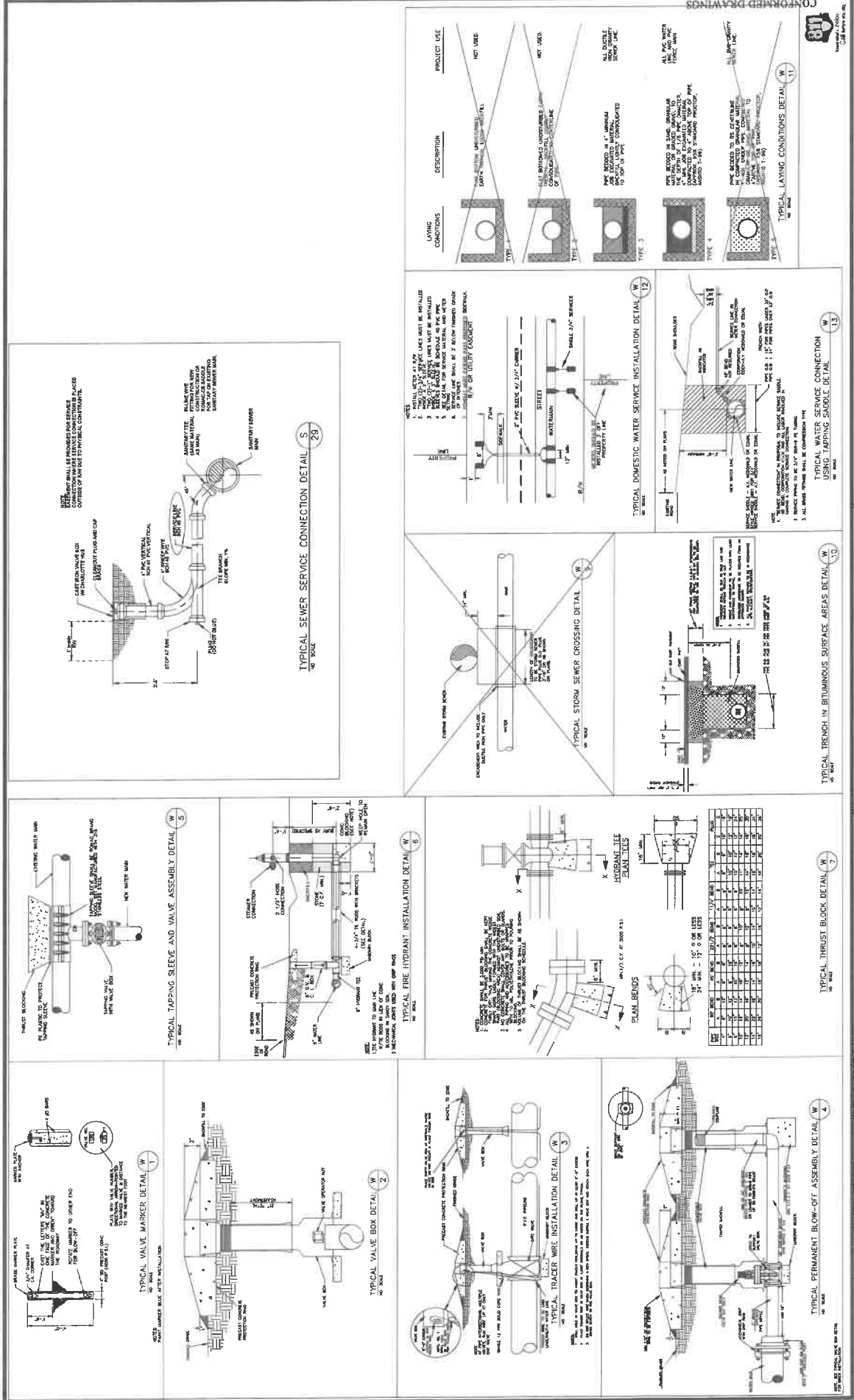
HARNETT REGIONAL WATER
700 MCGINNEY PARKWAY
LILLINGTON, NORTH CAROLINA 27546
919-893-7575
CONTACT: TOMMY BURKS, DIRECTOR

MEYERS ENGINEERING, PLLC
INC. LICENSE # 1151
THE CAROLINA PARK CENTER
514-545-5414
JAY@MEYERSENGINEERING.NET



ISSUED FOR: _____ DATE BY: _____

CONSTRUCTION DRAWINGS
ERWIN US 421 WATER LINE EXTENSION 2026
OVERALL PROJECT



CONFORMED DRAWINGS

HARNETT REGIONAL WATER
 700 MCKINNEY PARKWAY
 LILLINGTON, NORTH CAROLINA 27546
 910-893-8755
 CONTACT: TOMMY BURNS, DIRECTOR

CONSTRUCTION DRAWINGS
ERWIN US 421 WATER LINE EXTENSION 2026
 STANDARD DETAILS

D-01

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 LILLINGTON, NORTH CAROLINA 27546
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 FAYETTEVILLE, NORTH CAROLINA 27834
 910-436-8411
 JAY@MEYERSENGINEERING.NET

ISSUED FOR: _____ DATE: ____ BY: _____

